

**To:**  
**All members of the**  
**Environment and Sustainability**  
**Committee**

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Date: 16 June 2025

### Supplementary Agenda

### **Environment and Sustainability Committee - Tuesday, 17 June 2025**

Dear Councillor

I enclose the following items which have been slightly updated since publication on the agenda for the Environment and Sustainability Committee meeting to be held on Tuesday, 17 June 2025. Please note that the content of the report and appendices 1-4 have not changed, other than to make slight corrections to the appendix titles. An addendum has been provided to explain what has been updated, and an 'executive summary' has been added as Appendix 5.

**5. Approval to Conduct Statutory Consultation on Spelthorne Design Code 3 - 254**

Committee is asked to agree that the Publication Version of the Spelthorne Design Code be published for a 6-week public consultation under Regulation 12 of the Town and Country Planning (Local Planning) Regulations 2012.

Yours sincerely

Christeen Abee  
Corporate Governance

To the members of the Environment and Sustainability Committee

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S.M. Doran  
N. Islam  
A. Mathur  
J.R. Sexton

J.A. Turner  
H.R.D. Williams  
P.N. Woodward  
M. Buck

Substitute Members:

Councillors: M. Arnold, C. Bateson, H.S. Boparai, A. Gale and  
R.V. Geach



# Environment and Sustainability Committee

17 June 2025



<b>Title</b>	Approval to Conduct Statutory Consultation on Spelthorne Design Code
<b>Purpose of the report</b>	To make a decision
<b>Report Author</b>	Laura Richardson
<b>Ward(s) Affected</b>	All Wards
<b>Exempt</b>	No
<b>Exemption Reason</b>	N/A
<b>Corporate Priority</b>	Environment, Community
<b>Recommendations</b>	<p><b>Committee is asked to:</b></p> <ul style="list-style-type: none"> <li>Agree that the Publication Version of the Spelthorne Design Code be published for a 6-week public consultation under Regulation 12 of the Town and Country Planning (Local Planning) Regulations 2012</li> </ul>
<b>Reason for Recommendation</b>	<p>The Spelthorne Design Code (SDC) has been created in a collaboration between the public, a Task Group consisting of Members on a cross-party basis, supported by officers in the Strategic Planning Team and the consultant team, David Lock Associates and Feria Urbanism. Its purpose is to support the delivery of high-quality places. The Design Code aims to encourage the delivery of sustainable and locally distinctive development across the Borough.</p> <p>Regulation 12 of the Town and Country Planning (Local Planning) Regulations 2012 states that (b)(i) before it can be adopted, an Supplementary Planning Document (SPD) must be consulted on for a minimum of 4 weeks, which is also part of Spelthorne's Statement of Community Involvement. However, a 6-week consultation period is proposed to provide residents and stakeholders with ample time to share their views.</p>

## 1. Summary of the report

What is the situation	Why we want to do something
<ul style="list-style-type: none"> <li>Spelthorne Borough Council has been preparing a Borough-wide Design Code since mid-2024. This code aims to provide clear and locally-specific design guidance for new development, in accordance with the requirements of the Levelling Up and Regeneration Act 2023. This legislation mandates all local authorities to develop an area-wide Design Code to ensure the creation of high-quality places. Additionally, the SDC addresses concerns from both residents and elected Members regarding the importance of high-quality design in development projects.</li> </ul>	<ul style="list-style-type: none"> <li>The Government expects all local planning authorities to prepare Design Codes to improve the quality, character and sustainability of development. The SDC will help ensure that new developments are well-designed, respond to local character, and deliver high-quality places that meet the needs and expectations of residents. In order to formally adopt the SDC and make it a material consideration in planning decisions, a statutory consultation must be carried out.</li> </ul>
This is what we want to do about it	These are the next steps
<ul style="list-style-type: none"> <li>Seek approval from the Committee to initiate the statutory public consultation regarding the publication version of the SDC. According to planning regulations, this consultation is essential, as it allows residents, developers, and stakeholders the opportunity to formally provide their feedback on the draft document prior to its finalisation.</li> <li>The SDC has been developed through a community-led approach. In light of this, it is proposed to extend the consultation period to 6-week, surpassing the minimum 4-week legal requirement. This extension aims to ensure that residents, developers, and</li> </ul>	<ul style="list-style-type: none"> <li>Secure agreement to proceed with statutory consultation</li> <li>Begin the 6-week consultation period using online, print and public engagement methods</li> <li>Analyse feedback received and revise the draft SDC as necessary</li> <li>Bring the updated SDC to full Council with recommendation to adopt</li> <li>Upon approval, the SDC will be formally adopted as an SPD and used in planning decision-making</li> </ul>

stakeholders have ample time to submit their comments.	
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- 1.1 This report seeks approval to commence the statutory 6-week consultation for the SDC, a document that sets out the design requirements for proposed new development to ensure that it is locally supported, sustainable and functions well for all.
- 1.2 The SDC is set out in 6 chapters:
  - Chapter 1: Introduction - provides an introduction to the document
  - Chapter 2: The Design Process - sets out how applicants and design teams should work to design a development
  - Chapter 3: Places Past, Present and Future - sets out the history, present day and future design vision for the Borough and the key places within it
  - Chapter 4: Area Type Design Requirements - sets out different design requirements for different parts of the Borough, reflecting the fact that they have different characters and different anticipated development types
  - Chapter 5: Areas of Change - sets out more detailed design requirements for development in these 'Areas of Change'
  - Chapter 6: Preparing your Application - sets out how to prepare and submit a planning application, demonstrating that it is Code-compliant
- 1.3 Following the public consultation and consideration of the feedback from the Design Code Task Group, the adoption version will be brought back to the Environment and Sustainability Committee for consideration in the autumn, to seek a recommendation to Council for adoption.

## 2. Key issues

### Background

- 2.1 A design code is "A set of illustrated design requirements that provide specific, detailed parameters for the physical development of a site or area. The graphic and written components of the code should build upon a design vision, such as a masterplan or other design and development framework for a site or area". (National Planning Policy Framework, 2024)
- 2.2 Paragraph 131 of the National Planning Policy Framework (NPPF), updated in December 2024 states: "The creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Being clear about design expectations, and how these will be tested, is essential for achieving this. So too is effective engagement between applicants, communities, local planning authorities and other interests throughout the process."
- 2.3 The Levelling Up and Regeneration Act (2023) (LURA) sets out the legal requirement for Local Authorities (LAs) to develop area wide Design Codes,

which will allow Authorities, working with their communities, to set out what good design looks like for their area and their vision for the future. Following the enactment of the LURA the updated NPPF (December 2024) sets out the national policy expectation for the development and delivery of Design Codes in Paragraphs 131 – 134.

- 2.4 As set out above, the focus on the delivery of high-quality places is expressed in national policy but is also a matter of particular concern for both our residents and elected Members.

#### Bespoke Design Code for Spelthorne

- 2.5 The SDC has been a collaboration between the public, a Task Group consisting of Members on a cross-party basis, supported by officers in the Strategic Planning Team and the consultant team.
- 2.6 The SDC sets out the design requirements for proposed new development in the borough. It will ensure that new development is locally supported, sustainable and functions well for all its users. The SDC will be used to determine whether planning applications are acceptable in design terms and will support the emerging Spelthorne Local Plan. It contains simple, concise, illustrated design requirements for streets, open spaces and buildings. It also sets out expectations for the process to be followed when proposals are designed.
- 2.7 From the outset, the Council has adopted a dynamic, community-focused approach to developing the SDC. The Code is created through an iterative process, which is divided into four stages: Listen, Translate, Test, and Final Code for Adoption. This project includes multiple phases of community engagement, where each phase builds on the findings of the previous one, incorporating feedback loops and continuous refinement. The emphasis on deep community engagement ensures that the resulting Design Code aligns with local values and needs.

#### Community Engagement and Development of the Code

- 2.8 The SDC has taken a community-led approach to co-produce a Design Code for the Borough with local people through active community participation. This approach will help ensure new developments which are coming forward meet the real needs of the borough's diverse communities. Throughout the project and development of the Code, there has been continuous community involvement, both in-person and online.
- 2.9 During the first stage of preparing the Code, the 'Listen' phase, public feedback was gathered to understand the places of Spelthorne in detail, learn about what makes it special, what the key challenges are, and how the Design Code can enhance and improve the Borough in the future. For the online aspect of the public engagement, the SDC Commonplace page, which is an online engagement hub, was set up, which has since welcomed thousands of visitors and a few hundred subscribers keeping up to date with the latest project news.
- 2.10 One of the key features of the first stage of developing the Code was the Interactive Map on Commonplace, which enabled participants to drop pins on specific locations within Spelthorne and share their thoughts and pictures about that place, area or a more specific design feature. This engagement

opportunity was open to all local people for 14 weeks in autumn 2024 and received over 600 contributions.

- 2.11 In addition to online community engagement, the information gathered from local people has been enriched by in-person participation. In 6 locations across the Borough (Staines-upon-Thames Town Centre, Staines-upon-Thames Wider Area, Stanwell, Ashford, Sunbury-on-Thames and Shepperton), a series of 2-hour long Walking Tours were held, which were guided by local people to find out their opinions about local buildings, streets and spaces they liked and disliked and to gain an insight into design issues that matter to them. If local people were unable to attend, there was also a digital alternative offered and these Digital Walks, held virtually, allowed residents to share their views about different parts of the Borough that were of interest by guiding the team around an online map. This resulted in 4.5 hours of digital engagement.
- 2.12 To further ensure comprehensive community involvement, a public drop-in session with interactive activities was held in the Elmsleigh Shopping Centre in November 2025 and provided residents an opportunity to meet the Design Code Team, learn about Spelthorne in detail, engage in discussions, ask questions, and share their perspectives on what makes Spelthorne unique.
- 2.13 In order to ensure that the design of future development within Spelthorne reflects the diverse perspectives of residents, there has also been lots of youth engagement opportunities. Sessions were held at 2 local schools and Ashford Youth Club. Residents from 13-years-old and up were also welcomed to apply to join, and consequently took part in, the SDC Citizens' Panel.
- 2.14 During Stage 1, a Citizens' Panel was also established as a crucial element of the Council's community engagement in the development of the SDC. This Citizens' Panel was created to promote inclusivity and reflect Spelthorne's diverse demographics including age, gender, ethnicity, housing situation and geographical location, according to Census data. Any interested resident in the Borough was invited to apply and over 40 Panel members were selected, following a 7.5 week recruitment period, through a blind selection process to be demographically representative of the Borough, with members therefore representing Spelthorne's diverse communities and providing input into the creation of the SDC.
- 2.15 The Citizens' Panel have been actively involved in the next two stages of the project. Two Citizens' Panel focus sessions with our appointed consultants David Lock Associates and Fera Urbanism were held on 16 November 2024 and 23 November 2024. During the first workshop, public feedback so far was fed in and Panel members explored what makes Spelthorne unique, envisioning a Borough that respects its character while embracing innovation. The second workshop focused on safety and accessibility, discussing how future development can cater to everyone's needs. Members also shared personal experiences and their vision for various edge conditions and building types, considering how development fits within its surroundings. These valuable insights were then taken to be implemented in the Code, so that design rules reflect the values and aspirations of our community.

- 2.16 During the second stage of the project, the 'Translate' phase, the baseline data and community engagement feedback gathered over from local communities and other key stakeholders, along with the Citizens' Panel, was translated and the draft Code developed.
- 2.17 As part of continued community involvement, the Citizens' Panel met for a third time during the next stage of the project, the 'Test' phase', on 1 March 2025 to test the draft Code. This session was designed to engage the Citizens' Panel in reviewing and applying the draft Design Code to real-world development scenarios. The project team explained the Design Code's purpose, development and role in planning, how public feedback has shaped it, and who will use it. Panel members also tested the draft Code by reviewing past developments and applying it to real-world scenarios and example sites.
- 2.18 Overall, during the above-mentioned community engagement periods, there have been several active in-person participation opportunities, with 60+ hours of in-person engagement.
- 2.19 The project is currently in Stage 3, with a draft Code having been developed and tested by the Citizens' Panel, technical stakeholders and also local people during a public engagement opportunity on the draft Code for the wider community that ran for 3 weeks on Commonplace. Local people could download and view the draft Code, find out what was in the draft SDC, see how the draft Code responded to the community and provide feedback to help refine and develop the Code by answering a survey, which received 45 responses.
- 2.20 Continuing in the third stage, having amended the draft Code following Citizens' Panel, technical stakeholder, public and Task Group member feedback, a Publication Version of the SDC has been developed. Subject to the approval of the Environment and Sustainability Committee, a 6-week statutory public consultation will be held this summer, with formal adoption of the SDC expected in autumn.

#### Current Position

- 2.21 The Task Group met on 19 May 2025 for the final review of the publication draft of the SDC and to take a view on whether the Code can proceed to be considered by the Environment and Sustainability Committee.
- 2.22 The SDC is intended to be adopted as an SPD which is a planning policy document that builds upon, and provides more detailed guidance to, policies in the Local Plan.
- 2.23 An SPD forms part of the adopted development plan (also known as the Local Plan) and sets out further guidance to support the implementation of the relevant policies within the Plan. Once complete and having been subjected to the necessary period of consultation, the SDC, upon adoption, can be 'hooked' onto the existing design policy within the current and emerging Spelthorne Local Plan. Thus, it will be used when determining planning applications from the date of the adoption of the SPD.
- 2.24 The SDC includes a checklist for developers that must be submitted as part of a planning application. This checklist helps to clearly demonstrate how all required, recommended, and optional criteria in the Code have been

addressed. It is essential for the Design Code document to be accessible and easy to navigate for both technical and non-technical users.

- 2.25 Subject to Committee approval the proposed statutory consultation would take place between 23<sup>rd</sup> June 2025 to 3<sup>rd</sup> August 2025, therefore running for the proposed 6 weeks. The proposed dates would ensure that a substantial part of the consultation period is prior to the commencement of the school holidays. The consultation would be run on the SDC Commonplace platform, which is the digital engagement hub. The documents would be published on the Council website, as well as being available for the public to view in public libraries and at the Council Offices during office hours.
- 2.26 Promotion of the statutory consultation is further detailed in the Appendix 2- Statutory Consultation Strategy.

#### Digital Design Code

- 2.27 The SDC team is committed to creating a digital version of the Design Code, which will highlight only the relevant sections for each specific proposal. Funding for the digital element of the SDC has been secured as a one-off growth bid for 2025/26. This will enhance usability, accessibility, and the overall effectiveness of the Code in guiding development. The dynamic content will facilitate a more intuitive understanding of design principles, allowing stakeholders to explore different development options and see their real-time impacts, thereby increasing engagement and comprehension.
- 2.28 Additionally, the digital format will allow for easier updates and version control, enabling prompt implementation of changes and immediate dissemination of information. Version histories will be maintained and made public to ensure trust and accountability within the development process. It is anticipated that the digital version of the SDC will be available upon adoption of the Code.

### **3. Options analysis and proposal**

- 3.1 **Option 1 - Recommended option:** The Committee approve the commencement of the public consultation.

**Option 2 – Not Recommended:** The Committee resolve to seek further amendments to the SDC before commencement of public consultation.

This option is not recommended as any delay in the start of the statutory consultation, outside the window set out at para 2.25 would mean that it would not be advisable to begin the consultation until September, as consulting during summer typically results in lower levels of engagement. The knock-on effect of this would mean the adoption of the SDC would be delayed, likely to late 2025/early 2026.

**Option 3 – Not Recommended:** The Committee resolve to reject the request for publication of the SDC.

This option is not recommended as in order to adopt the SDC, so that it can be considered as a material consideration in decision making a statutory consultation is required by legislation as set out above. If a public consultation is not held the project cannot move forward and the SDC cannot be adopted.

#### **4. Financial management comments**

- 4.1 Undertaking and evaluating the consultation to be undertaken by resources covered by existing budgets.

#### **5. Risk management comments**

- 5.1 A risk register is used in projects to document, assess, and manage potential risks that could impact the project's success, ensuring proactive mitigation and response strategies. The risk register for the project is regularly monitored and updated by the Project Manager.
- 5.2 Failure to proceed with the statutory public consultation for the final SDC may result in significant reputational damage to the Council, undermining its commitment to community engagement, transparency, and proactive planning.
- 5.3 The Council has already invested considerable financial and staff resources in the development of the SDC. Not proceeding with the public consultation would render these efforts ineffective, resulting in wasted expenditure without achieving the intended planning and design outcomes.
- 5.4 Without the adoption of the SDC, the Council will be less equipped to take a proactive approach to development management. This may lead to inconsistent planning outcomes and a missed opportunity to deliver high-quality, well-designed places across the Borough.
- 5.5 In the absence of an adopted SDC, developers may proceed with proposals that do not align with the Council's strategic vision for high-quality, sustainable, and inclusive places. This could undermine the Council's objectives for placemaking and community well-being.
- 5.6 The SDC is intended to complement the new Local Plan, currently under examination and anticipated to be adopted in the autumn 2025. Any delay in the adoption of the Design Code risks misalignment with the Local Plan, potentially weakening the overall planning framework and reducing the effectiveness of both documents in achieving comprehensive, cohesive development outcomes.
- 5.7 In light of Local Government Reorganisation (LGR), a new unitary authority may have other priorities. There is a risk that if the consultation and adoption of the SDC is not progressed in line with the current project programme, it may not be implemented.

#### **6. Procurement comments**

- 6.1 No comments received.

#### **7. Legal comments**

- 7.1 The SDC has been developed in accordance with the National Model Design Code.
- 7.2 Public consultation required under Regulation 12 of the Town and Country Planning (Local Planning) Regulations 2012.



## **8. Other considerations**

8.1 There are none.

## **9. Equality and Diversity**

9.1 This will be dealt with as an integral part of the Design Code.

## **10. Sustainability/Climate Change Implications**

10.1 This will be dealt with as an integral part of the Design Code.

## **11. Timetable for implementation**

11.1 The project timeline and general information can be found on [Have Your Say Today - SDC - Commonplace](#)

11.2 The proposed dates for the public consultation are 23<sup>rd</sup> June 2025 to 3<sup>rd</sup> August 2025.

11.3 If the consultation dates are agreed by the Committee and the project proceeds on the agreed timetable, it is anticipated that the SDC will be adopted in autumn 2025.

## **12. Contacts**

12.1 Cllr Burrell is the Design Code Task Group Chair. Other members are Cllrs Gibson, Williams and Clarke. Cllr Beecher attends the Project Board meeting as chair of the Environment and Sustainability Committee.

12.2 Queries for Officers can be emailed to the Design Code Team: [designcode@spelthorne.gov.uk](mailto:designcode@spelthorne.gov.uk)

12.3 Laura Richardson is the Project Lead: [l.richardson@spelthorne.gov.uk](mailto:l.richardson@spelthorne.gov.uk)

**Background papers:** There are none.

### **Appendices:**

Appendix 1 - Spelthorne Design Code – Final Draft for Consultation (May 2025)

Appendix 2 – Statutory Consultation Strategy

Appendix 3 – The Engagement Process (Stage 1 to Stage 2)

Appendix 4 – Draft Code Public Engagement Report (Stage 3)

Appendix 5 – Executive Summary

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# Environment and Sustainability Committee

## Addendum Report

17 June 2025



<b>Title</b>	Addendum - Approval to Conduct Statutory Consultation on Spelthorne Design Code
<b>Purpose of the report</b>	To make a decision
<b>Report Author</b>	Laura Richardson
<b>Ward(s) Affected</b>	All Wards
<b>Exempt</b>	No
<b>Exemption Reason</b>	N/A
<b>Corporate Priority</b>	Environment, Community
<b>Recommendations</b>	<b>Committee is asked to:</b> <ul style="list-style-type: none"><li>• Agree that the Publication Version of the Spelthorne Design Code be published for a 6-week public consultation under Regulation 12 of the Town and Country Planning (Local Planning) Regulations 2012</li></ul>
<b>Reason for Recommendation</b>	<p>The Spelthorne Design Code (SDC) has been created in a collaboration between the public, a Task Group consisting of Members on a cross-party basis, supported by officers in the Strategic Planning Team and the consultant team, David Lock Associates and Feria Urbanism. Its purpose is to support the delivery of high-quality places. The Design Code aims to encourage the delivery of sustainable and locally distinctive development across the Borough.</p> <p>Regulation 12 of the Town and Country Planning (Local Planning) Regulations 2012 states that (b)(i) before it can be adopted, an Supplementary Planning Document (SPD) must be consulted on for a minimum of 4 weeks, which is also part of Spelthorne's Statement of Community Involvement. However, a 6-week consultation period is proposed to provide residents and stakeholders with ample time to share their views.</p>

## **1. Summary of the report**

- 1.1 This addendum report is for clarification. The title of the appendices within the body of the main Committee Report and the title of the appendices have been updated to correct a discrepancy in naming. There has been no change to the content.
- 1.2 Appendices 2, 3 and 4 have been updated to add the appendix number to the title within the document for clarity. There has been no change to the content.
- 1.3 An additional appendix, Appendix 5 – Executive Summary has been published.

## **2. Key issues**

- 2.1 Refer to the main report.

## **3. Options analysis and proposal**

- 3.1 Refer to the main report.

## **4. Financial management comments**

- 4.1 Refer to the main report.

## **5. Risk management comments**

- 5.1 Refer to the main report.

## **6. Procurement comments**

- 6.1 Refer to the main report.

## **7. Legal comments**

- 7.1 Refer to the main report.

## **8. Other considerations**

- 8.1 Refer to the main report.

## **9. Equality and Diversity**

- 9.1 Refer to the main report.

## **10. Sustainability/Climate Change Implications**

- 10.1 Refer to the main report.

## **11. Timetable for implementation**

- 11.1 Refer to the main report.

## **12. Contacts**

- 12.1 Cllr Burrell is the Design Code Task Group Chair. Other members are Cllrs Gibson, Williams and Clarke. Cllr Beecher attends the Project Board meeting as chair of the Environment and Sustainability Committee.

- 12.2 Queries for Officers can be emailed to the Design Code Team:  
[designcode@spelthorne.gov.uk](mailto:designcode@spelthorne.gov.uk)
- 12.3 Laura Richardson is the Project Lead: [l.richardson@spelthorne.gov.uk](mailto:l.richardson@spelthorne.gov.uk)

**Background papers:** There are none.

**Appendices:**

- Appendix 1 - Spelthorne Design Code – Final Draft for Consultation (May 2025)
- Appendix 2 – Statutory Consultation Strategy
- Appendix 3 – The Engagement Process (Stage 1 to Stage 2)
- Appendix 4 – Draft Code Public Engagement Report (Stage 3)
- Appendix 5 – Executive Summary

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# SPELTHORNE DESIGN CODE

SPELTHORNE BOROUGH COUNCIL

FINAL DRAFT FOR CONSULTATION  
MAY 2025



The Spelthorne Design Code Project Team would like to acknowledge everyone in the community in Spelthorne who has given up time to contribute to the development of this Code, whether through walking tours, school and youth groups, online feedback, public events or as part of the Citizens Panel. Your assistance, expertise and enthusiasm has been invaluable and is much appreciated.

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Prepared for Spelthorne Borough Council by



Fathom Architects





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# Introduction

## WHAT IS THE SPELTHORNE DESIGN CODE?

The Spelthorne Design Code has been produced to provide a framework to support high quality design in the borough, that is reflective of local character and design preferences. It sets out the design requirements for proposed new development to ensure that it is locally supported, sustainable and functions well for all.

It has been drafted to accord with national planning policies and guidance, including the National Model Design Code.

It is underpinned by an overarching Vision and Principles and more detailed Visions for each place within the Borough. The Design Code covers a range of Area Types that share similar design characteristics and issues. It places a particular focus on Areas of Change, identified by the [Local Plan](#), which are subject to more detailed Design Requirements.

The Design Code is based on wide-ranging inputs including that from the Spelthorne community, other stakeholders and a wider understanding of the places within the borough, to ensure it is locally-supported, robust and can be used in practice. The process has prioritised and been based around local engagement at every stage, including the use of an innovative Citizens Panel (a demographically representative group of Spelthorne residents), to ensure that the Code reflects and responds to community views and visions for the Borough.

The Design Code will be used to determine whether planning applications are acceptable in design terms and will support the emerging Spelthorne [Local Plan](#). It contains simple, concise, illustrated design requirements for streets, open spaces and buildings. It also sets out expectations for the process to be followed when proposals are designed.

The Code is not intended to stifle design creativity and the highest quality design, but to ensure that all development is Spelthorne demonstrates and delivers good design.

**Status – either SPD or Supplementary Plan – section to be completed once adoption route agreed. The aim for the Design Code is to be adopted as a Supplementary Plan, however this requires the requirements for adoption to be published by MHCLG, anticipated Spring 2025.**

## USERS

The Spelthorne Design Code is intended to be used by the following groups of users:

- Developers applying for planning permission and their design teams
- Planning officers and planning committee members assessing the suitability of proposed designs
- The wider community, seeking to understand what sort of development is supported in their local area

## HOW IT WILL BE USED TO DETERMINE PLANNING APPLICATIONS

Applicants for planning permission will need to demonstrate adherence to the Design Requirements in their proposals and planning applications. Whether a development meets the Design Code requirements will then be a material consideration in the determination of the planning application.

The Design Code has been produced in parallel with, and to support, the Spelthorne [Local Plan](#). Together, the [Local Plan](#) and Design Code, will support the delivery of high quality development and infrastructure in the most appropriate location.

## HOW WAS IT CREATED?

The Design Code was created through extensive community and stakeholder engagement, and in-depth urban design analysis of the existing borough and anticipated future development.

A full account of the process is set out in Appendix B.

# What does the Design Code include?

The Spelthorne Design Code sets out the design requirements for proposed new development in the borough. It will ensure that new development is locally supported, sustainable and functions well for all its users.

The Design Code will be used to determine whether planning applications are acceptable in design terms, and will support the emerging Spelthorne Local Plan. It contains simple, concise, illustrated design requirements for streets, open spaces and buildings. It also sets out expectations for the process to be followed when proposals are designed. It is based on wide-ranging input including that from the local community, other stakeholders and wider understanding of the places within the borough, to ensure it is locally-supported, robust and can be used in practice.

The Spelthorne Design Code includes:

- A vision for development in the borough and its key places
- Design principles across a range of topics
- Expectations for a comprehensive and considered approach to the design process
- Tailored design requirements for different area types within the borough, covering Buildings, Open Spaces, Streets and Public Realm, Landscape and other physical aspects of the design of proposals.

Other local policy documents deal with different areas of the built and natural environment in Spelthorne.

The [Local Plan](#) covers:

- The amount and location of development
- The delivery of supporting infrastructure
- Policies that deal with flooding, developer obligations, affordable housing and others

Surrey County Council's **Local Transport Plan 4** covers:

- Transport policies, schemes and other transport matters

Surrey County Council also publishes the **Healthy Streets Design Code**, which sets out the requirements for the design of streets and highways. Its key requirements have been included in this Code.

The Spelthorne **Local Cycling and Walking Infrastructure Plan** identifies networks and priorities for investment to support walking, cycling and other forms of active travel in the borough.

The Spelthorne Local Plan is supported by a number of Supplementary Planning Documents (SPDs), that provide further guidance on how to implement Local Plan policies. These include:

- Climate Change SPD
- Flooding SPD
- Housing Size and Type

## INFORMATION IN APPENDICES

The Design Code is supported by a series of Appendices:

### Appendix A: Understanding Spelthorne Today

This appendix sets out relevant background design information about the borough today, including:

- Historic Development
- Green and Blue Infrastructure
- Movement
- Built Form
- People & Places
- Future Development
- Detailed characterisation of Spelthorne's Area Types

### Appendix B: Community Engagement

This appendix sets out how the Code was created in collaboration with the community in Spelthorne.

### Appendix C: Residential Extensions Guidance

This appendix adds further information to the key dimensional guidance for residential extensions. It is drawn from the previous 'Design of Residential Extensions and New Residential Development' Supplementary Planning Document (SPD).

# How to use the Design Code

## FINDING THE INFORMATION YOU NEED

The Design Code is divided into five key chapters, plus this Introduction chapter. These are to be referred to at the different stages of preparing or assessing a design proposal as set out in the diagram on the following page.



This icon in the Code highlights areas and themes identified as particularly important to the community.



This icon in the Code highlights where you can **find out more** about a subject in supporting appendices.

This highlighted and underlined text indicates that this is a key term that is defined in the glossary and elsewhere in the document.

## AREA TYPES AND AREAS OF CHANGE

The Design Code sets out Design Requirements for developments in different Area Types in Chapter 4. They are denoted by **purple text** (e.g **TC-S1** for **Town Centre Neighbourhoods**, **Street requirement 1**), for ease of cross-reference.

In some parts of the borough, such as Staines-upon-Thames town centre, it is anticipated that there will be significant new development and change. These Areas of Change have additional detail and Design Requirements set out in Chapter 5.

## DESIGN REQUIREMENTS

Design Requirements are set out as follows:

**Aim: an explanation as to why this set of requirements is important, and what outcome should be achieved. This Aim is particularly important for non-standard design proposals that propose innovative and high-quality approaches to achieve the same outcome (see 'Comply or Justify').**

Requirements are then arranged as follows:

- **Must:** all proposals must comply
- **Should:** all proposals should comply unless non-compliance can be justified, and demonstrating compliance will add supporting weight to the design element of the planning application decision

## ADVISORY DESIGN GUIDANCE

Some parts of the Code sets out guidance, best practice or design inspiration from elsewhere that could provide the basis for the development of design proposals. These are design ideas that development **could** implement, and are highlighted as such.

## COMPLY OR JUSTIFY

The Design Code is to be used following a principle of '**Comply or Justify**'. Deviation from requirements set out will only be permitted with robust and evidence-based justification that any proposed design solutions still achieve the underlying Aim of the requirement.

Deviation from '**must**' requirements will require a very high level of justification.


Proposals that do not comply with these principles and fail to provide compelling justification are likely to be refused

## STAGE


## WHERE TO LOOK

**PROCESS** What is a good design process? .....▶

Learn about how design is considered and assessed in the planning system.  
Understand how to approach the design process in a way that will achieve good outcomes and Design Code compliance.


**CHAPTER 2**  
**The Design Process****CONTEXT** Where is the site? .....▶


Find out about the places that the site falls within, and learn about its history, present and future vision.  
Consider the community's design principles and objectives that new development should achieve.


**CHAPTER 3**  
**Places Past,  
Present & Future****REQUIREMENTS** What are the design requirements? .....▶

Find the Area Type that the site sits within, which sets the Design Requirements for the development.


**CHAPTER 4**  
**Area Type Design  
Requirements**

Some parts of the borough are designated 'Areas of Change' and have additional .....▶  
design requirements as well as their general Area Type Requirements.


**CHAPTER 5**  
**Areas of Change****SUBMISSION** What needs to be submitted with the planning application? .....▶

Learn about what needs to be submitted with a planning application to show Code compliance.  
Find out how Spelthorne Borough Council can work with you to achieve well-designed development.

**CHAPTER 6**  
**Preparing your  
Application**



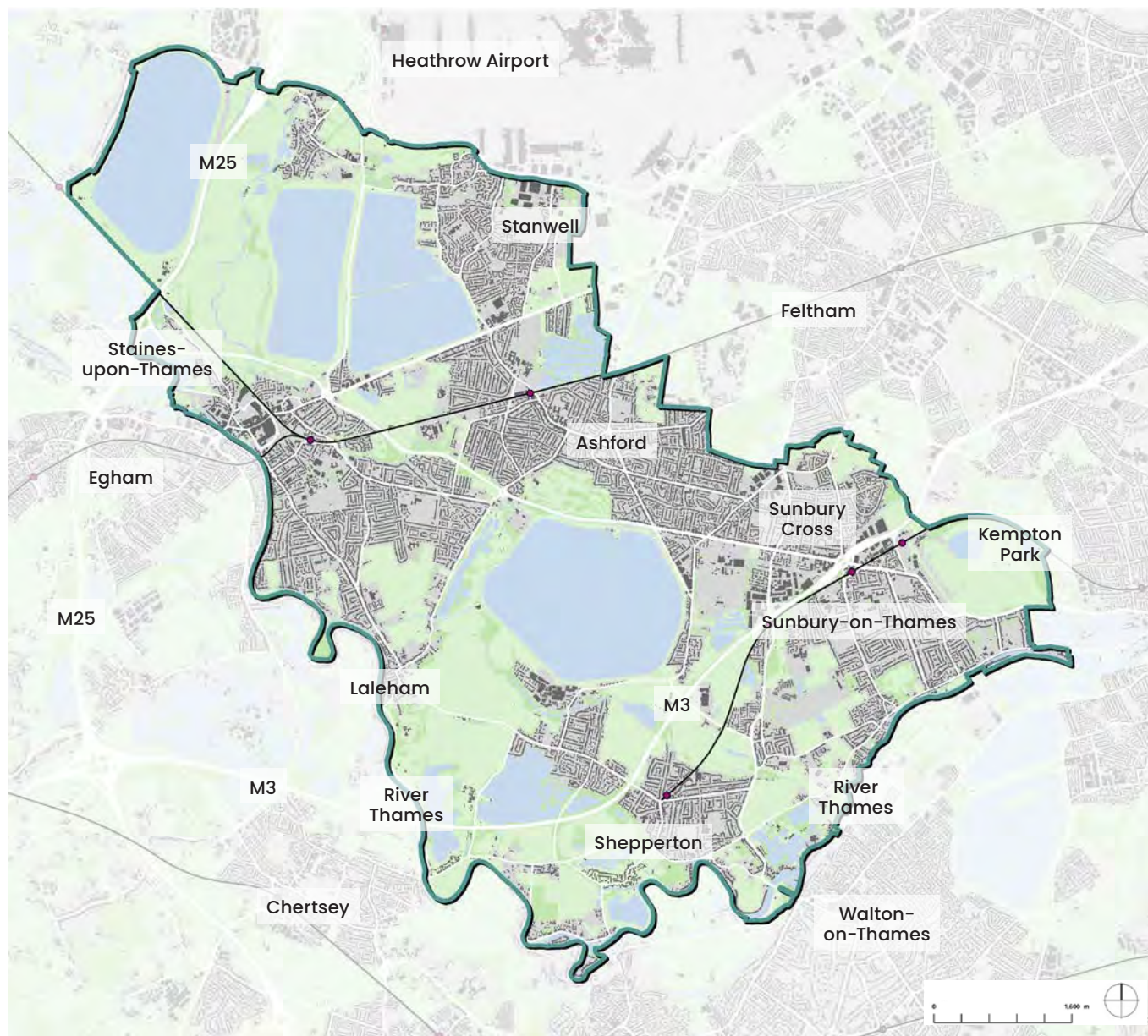
# About Spelthorne

**Spelthorne is a borough in the north of Surrey, on the fringes of London, with a population of around 100,000 people. It sits on the northern banks of the River Thames, and to the south of Heathrow Airport.**

Until the late 19th century, the area was a predominantly rural part of the former county of Middlesex, with Staines as the main market town. The arrival of the railway and growth of London sparked several waves of suburban growth, transforming the existing towns and villages into the suburban fringes of London, balancing the attractiveness of living near a city with the green open spaces of the countryside, common to much of 'urban' Surrey today. Since the creation of the Metropolitan Green Belt in the early 1970s, the built-up area has changed very little. The borough's built character is very strongly related to its historic development patterns.

The borough hosts major infrastructure, particularly reservoirs, water supply and motorways that support London and the wider south-east. Kempton Park, Shepperton Studios and BP's offices in Sunbury are significant landmarks and destinations.

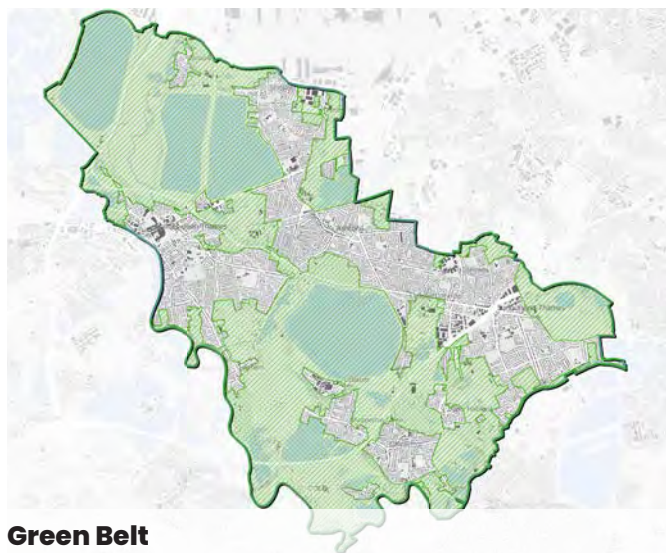
Spelthorne is likely to see significant residential-led development in the future, particularly in its well-connected town centres. Well-designed development offers a significant opportunity to create new, integrated and valued places and neighbourhoods that could provide benefit to new and existing communities.



**Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.

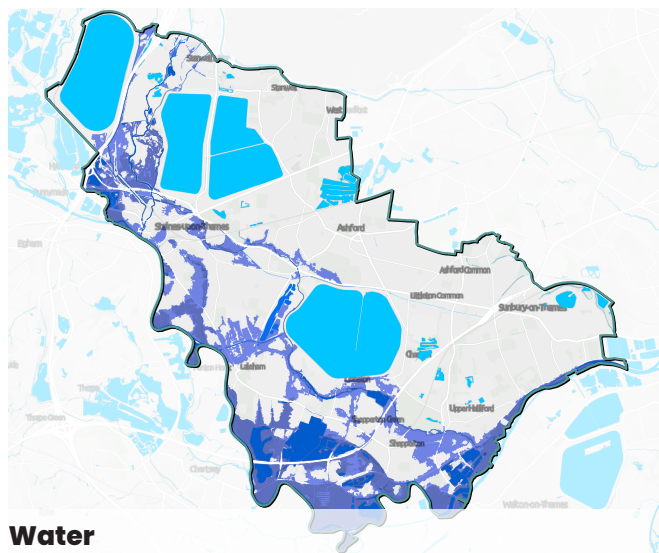


## Key features of Spelthorne



### Green Belt

Much of the borough is covered by the Metropolitan Green Belt, placing a strong focus on development in existing built up areas.



### Water

Bounded by the River Thames to the south, Spelthorne hosts major reservoirs and water supply infrastructure for the wider London and southeast, as well needing to manage flood risks.



### Urban Morphology

Many parts of Spelthorne bear the hallmark of the era in which they were developed, with strong patterns of regular plots, straight streets and suburban development.



### Attractive green open spaces

Green open spaces are much valued by the community and there are a wide variety, from parks to spaces for nature.



### The River Thames

The River Thames provides an opportunity for leisure, exercise and breathing space, but in places the frontage is underused and could be improved.



### Busy High Streets

Spelthorne's places are focused on bustling and vibrant high streets that provide local distinctiveness and valued retail and community provision.

# The Design Vision & Borough-Wide Principles

The Design Code has drawn on the views of the local community to define what good design is in Spelthorne, and the vision for how places should look, function and engage the community (both present and future) in coming years. The vision is set out across five themes, with supporting design principles to help make it happen.

## BOROUGH-WIDE DESIGN VISION

All development in Spelthorne will contribute to achieving the vision for future design of places in the borough.

## BOROUGH-WIDE DESIGN PRINCIPLES

All proposals for new development in the borough **must** apply the following design principles.

**WHERE DID THIS COME FROM?**

The Vision for the future and Principles for change were developed by the Spelthorne Design Code Citizens Panel and wider community through the engagement process.



## Sustainable Urban Design

Blend modern infrastructure and development with heritage through sustainable, high-quality and timeless architecture and design, reflecting the borough’s historic identity on the edges of both city and countryside.

- Use the Design Code and your own studies to understand what is important to conserve, what new development can learn from the past, and what the priorities for change are in the local area, before considering how to address these in your design proposals.
- Design for longevity, adaptability, ease of maintenance and to make a long-term contribution to the places of Spelthorne.
- Reflect key characteristics such as building grain, roofscapes, detailing and building lines, and avoid abrupt changes in character without a clear transition between existing and new.



## Commitment to Green Space

Protect, maintain and rejuvenate green spaces, with a focus on the importance of integrating natural areas into urban environments for residents’ well-being and improved biodiversity.

- Spelthorne’s historic development is strongly tied to the desire healthy urban living, being connected to both city and nature. New development should continue to enhance this approach, with usable, accessible and welcoming green open spaces.
- Make connections to the rivers of Spelthorne for both people and nature, and provide a range of green open spaces for new and existing residents to improve provision for all.
- Respect and retain riverside settings that provide amenity, placemaking and functional benefits.
- Make streets green spaces with trees and planting to provide shade and access to nature.





### Connectivity

Enhance access to and the quality of public transport links, and improve the quality and safety of routes for pedestrians and cyclists.



### Strong, Mixed Communities

Create inclusive places and spaces that cater to all, using design to physically and socially unite existing and new communities.



### Climate Change Resilience

Mitigate the impact of development and adapt to varied risks that may be worsened by climate change, through thoughtful design and natural solutions, that can also enhance the quality of open spaces for people and nature.

- Create streets and enhance existing streets that reduce car dominance and prioritise active travel movement, particularly major arterial roads and town centre roads.
- Include supporting facilities such as cycle hubs, cycle parking, seating, water refill points
- Daily uses should be within walking distance of all homes, and all uses designed so that they can co-exist with each other, especially in Spelthorne's town centres.
- Encourage the use of riversides for walking, cycling, leisure and recreation
- Create connections between existing and new neighbourhoods

- Create healthy spaces for people, that encourage the development of a community and a range of social interaction.
- New development should feel part of the surrounding area, and encourage social interaction
- Homes and buildings should be adaptable for the future, and reflect the diversity of living needs of Spelthorne's existing and new communities. They should be practical, with enough storage and outdoor amenity space for modern living.
- Ensure all public space is safe, comfortable and secure for all.

- Reuse and refurbish existing buildings first where this will reduce lifetime carbon emissions
- Use natural and sustainable design solutions to manage increased intensity of surface water flooding events
- Seek betterment for surrounding areas where possible through the replacement of existing impermeable surfaces with more permeable materials and planting
- Design drainage features that can be managed and maintained over the long term
- Use a diverse and robust mix of native species in planting that can withstand changes in climatic conditions



# The Design Process

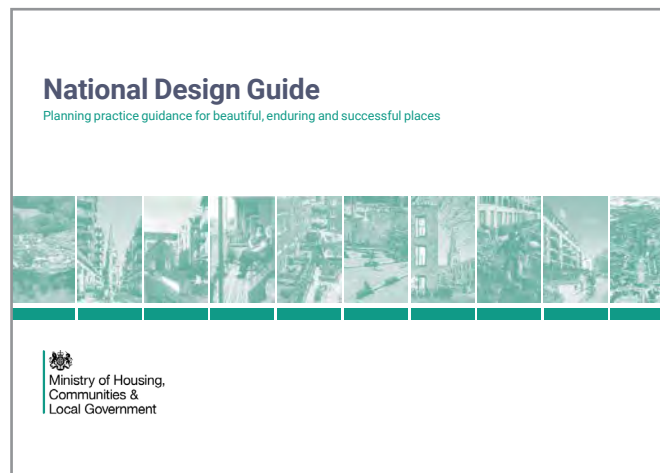


# The Design Process

## The Role of Design in the Planning Process

The National Planning Policy Framework (NPPF) sets out the importance of design within the planning process to achieving the goal of high quality, beautiful and sustainable buildings and places. Key to this is a common understanding of design expectations, with design guides or design codes being a vital tool that all local planning authorities should prepare.

Design within the planning process is considered within the framework established by the National Design Guide (2019).



Locally-specific design codes, prepared through a process of effective community engagement such as the Spelthorne Design Code, should take into account the guidance set out in the National Model Design Code, which is based on the framework of the ten characteristics established in the National Design Guide.

The Spelthorne Design Code has been prepared in line with the National Model Design Code's guidance, and makes reference throughout to the ten characteristics of well-designed places.

Design coding is one tool available to ensure high design quality. The NPPF recognises the importance of other tools and methods available to support a good design process. Early engagement between applicants, the community and the local authority is supported and will lead to more successful design outcomes.

## Approach

No two sites or proposals are the same, and the design process for each will vary depending on circumstances. Larger, more complex sites with more ambitious proposals will require more design thinking than smaller, simpler proposals.

**Design is an iterative process** – you may not get the right answer on the first try! There may be multiple ways of addressing an issue or opportunity, and many different competing requirements by different stakeholders. Use of community engagement and the council's pre-application services are encouraged to help you find the optimum solution.

However, there are a number of guiding principles to an effective design process within the planning system, that should be followed by all applicants. This section sets out those key principles. The following section on Key Steps sets out how to practically undertake an effective process, and how to integrate it within the planning application process.

All designs should be inspired first by **learning about the place and its wider context**. The Design Code sets out key information about each of the places and area types within the borough for context, and the 'Understanding Spelthorne Today' appendix provides a further, deeper understanding of place. This information should be supplemented by site visits, research, community engagement and analysis.

Think about **who you are designing for** to ensure that places are inclusive and work well for all. Different social and ethnic groups, genders, ages, abilities and other characteristics all use and experience the built environment in different ways. This can be particularly important in ensuring that places feel safe, secure and welcoming to all.

Appropriate engagement should run throughout the process, with technical stakeholders, the community and the local authority. The **right engagement at the right time** can inform and influence design proposals to be better and widely supported by all stakeholders. Good engagement should seek to build consensus and help applicants to learn from the existing and potential new community that they seek to serve.

Be **opportunity-led and positive** with proposals. Mitigating any potential harm is important, but needs to be balanced against maximising the opportunity present on a site.

**Environmental sustainability** should be woven throughout all design, with the expectation that development in Spelthorne is of high standards in climate change mitigation and adaptation, sustainable water management, green infrastructure and biodiversity. The Climate Change Supplementary Planning Document which sets out design approaches and a design checklist.

A strong design brief and process and starts with testing whether the **reuse and refurbishment of existing assets** is feasible. Re-use of existing buildings can both reduce embodied carbon emissions from construction of new buildings, but also retain the existing character and heritage of a place.

The built environment can have a significant impact on the **health and wellbeing** of its users. The creation of places that can help people to live physically active and mentally stimulating lives is of vital importance. This includes ensuring physical accessibility for all ages, prioritising active travel, making homes and buildings adaptable for all stages of life, and considering how neurodivergence needs can affect people using the public realm. It also extends to mental wellbeing: ensuring that people can meet, interact, and also relax in safety and comfort.

Often, the **details matter**, even at an early stage of design. This can be particularly important if there are key technical constraints that need to be overcome to deliver a scheme, which should be tested early in the process. It can also be important in engaging the community, where what is important to them may be quite specific.

All open spaces should have a clear and well-defined use carried through their design, which should be set out in the design proposal.

**Landscape design input should be integrated** into the overall design process for a site, and should be able to influence the built form as it relates to open spaces. Landscape and open space design, when considered as a holistic part of the design of schemes, can have a significant impact on the quality and success of new development.

When considering details and **architectural style**, this can take a number of forms that may be appropriate to the context. The Design Code sets out key parameters for different area types but does not prescribe architectural styles, which should be considered carefully by applicants and design teams, as it may be an area of particular interest to the local community. The architectural style and language chosen should be applied consistently. Considerations for different architectural approaches are set out in the diagram to the right. It is also possible, with a degree of design sophistication and subtlety, to blend different approaches in a transitional approach where this suits the context, picking up and re-interpreting key vernacular or traditional contextual characteristics in a modern way.

Design teams should anticipate what aspects of their proposals will need ongoing **stewardship and management**. This may be as simple as ensuring that there is accessible and sufficient storage for facilities management, or designing to ensure that highways, drainage and open space can be adopted by the local authority, through to working with wider teams to ensure long-term management financial arrangements are put in place for buildings and spaces.

Explaining your proposals to stakeholders, the community and as part of your application can make a huge difference in how they are received, and also in reaching clarity in design thinking. A wide range of **graphical communication techniques** are available which should be employed at various stages. Hand-drawn sketches can be helpful early on in exploring ideas, before resolving to detailed plans and computer-generated visualisations.



A scale of potential architectural design approaches with examples showing different applications of the approach to recent development in England.



## Key Steps

An effective design process should demonstrate (as part of its submission materials within the Design & Access Statement) that it has undertaken the following key steps. They should be undertaken and supported with studies at an appropriate level of detail for the scale and context of the proposal.

### Step 0: Setting the brief and appointing the team

The most important step in a good design process is the creation of a flexible, design-led project brief that responds to all planning policy, national legislation, the site and its context. This should set key parameters and expectations but also allow flexibility for change once design teams have had a chance to assess the site for capacity and potential.

As part of setting a brief, on larger proposals applicants will need to put together a specialist team appropriate to the project, with a strong design background, and all skills involved from an early stage. A co-ordinating lead will need to be put in place to ensure that trade-offs and decisions between different priorities are handled consistently and in line with the project brief. All disciplines should be involved in regular multi-disciplinary sessions to ensure that technical and other inputs into the design process are heard and considered throughout the process.



Example of a site analysis plan highlighting the most important spatial features of a site and its surrounding context. These key features should influence the resulting design.

### Step 1: Understanding the site and context

A full understanding of the site from a range of perspectives is vital for developing high-quality proposals that respond properly to the context and needs of the site. This should include as a minimum an understanding of the site and local context in relation to the following themes:

Environmental and Physical Constraints:

- Water and flood issues, including surface water, fluvial flood risk and groundwater issues
- Existing Utilities
- Protected Habitats and Ecology
- Existing Green Infrastructure
- Noise, Air Quality, Contamination

Heritage, Context and Placemaking:

- Heritage Assets
- Built form and urban typologies
- Heights, floor area ratios, grain and key dimensions
- Historic mapping and street patterns
- Land uses
- Connectivity and Mobility:
- Active travel and public transport connectivity
- Street hierarchy

As part of this understanding and analysis process, community engagement is a vital tool to learn more about a place, its context and local ambitions or priorities. This can take a number of forms from informal meetings, to drop-in events and co-design workshops.

Designers are encouraged to look beyond the boundary of the site and consider how their proposals will fit within the wider context. This may help inform where key uses, streets, open spaces and built form are located more effectively than looking at a site in isolation. Drawings will be expected to include an appropriate level of contextual information on them.

## Step 2: The Vision

A clear vision of what the future development will be is a vital tool to keep projects on track and delivering on their promise. This could include a vision of character, function and what it might do for the existing and new community. Effective design visions are often backed up by a coherent narrative and effective, engaging concept diagrams, making reference to the surrounding context.

Community engagement to co-develop and test visions can be an important part of this step, establishing a shared approach to a site from the outset, in line with the Design Code.

## Step 3: Developing and Testing Options

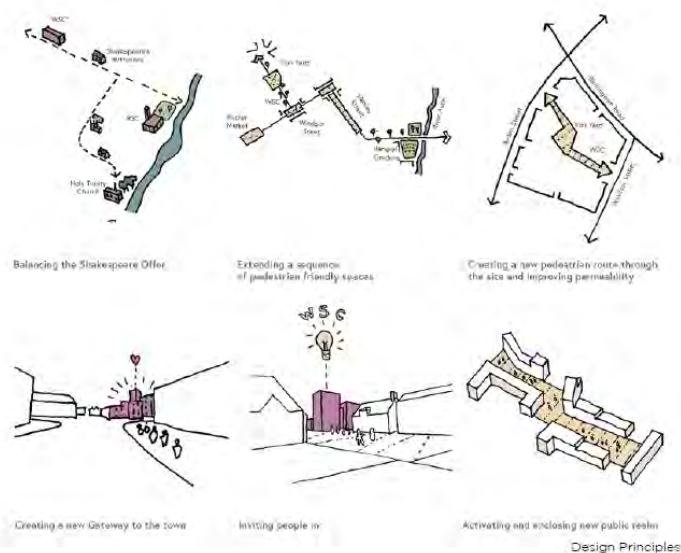
Design options to achieve the design vision should be prepared and explored iteratively. This will typically initially focus on massing, location of key uses, broad spatial arrangement of open spaces and relationships with surrounding areas.

Options testing through community engagement and with technical stakeholders can be a valuable part of the design process, helping everyone become involved before decisions are fixed.

Options should be appraised against the design vision, the Design Code, planning policy, their ability to achieve site opportunities, priorities learnt from community engagement and the wider brief. Engagement with the local authority through the pre-application process is encouraged.



Example options testing for a site, exploring different approaches to retaining/replacing buildings and the resulting layout changes needed.



Example of concept diagrams showing clearly the key structuring elements that drive the design, and why



### Step 4: Site Parameters

Once a preferred option is chosen, key site parameters such as the built form envelope, access, green infrastructure and open spaces should be established and communicated through the design team, to form a basis for further work and design development. These parameters could be agreed with the council through the pre-application process, or even through an outline planning application. A site-specific Design Code may need to be prepared to guide future design teams.

Further community engagement at this stage can explain why and how this option has been chosen, and how community involvement has helped to influence this.

### Step 5: Resolving the Details

For a full or reserved matters application, design teams will then begin to resolve details such as façades, materials, detailed landscape and public realm proposals and other matters. These should be within the parameters established earlier, especially if those parameters have been agreed as part of an outline planning application or other method.

At this stage more complete visualisations may help the community to understand a scheme, its materials, architectural treatments and façades, and landscape proposals.



**Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.



Example storey heights parameter plan



Example visualisation for a residential street. clearly demonstrating proposed character and use





3



# Places Past, Present and Future

# Staines-upon-Thames

Staines-upon-Thames is the largest town of the borough, a market town on the River Thames in the northwest of Spelthorne. Historically known simply as Staines (being renamed in 2012), the town is the largest in Spelthorne with the largest shopping area, key facilities and a growing population.



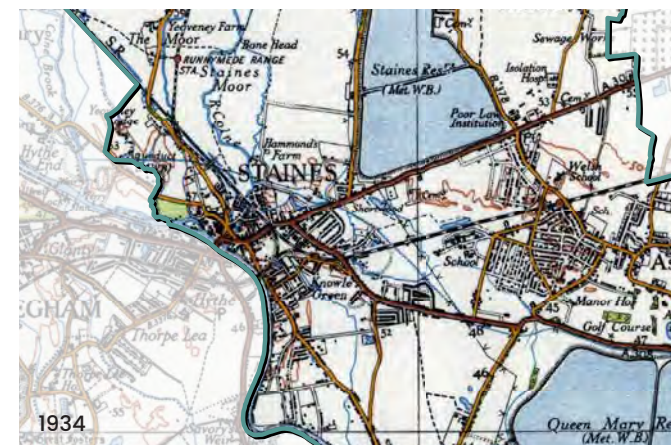
## PAST

The location of Staines is likely to have originated from the position of a Roman bridge across the Thames. The earliest records of Staines as a settlement are from the town's first market, held in 1218. Construction of the current Staines bridge was completed in 1832 representing the first major development since medieval times; substantially changing the town's street pattern to accommodate the new bridge location.

The arrival of the railway in 1848 stimulated growth of the town, including residential development along London Road and Kingston Road. In 1864, the Hale Mill linoleum factory opened, becoming a key economic driver to the town and occupying up to 20 ha of land at its height in the 1920s.

The town grew southeast in the early to mid-20th century, with widespread construction of suburban semi-detached housing which remains today. Some of the post-WWII housing was built specifically to accommodate Heathrow Airport workers, as the airport rapidly expanded.

Later 20th century development was increasingly car-oriented, and infrastructure projects included construction of the A30 bypass in the 1960s. The Elmsleigh Shopping Centre opened in 1980, along with a multi-storey car park. The closure of the Hale Mills linoleum plant in 1973 opened up availability of this site which was redeveloped in the late 1990s to become the Two Rivers Shopping Centre, along with large swathes of surface-level car parking.





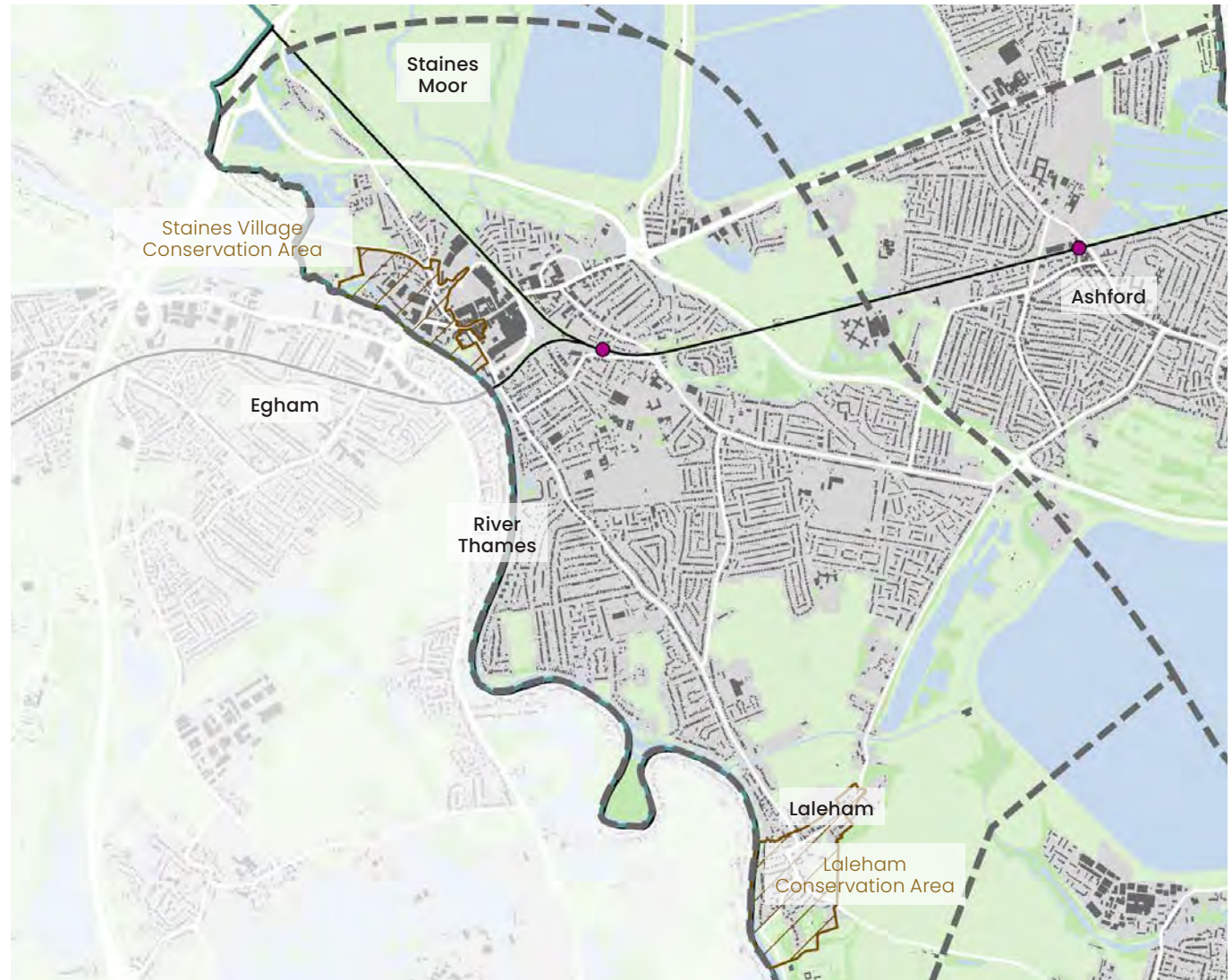
## PRESENT

Today's urban form of Staines town centre is heavily influenced by 20th century car-oriented development, resulting in large block forms comprising shopping centres, office blocks, logistics and warehousing uses. The four-lane A308 road (Clarence Street / Thames Street) adds to the centre's car dominance, causing severance between the High Street and the Market Square. In contrast to these modern additions to the town, the High Street contains a many smaller older buildings, providing a more traditional town centre character. The High Street has also been pedestrianised, improving the pedestrian experience.

Notable buildings in the town include the Renaissance style Town Hall built in 1880. Also, a large vacant department store building (formerly Debenhams) is located on the corner of the High Street and Thames Street (A308). Twenty-first century development includes the two towers (15 and 13 stories) currently being constructed on the former Masonic Hall and Telephone Exchange sites, representing some of the highest site densities in the borough to date.

In contrast, Church Street (west of the centre) has retained a distinct 'village' character despite its close proximity to 20th and 21st century developments. A fine urban grain, mixture of building types, and proximity to the Grade II\* listed St Marys Church and cemetery provide a rural village feel.

A large part of Staines is protected through Conservation Area status, extending along the bank of the Thames to include St Mary's Church in the northeast, Church Street, Bridge Street, and Clarence Street. This covers the Market Square (with town hall), the entrance to the High Street, and the (currently vacant) department store building on Thames Street.



The remainder of Staines comprises largely of suburban dwellings from a range of eras; from typical 1930s semi's through to more modern, 1990s cluster-style residential layouts. The predominant housing layout in the older, southeastern part of Staines is typically regular and linear.



**Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.

## The Design Vision & Principles for Change

**Staines-upon-Thames** will be an inclusive, well-connected urban centre with improved riverside access, better design, and flood mitigation. Key priorities include balancing heritage, enhancing connectivity, and transforming the area into a modern, safe town with green spaces and a public riverfront.

### DESIGN PRINCIPLES



#### Sustainable Urban Design

- Conserve the street-level and townscape experience of areas with strong place identity
- Create new town centre neighbourhoods that are integrated with their surroundings and improve the townscape of the area



#### Commitment to Green Space

- Improve connections to the rivers, physically and visually, with improved safety and quality of spaces adjacent to the Thames
- Create new urban public open spaces to enhance the town centre



#### Connectivity

- Create new walking and cycling connections through new town centre neighbourhoods
- Improve the safety and security of existing paths and cycle routes



#### Strong, Mixed Communities

- Development that integrates new residents into the existing community, through physical links and new shared infrastructure and facilities
- Improve safety in public spaces



#### Climate Change Resilience

- Improve surface water permeability by converting impermeable hard surfaces to softer, permeable and planted spaces
- New development must not worsen fluvial, surface water or groundwater flood risks

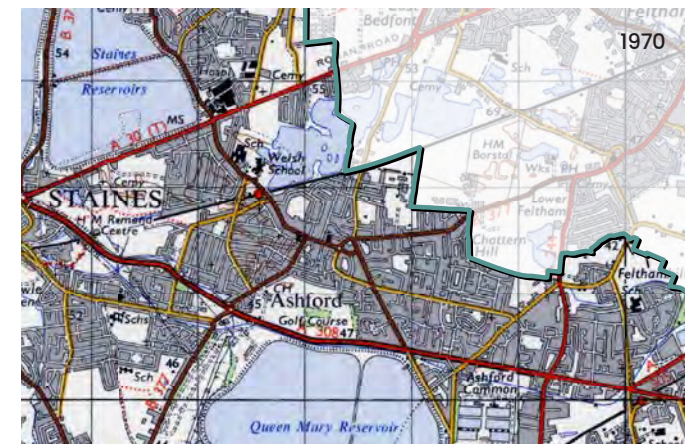
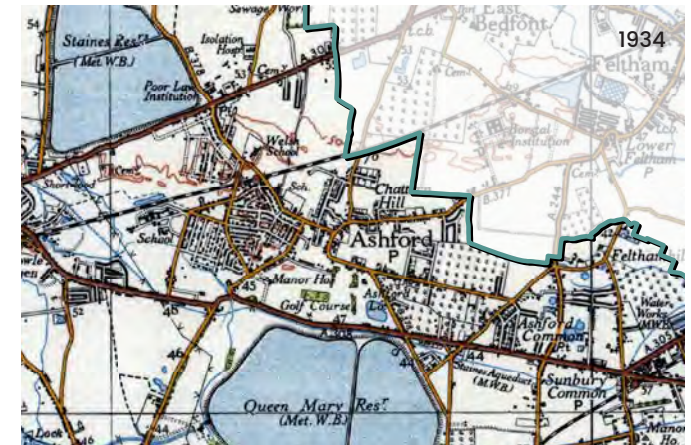
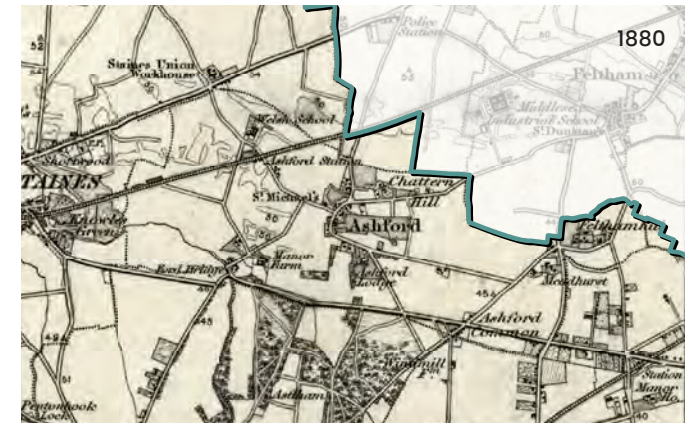
#### WHERE DID THIS COME FROM?

The Vision for the future and Principles for change were developed by the Spelthorne Design Code Citizens Panel and wider community through the engagement process.



# Ashford

Ashford is a large town located centrally within Spelthorne. The town is predominantly suburban with a high proportion of semi-detached homes. Ashford has a well-used high street (Church Road), a railway station, and several local / neighbourhood centres spread throughout the suburban area.



## PAST

Ashford was originally recorded as Exeфорde on the Middlesex Domesday map. The land was held by Robert, Count of Mortain, a half-brother to William the Conqueror.

The modern-day name of Ashford derives from a crossing point over the River Ash. In 1789, this crossing was upgraded to a stone bridge by the Hampton and Staines Turnpike Trust. Today, the river is located to the south of Ashford meandering north of Staines bypass and passing under Fordbridge Roundabout.

Before the 19th century, Ashford Common was a large area of common land found in the south and east of the town. This was used during the reign of King George III for British military displays. Public rights were removed from this land in 1809 through the Inclosure Act, and much of the land is now developed.

In 1902, Ashford Manor Golf Club was established within Ashford's manorial estate. In the same year, the construction of Staines Reservoir was completed. In 1924, construction of the Queen Mary Reservoir was completed. At the time, it was the largest reservoir in the world. The reservoir was used to test submersibles during World War II.



**PRESENT**

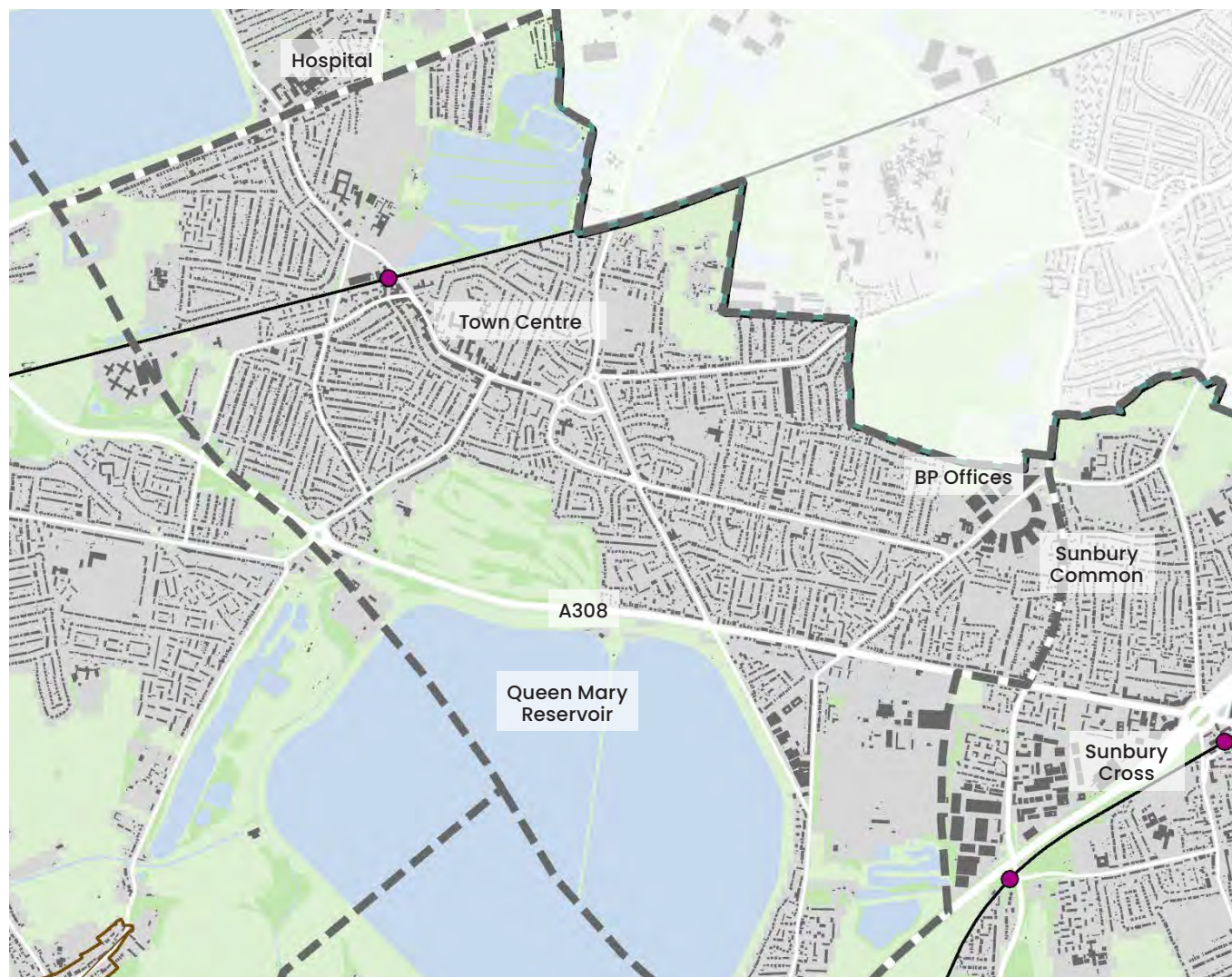
Ashford's land use is largely suburban residential. The predominant housing type is low-rise detached and semi-detached; mainly built between 1885 and 1960. A recent development north of Church Road introduces some higher densities with apartment blocks up to five stories high.

The town centre is predominantly linear in form along Church Road, extending east-west from St Matthew's Parish Church to Ashford railway station. The centre includes a wide range of shops and services including several convenience stores, takeaways, coffee shops, hairdressers, health & beauty salons, a library and a bank.

Ashford includes eight primary schools and two secondary schools. Ashford Hospital is located northwest of the A30 London Road, providing mostly day surgical and outpatients services. The prison HMP Bronzefield is also located on the edge of Ashford. This is the largest female prison in Europe.

Ashford includes several churches, including CofE churches St Matthew's (Church Road) and St Hilda's (Woodthorpe Road); and Roman Catholic church St Michael's (Fordbridge Road). The latter was designed by Sir Giles Gilbert Scott in a distinctive Romanesque Revival style, built in several stages between 1927 and 1960.

Access to green space in Ashford is limited to several relatively small green areas. These include Hengrove Park, Woodthorpe Road Play Area, Ashford Recreation Ground, and Feltham Hill Road Recreation Ground. Notably, Ashford Manor Golf Club constitutes a large proportion of the overall green space; however, this is not publicly accessible.



**Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.



## The Design Vision & Principles for Change

**Ashford** will be a safe, community-focused area with green spaces, a revitalised High Street, local parks, and minimal high-rise development. Community feedback emphasises keeping Ashford family-friendly with a focus on youth and vibrant public spaces.

### DESIGN PRINCIPLES



#### Sustainable Urban Design

- Prioritise apartment development close to public transport and main streets
- A wide mix of different types of homes in new development, that integrate well and are inspired by the existing character of the town



#### Commitment to Green Space

- Trees, planting and street greening throughout the High Street, major roads and all public realm



#### Connectivity

- Improve walking and cycling space, especially around the High Street and station
- Successful car parking arrangements that do not add to the car dominance of streets



#### Strong, Mixed Communities

- Include space for local independent businesses as a core part of the appeal and vibrancy of the town centre
- Improve safety in public spaces



#### Climate Change Resilience

- Improve surface water permeability by converting impermeable hard surfaces to softer, permeable and planted spaces
- Create more shade and cooling in streets and open spaces

#### WHERE DID THIS COME FROM?

The Vision for the future and Principles for change were developed by the Spelthorne Design Code Citizens Panel and wider community through the engagement process.

# Sunbury-on-Thames

Sunbury-on-Thames is located in the east of Spelthorne Borough. It has a number of distinct areas including Lower Sunbury, Sunbury Common and the Sunbury Cross shopping centre. Sunbury is well provisioned with open green spaces, schools, shops and services. Sunbury train station provides direct services to central London. The town is divided by the M3 motorway.



## PAST

The name 'Sunbury' has an unclear origin with variations of the name included in two Anglo-Saxon charters and the later Domesday Book of 1086.

An important early development was Sunbury Park located close to the River Thames, being the site of a Tudor Manor House built for a courtier of Elizabeth I. The area along the river became a gentrified area with many large properties built by wealthy residents. These included a group of Huguenot refugees, and it is possible that French Street is named after these settlers.

Sunbury was historically based around this area. To the north, Sunbury Cross is an historic intersection of five main roads, along with

scattered/linear development along Green Street connecting to the River Thames. Until the railway arrived in Sunbury in 1864, the wider area was mainly open fields and common land. Much development took place in the interwar and post-war periods, developing Sunbury Common into a suburb with a predominance of detached and semi-detached homes.

Significant change took place in Sunbury Cross during the 1970s with the construction of the M3 junction, and of several high-rise buildings and the Sunbury Cross shopping centre.



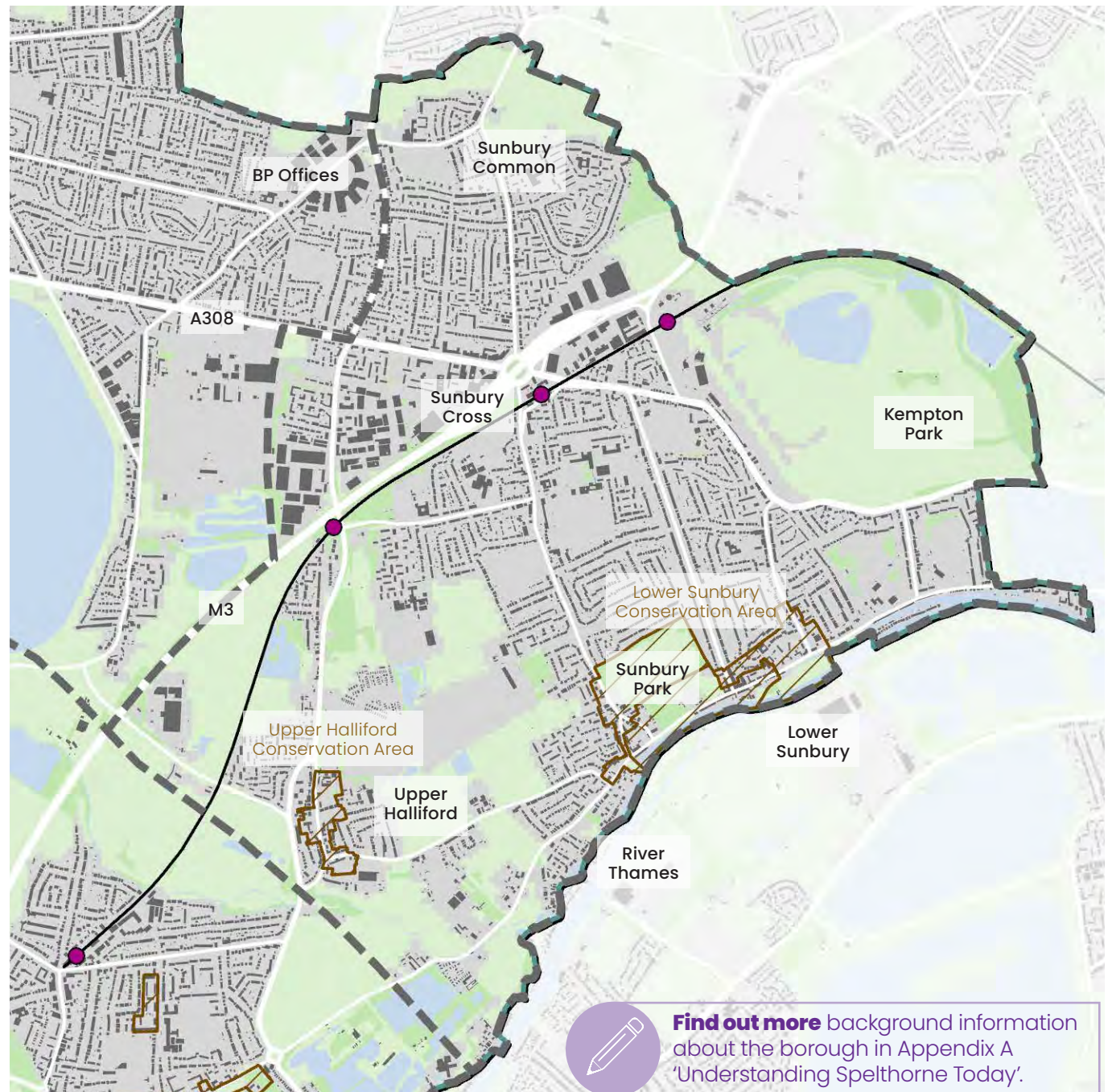
## PRESENT

Sunbury's land use is predominantly suburban residential, with some urban areas and industrial areas. To the northwest this includes the British Petroleum (BP) International Centre for Business & Technology, a research and office campus.

Sunbury Cross has a more urban development form with several high-rise buildings and a shopping centre. There are a range of shops, fast food outlets, health & beauty services, convenience stores, supermarkets, and a Premier Inn hotel. Community buildings include a library and church.

There are a range of green spaces across Sunbury including Medhurst Sports Ground, Groveley Road Recreation Ground, Kenyngton Manor Recreation Ground, Cedars Recreational Ground, Sunbury Park, and Lower Hampton Road Park. Sporting venues include the Gaflac Sports Ground, Kempton Cricket Club, Sunbury Cricket Club and Sunbury Sports Bowls Club. Adjacent to the east of Sunbury is also Kempton Park Racecourse, an 85-hectare site with equestrian racecourse involving adjoining inner and outer courses for flat and National Hunt racing.

Lower Sunbury, along the River Thames, has a contrasting 'village' feel compared to the wider area, with a range of historical properties, a finer urban grain, and adjacency to the historic Sunbury Park. Some of this area has Conservation Area status, incorporating much of Thames Street, Church Street, Sunbury Court, and part of French Street. While the manor house of Sunbury Park was demolished in post-war years, the parkland and walled gardens remain and add to the historic character of the area.



**Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.

# The Design Vision & Principles for Change

The different areas of **Sunbury-on-Thames** will maintain their distinctive and varied characters and a comfortable, well-designed environment with ample common spaces, reduced congestion and a welcoming atmosphere. Key priorities include improving accessibility for all, particularly those with reduced mobility. **Sunbury Cross** will become a safer, more human-scale place for residents and visitors, with reduced impact from vehicles, and reduced severance caused by infrastructure.

## DESIGN PRINCIPLES



### Sustainable Urban Design

- Prioritise apartment development close to public transport and main streets
- Built form that creates human-scale environments with improved safety
- Ensure sensitive intensification of existing suburban areas



### Commitment to Green Space

- Trees, planting and street greening throughout major roads and all public realm
- Reflect existing 'green' and verdant characters
- New well-maintained green spaces that are designed positively



### Connectivity

- Reduce severance caused by infrastructure
- Improve walking and cycling provision in streets and to the rail station
- Reduce the dominance of cars in the streets
- Enhance walking and cycling connections to, from and along the River Thames



### Strong, Mixed Communities

- Improve safety in public spaces
- Include a wide mix of homes, supported by community facilities



### Climate Change Resilience

- Improve surface water permeability by converting impermeable hard surfaces to softer, permeable and planted spaces
- Create more shade and cooling in streets and open spaces

#### WHERE DID THIS COME FROM?

The Vision for the future and Principles for change were developed by the Spelthorne Design Code Citizens Panel and wider community through the engagement process.



# Shepperton

The village of Shepperton is located to the south of the borough, characterised by a thriving high street and many attractive tree-lined residential streets. Shepperton can be divided into two distinct areas with the main built-up area to the north, and Old Shepperton to the south. The town includes various shops and cafés, as well as a railway station providing direct trains to London Waterloo.



## PAST

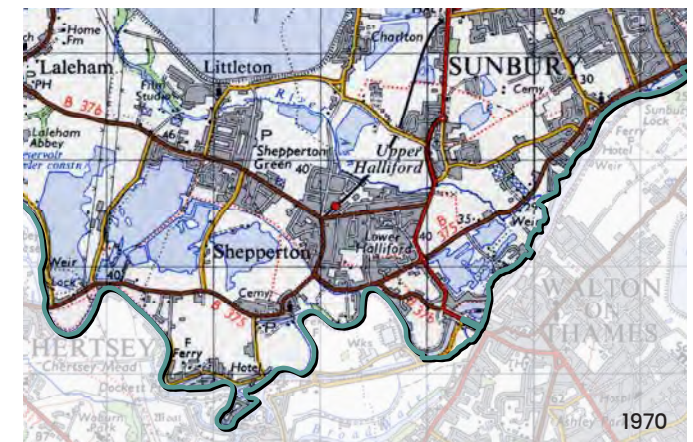
There is evidence of human activity in Shepperton since the middle-Neolithic period, from the discovery of a penannular ring ditch in the 1980s. In 1086, the Domesday Book recorded a population of 25 households in Shepperton (then referred to as 'Scepertone').

Shepperton is strategically located nearby the River Thames which has been a vital transport route since the late 13th century. Products including grain, vegetables, and building materials were transported by the river. To aid river navigation, Shepperton Lock and Sunbury Lock were built near Shepperton in the 1810s.

Shepperton originally developed as a settlement on the River Thames; the area known as Old

Shepperton today. Church Square in Old Shepperton served as the original settlement nucleus. The square has a range of historic buildings, most notably the St Nicholas Parish Church, built in 1614. Sir Nikolaus Pevsner described the square, with its glimpse of the River Thames, as "one of the most perfect village pictures that the area has to offer".

The construction of the Shepperton branch line in 1864 led to a new focus of development away from the existing village and 1 mile to the north where Shepperton station had been constructed. This led to the formation of Shepperton town, connecting south to Old Shepperton via the high street and Church Road.





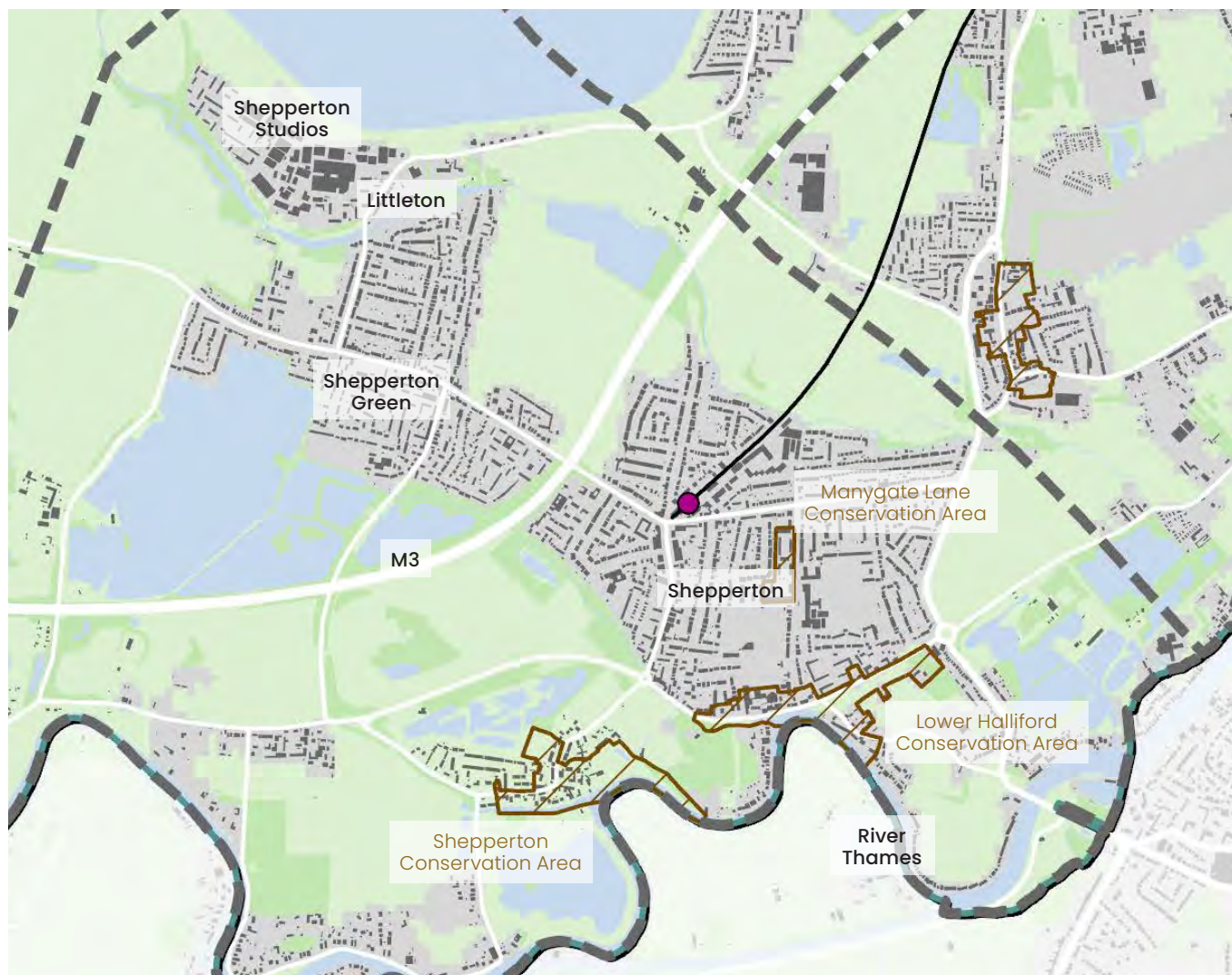
## PRESENT

Shepperton's high street is a thriving centre with a wide range of shops and cafés, including independent businesses. The high street is a wide road with parking roads either side. However, there is a good sense of enclosure owing to the mature trees which line the street. There is a large mix of land uses including housing, offices, a library and a church; adding to the vibrancy of the area.

The residential areas in Shepperton are suburban developments mostly constructed in a linear pattern. Houses are mostly detached and semi-detached, along tree-lined streets. Broadlands Avenue is an attractive area with generous plot sizes and large, detached houses.

Shepperton includes three conservation areas: Old Shepperton, Lower Halliford, and the Manygate Lane estate. Old Shepperton includes several historic buildings such as the parish church, two public houses, an 18th century riverside manor, and a Grade II\* listed timber framed Old Rectory building built c.1500. Lower Halliford includes several detached classical 18th century riverside houses, and the meadow along Russell Road. The Manygate Lane estate is a contrasting modernist development characterised by modular rectangular, white-painted houses from the mid-20th century.

There are several accessible green spaces in Shepperton, including Shepperton Recreation Ground and Manor Park. Unlike other areas in Spelthorne, Shepperton has close proximity to surrounding fields and rural areas, as well as Public Rights of Way providing a variety of walking routes.



**Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.

## The Design Vision & Principles for Change

**Shepperton** will preserve its village charm while embracing well-designed new development. The community supports a new square in the centre, better cycling infrastructure along the Thames, and a semi-pedestrianised High Street that retains independent shops while supporting sustainable growth and transport.

### DESIGN PRINCIPLES



#### Sustainable Urban Design

- Ensure sensitive intensification of existing suburban areas reflects the existing street scene and architecture



#### Connectivity

- Improve walking and cycling provision in streets and to the rail station
- Reduce the dominance of cars and highway infrastructure in the streets



#### Climate Change Resilience

- Improve surface water permeability by converting existing impermeable hard surfaces to softer, permeable and planted spaces



#### Commitment to Green Space

- Reflect the existing 'green' and verdant character of the place
- Include planting, seating and high quality materials throughout the public realm



#### Strong, Mixed Communities

- Improve safety in public spaces
- Include a wide mix of homes, supported by community facilities

#### WHERE DID THIS COME FROM?

The Vision for the future and Principles for change were developed by the Spelthorne Design Code Citizens Panel and wider community through the engagement process.



# Stanwell

Stanwell is a predominantly residential suburban area in the north of Spelthorne Borough, located east and northeast of the Staines Reservoirs. It is the northernmost settlement in Surrey. There is a small village centre to the north of Stanwell, with a village green, pub, church, and small range of shops and services. Stanwell is in close proximity to Heathrow Airport to the north.

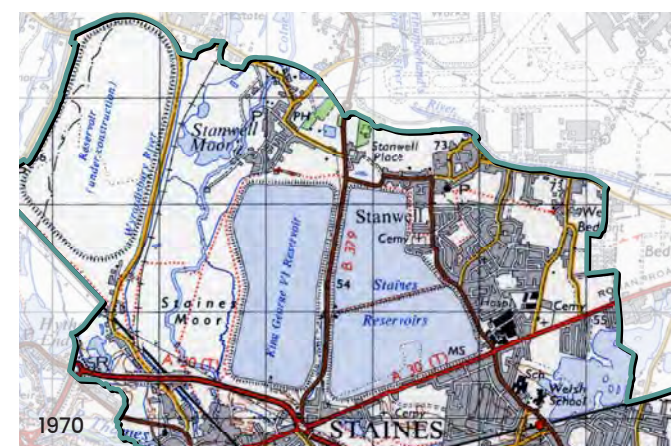
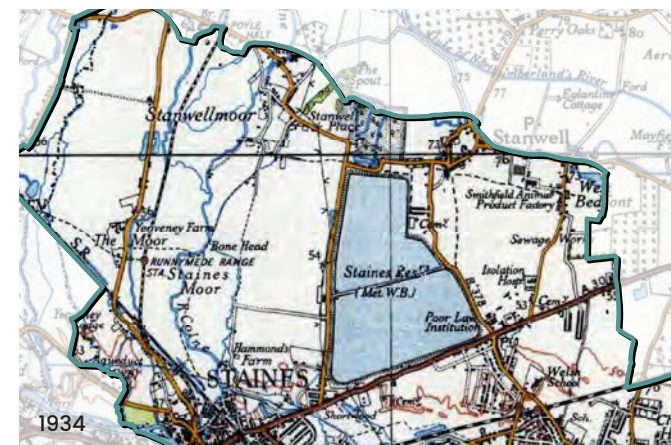


## PAST

The Domesday book of 1086 records 'Stanwelle', unusually retained by a man with an Anglo-Saxon name. In 1603, the manor of Stanwell was granted to Thomas Knyvet who helped to foil the attempt of Guy Fawkes to blow up the Houses of Parliament. Up until the 20th century the area was mainly farmland and moor land surrounding the village of Stanwell.

The extent of Stanwell was cut substantially during the 20th century with the construction of the Staines Reservoirs in 1902 and the development of Heathrow Airport (originally Harmondsworth Aerodrome) which became operational in 1930. Additionally, some land was taken west of Stanwell Moor for the construction of the M25 in the 1980s.

Large-scale construction of new homes began following World War II. Over 300 prefabricated houses were built between Town Lane and Long Lane between 1945 and 1948. Several smaller developments of terraced and semi-detached houses, including those either side of Park Road, were built since 1954 by the British Airways Staff Housing Society.





## PRESENT

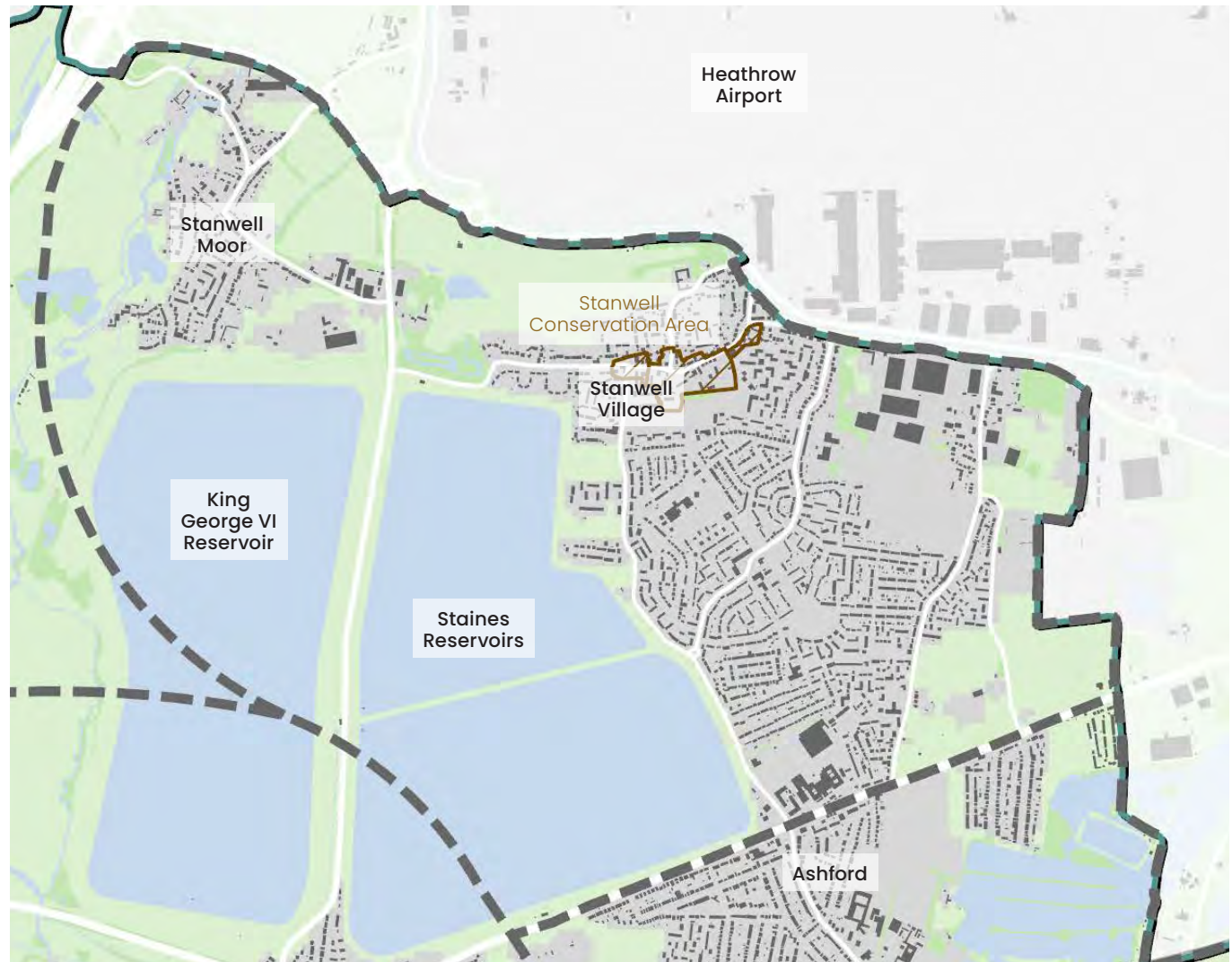
The suburban residential areas today comprise largely of semi-detached and terraced housing built between the 1950s to 1970s, with some early 21st century development. The urban form involves regular perimeter blocks with areas of green space distributed throughout the development.

The historic centre of Stanwell, a Conservation Area, retains a distinctive village feel from the surrounding 20th century suburban development. The urban form is nucleated, with St Mary the Virgin Church and the village serving as focal points. A variety of historic properties from different time periods adds to the village character. The centre benefits from several shops and services including a convenience store, public house, a range of takeaways and a florist.

The wider area of Stanwell also includes Stanwell Moor, a distinct settlement located north of King George VI Reservoir. Residential properties are predominantly from the mid-to-late 20th century. Stanwell Moor offers several facilities including a village hall, parade of shops, and public house.

There is a good provision of green space throughout Stanwell. This includes Village Park, Lauser Road Park, West Bedfont playing fields, Clyde Road Park, and Stanwell Moor playing fields.

While Stanwell is in close proximity to Heathrow Airport, there is limited access between the settlement and the airport due to the perimeter road and adjacent watercourses. In January 2025, the government invited Heathrow Airport to bring forward proposals for a third runway. While details of the proposed airport expansion are not yet known, these plans have the potential to impact the future character of Stanwell.



**Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.

## The Design Vision & Principles for Change

**Stanwell** will develop a clearer place identity and have better integration with the rest of Spelthorne. Development should focus on addressing infrastructure gaps, prioritise healthy placemaking and create more connected spaces, both green and built, to foster community cohesion and opportunity.

### DESIGN PRINCIPLES



#### Sustainable Urban Design

- Ensure that edges between different land uses successfully manage any impacts from one use to another
- Create places that have sufficient density to be vibrant, sustainable and safe



#### Commitment to Green Space

- Extend and enhance existing green spaces, with green corridors into new development
- Create new green spaces that can host community events and become places to meet, socialise and relax



#### Connectivity

- Enhance walking and cycling connections to existing streets and the wider context, including Heathrow and employment areas
- Improve the safety, security and attractiveness of existing links



#### Strong, Mixed Communities

- Prioritise health and wellbeing as a key design driver for new development
- Improve safety in public spaces
- Include a wide mix of homes, supported by community facilities



#### Climate Change Resilience

- Improve surface water permeability by converting impermeable hard surfaces to softer, permeable and planted spaces
- Create more shade and cooling in streets and open spaces

#### WHERE DID THIS COME FROM?

The Vision for the future and Principles for change were developed by the Spelthorne Design Code Citizens Panel and wider community through the engagement process.

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# Area Type Design Requirements



# Spelthorne's Area Types

Spelthorne has a number of different Area Types, which are distinctive from each other in urban design characteristics and their future patterns of development.

Different Area Types have different Design Requirements for future development that are appropriate to the area. Find the Area Type your proposal is in on the Area Types Plan to the right to see which requirements apply.

Designated 'Areas of Change' have more detailed coding requirements in addition to their Area Type Design Requirements. These are found in Chapter 5.

Most proposals in Spelthorne will be covered by the Code. **Other Area Types, and some development types, do not have detailed coding.** These design proposals should be in accordance with:

- Requirements for [Design Process](#) (Chapter 2)
- Design Code Vision and principles for the borough (Chapter 1)
- Design Code Vision and principles for the place (Chapter 3)
- Policy requirements as set out in the [Local Plan](#) and other valid policy or guidance

## CODED AREA TYPES



High Streets



Town Centre N'hoods



Inner Suburban



Suburban

## AREA TYPES NOT CODED



Village

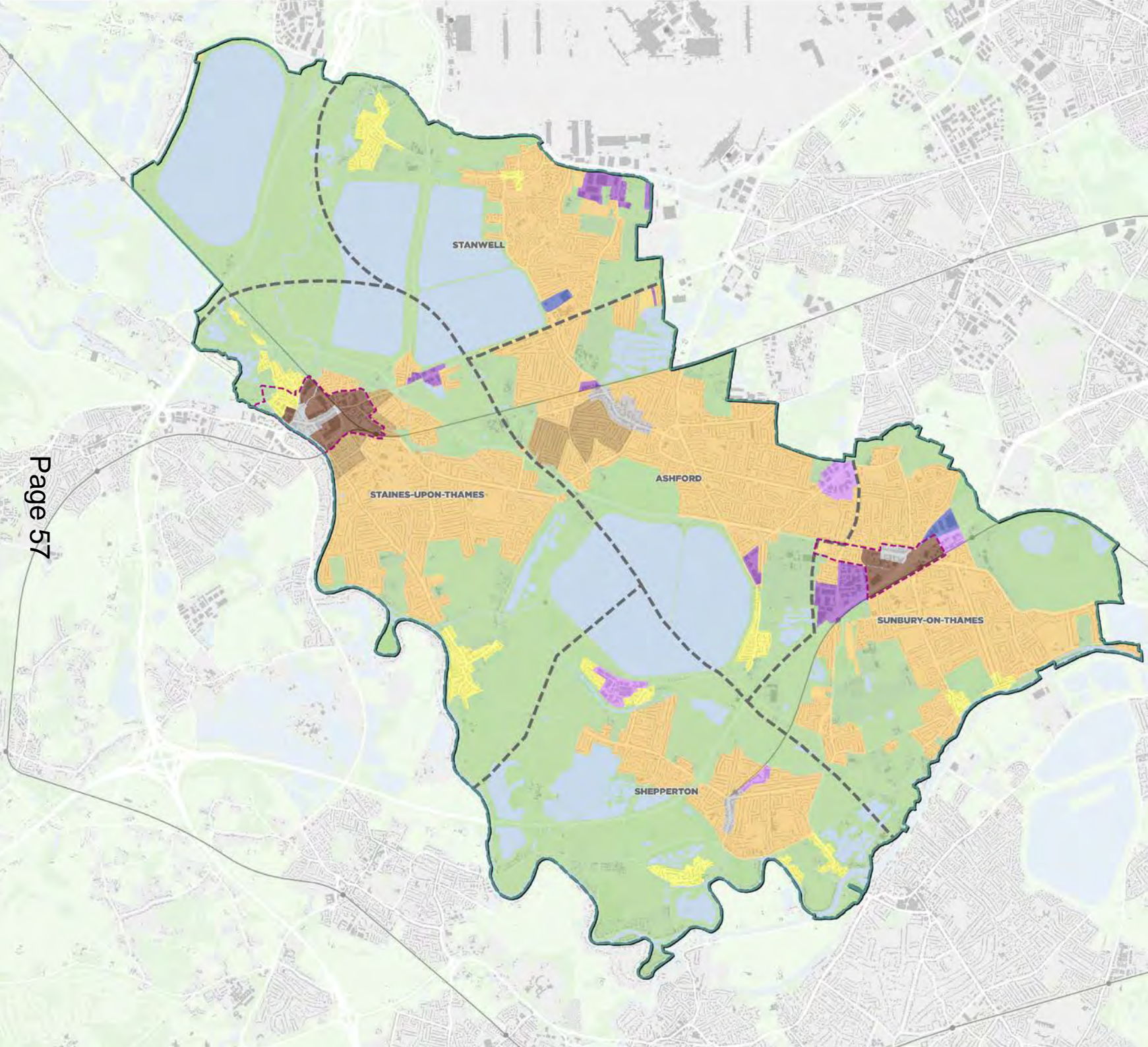


Employment



Retail Park





Places  
See Chapter 3



Areas of Change  
See Chapter 5

### CODED AREA TYPES



High Streets



Town Centre  
Neighbourhoods



Inner Suburban



Suburban

### AREA TYPES NOT CODED



Green Belt



Village



Business Park



Light Industrial



Retail Park



Waterbodies

# High Streets

## OVERVIEW

High Streets are the vibrant heart of Spelthorne, and are both functional places and a core part of the borough's place identity.

There are four identified High Streets within Spelthorne. They are distinct in character from each other but have a number of common features.

- Staines-upon-Thames
- Ashford
- Sunbury Cross
- Shepperton

Staines-upon-Thames has a thriving High Street which is pedestrianised along its core length. Others remain busy streets for vehicles as well as people.

Staines and Ashford are the most historic High Streets, well-developed by the end of the 19th Century. Shepperton and Sunbury Cross develop further in the Inter-War and post-War period.

## CODED DEVELOPMENT TYPES

All development along High Streets in Spelthorne is anticipated to be of the form of mixed use buildings, with retail or commercial ground floors and residential dwellings or office space on floors above.

## AREAS OF CHANGE

Staines-upon-Thames and Sunbury Cross High Streets are both parts of Areas of Change. As well as the requirements set out in this section, they are subject to further spatial coding requirements set out in Chapter 5.

Development in Ashford and Shepperton High Street is anticipated to be incremental and governed by the design requirements set out in this section.

## LOCATIONS

Locations of High Streets in Spelthorne are shown on the following page.

## DESIGN AIMS

Development in High Streets **will**:

- Be incremental in form, being guided by existing dimensions of height, width, set back and building line
- Include retail and commercial uses on the ground floor to ensure the continued vibrancy and importance of High Streets as key places in Spelthorne
- Support a transformation in the public realm to prioritise active travel
- Be attractively and thoughtfully detailed and articulated with appropriate materials to integrate visually with the context and surrounding place



Spelthorne's High Streets are distinctive and important to the community. Development must 'fit in'.

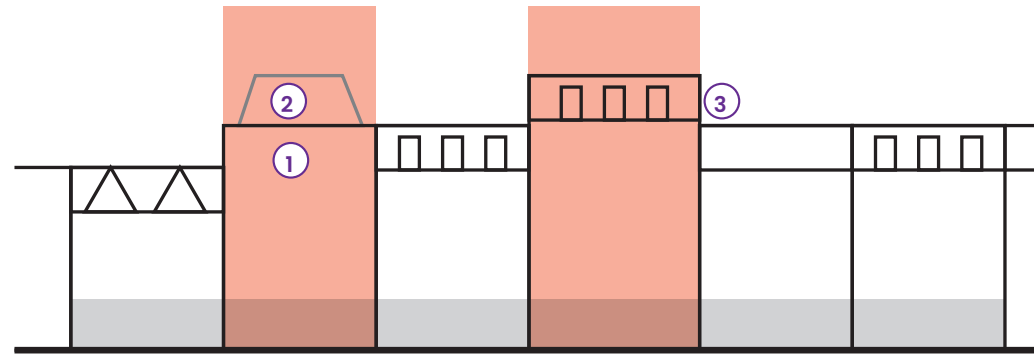




## HS-1 BUILDING HEIGHTS

Maximum heights of new development **must** comply with the requirements set out in the diagram to the right, and not exceed the maximum heights specified below.

- Staines-upon-Thames: 6 storeys (approx 18m). Further detail in Chapter 5, Areas of Change.
- Ashford: 5 storeys (approx 15m)
- Shepperton: 5 storeys (approx 15m)
- Sunbury Cross: 5 storeys (north side, approx 15m), 8 storeys (south side, approx 24m). Further detail in Chapter 5, Areas of Change.



Development between different heights may:

1. Have one storey higher than the lower adjacent building, up to the maximum heights specified.
2. Have one storey higher than the taller adjacent building, provided it is set back from the building line and the flank facing the lower adjacent building, and does not exceed the maximum heights specified.

Development between similar heights may:

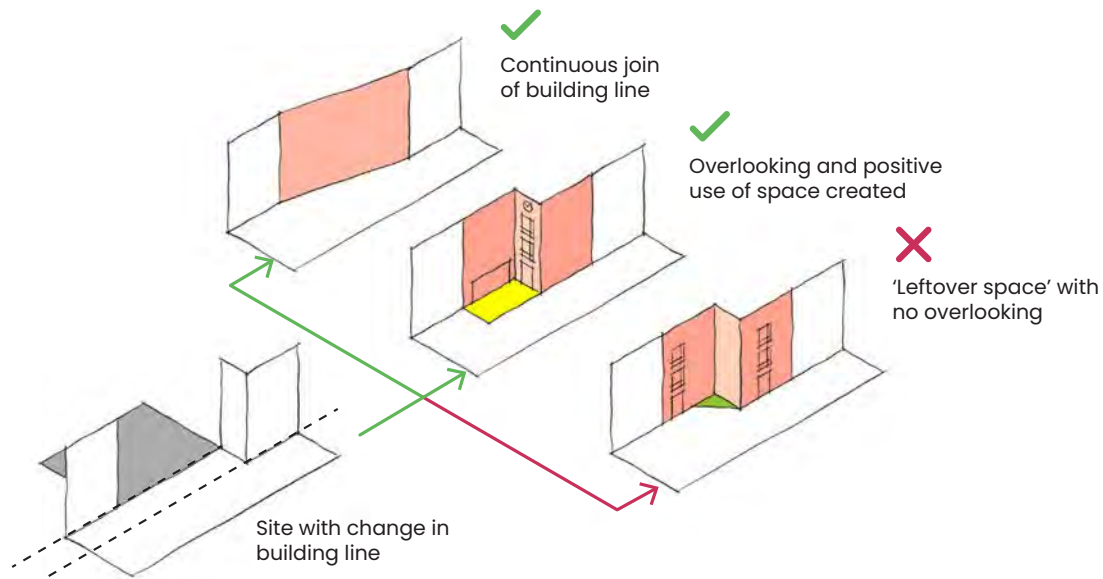
3. Be one storey higher than adjacent buildings, up to the maximum heights specified.

## HS-2 BUILDING LINES

Spelthorne's High Streets have a consistent building line, with most buildings built up to the front of the plot and joining adjacent buildings.

New development **must**:

- Match the surrounding building line and build to the front of the plot
- Ensure where possible at least a 2m footway width in front of the building
- Join adjacent buildings with a party wall
- Where there is a change in building line from one side of a plot to another address the change as shown to in the diagram to the right





### HS-3 BUILDING GRAIN

The plot structure of Spelthorne's High Streets typically leads to relatively narrow buildings, referred to as a fine urban grain. It is also common to see buildings that are wider than this typical grain subdividing the frontage so as to match.

Buildings **must** visually match the prevailing building width of:

- Staines-upon-Thames: 6-10m
- Ashford: 5-10m
- Shepperton: 6-20m, with most buildings wider than 10m subdivided visually
- Sunbury Cross: 6-10m, with most buildings wider than 10m subdivided visually



Ashford - plots and building figure ground showing fine grain of built form



Subdividing the frontage of a single building to match the prevailing grain and rhythm of a High Street (Lower Marsh, Waterloo)

### HS-4 VERTICAL MIX OF USES

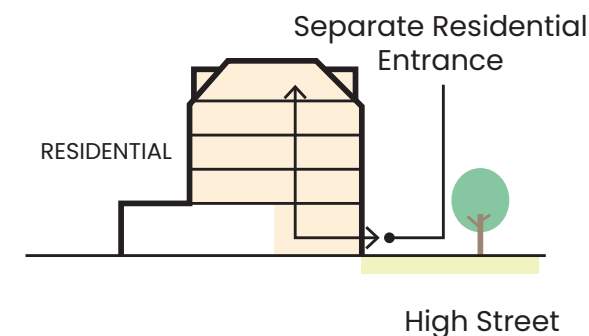
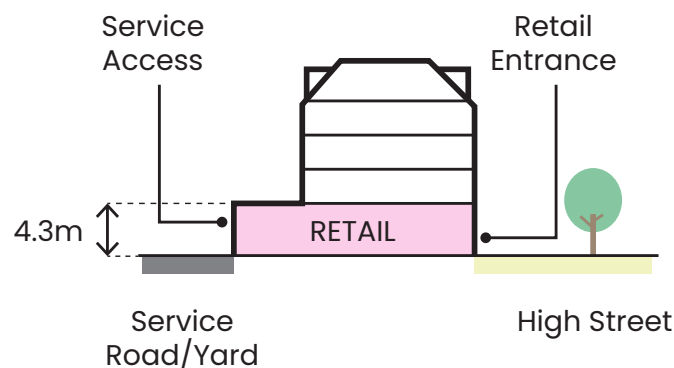
High Streets are defined by their varied ground floor uses. Buildings on High Streets are expected to be mixed-use vertically.

Buildings **must**:

- Have a commercial ground floor suitable for flexible retail use, with a storey height of at least 4.3m to allow for future changes in use
- Have either office or residential dwellings (apartments) on upper floors, with storey heights typically lower, of around 3m
- Have separate entrances for upper floor offices or dwellings, from the High Street

Buildings **should**:

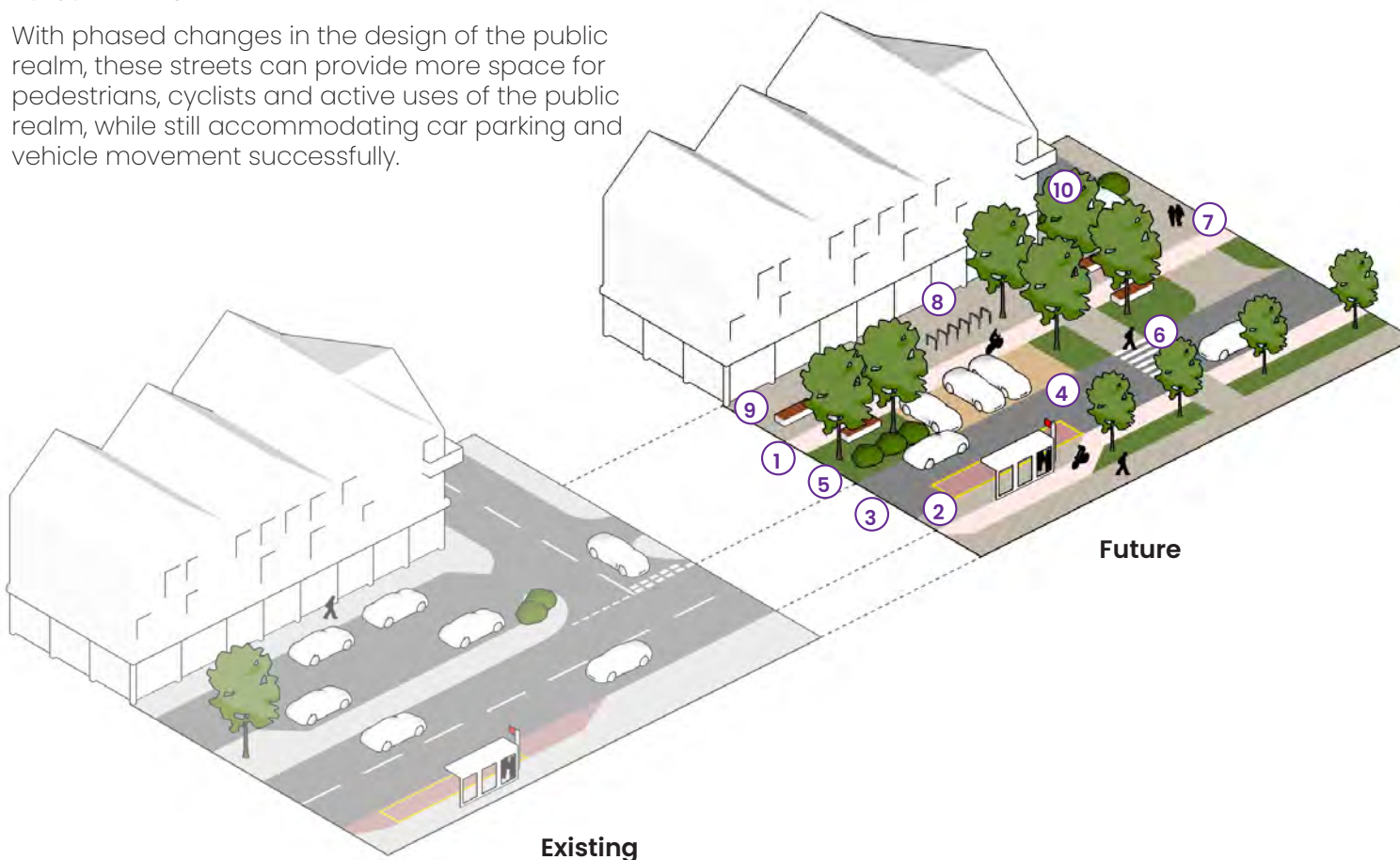
- Locate servicing for retail units to the rear of the building. If servicing is necessary from the street this should be outside of retail hours.



## HS-5 HIGH STREET PUBLIC REALM

Ashford, Shepperton and Sunbury Cross have High Streets with through vehicle traffic, service lanes and occasional streets. They are typically between 25-35m in width.

With phased changes in the design of the public realm, these streets can provide more space for pedestrians, cyclists and active uses of the public realm, while still accommodating car parking and vehicle movement successfully.



Public realm changes to High Streets must follow the principles of Surrey's Healthy Streets Design Code.

Spelthorne's High Streets **should**:

1. Include dedicated cycling provision to LTN 1/20 standards
2. Provide dedicated passenger waiting space at bus stops that does not block footways
3. Limit vehicle carriageway widths to the minimum required
4. Provide on-street parking as bay, parallel or angled arrangements off the main carriageway, with differentiated surface treatments and a maximum of six parking spaces in a run
5. Integrate parking bays into a flexible planting and street tree strip, which can also accommodate seating, lighting, cycle parking and wayfinding
6. Provide frequent pedestrian crossings at key desire lines
7. Provide continuous footways at junctions with side roads
8. Include frequent cycle parking at gateways and junctions along the street
9. Include seating at least every 50m
10. Use streets and landscape design to mark key nodes in the street



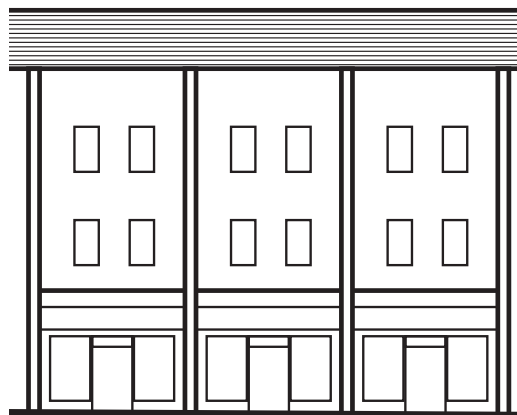
The quality of the public realm, and poor facilities for walking and cycling, detracts from High Streets today.

## HS-6 SHOP FRONTS

The design of shop fronts and building façades will make a strong contribution to the character of the High Street.

Shop fronts **must**:

- Adopt a unified approach to shop front design where buildings are architecturally in the same group within a terrace (e.g. the same building) (diagram centre right)
- Adopt an individual approach to shop front design where buildings are individual (diagram far right)
- Reflect the width of historic plot pattern in their design



Unified approach to shop frontages on the same terrace of buildings

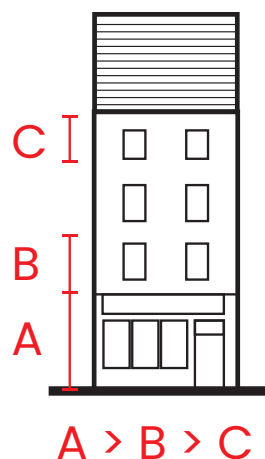


Individual approach to shop frontages on individual buildings

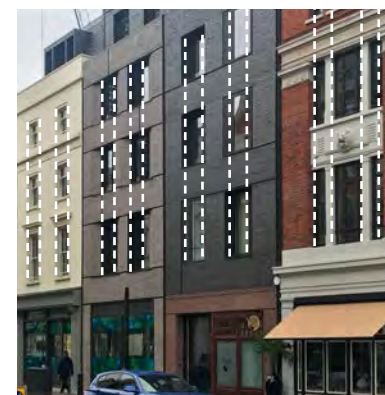
## HS-7 FACADES

Above shop fronts, High Street façades **must**:

- Have a roof visible from the street, e.g. a gable end, pitched roof or mansard roof. Flat roofs will not be accepted.
- Have a base of a single storey, used as a shop front
- Be visually proportioned so that floor and window heights decrease vertically
- Match the rhythm of windows along the street
- Use materials with texture or decorative detail visible at close distances, such as brick, to create visual interest



Proportions of base, middle and top floors in relation to each other



Matching the rhythm of windows along a High Street



Example of decorative detail separating shop front from upper floors (Wellington House, MATT Architecture)



Rich, detailed building façades which have parameters in common with adjacent buildings are popular.

# Town Centre Neighbourhoods

## OVERVIEW

Staines-upon-Thames and Sunbury Cross are town centres where a significant increase in homes is anticipated close to or within the town centre. This development will form new mixed-use neighbourhoods, where facilities and amenities are within walking distance of homes, and other destinations can be accessed by frequent public transport.

Town centre neighbourhoods will be of higher residential density than is typically seen in Spelthorne. They will have residential dwellings and supporting commercial and community facilities, as well as retail space that integrates with the rest of the town centre.

Some parts of town centre neighbourhoods have already been built, for example along London Road in Staines-upon-Thames. As neighbourhoods develop or are regenerated, it will be important to integrate these neighbourhoods into the surrounding town centre, with new streets and open spaces that can bring the existing and new community together.

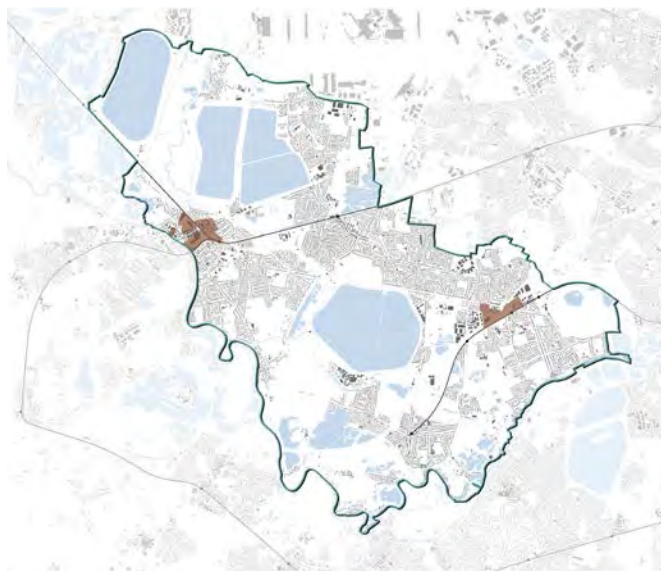
## CODED DEVELOPMENT TYPES

All development in Town Centre Neighbourhoods is anticipated to be in the form of medium to high density residential-led mixed use buildings, with some retail or commercial ground floors. This will include a range of densities and development types from townhouses to towers.

## AREAS OF CHANGE

All Town Centre Neighbourhoods in Staines-upon-Thames and Sunbury Cross are designated as parts of Areas of Change. As well as the requirements set out in this section, they are subject to further spatial coding requirements set out in Chapter 5.

## LOCATIONS



## DESIGN AIMS

Due to the importance of design quality in new and emerging Town Centre Neighbourhoods, Design Aims and corresponding Requirements are set out across six themes:

- The Street & Ground Floor
- Scale & Massing
- Open Spaces
- Homes & Practicalities
- Detail & Richness
- Climate Change & Sustainability



New neighbourhoods must be a part of the existing place, helping new residents be part of the community.



High quality open spaces, affordable, attractive new homes, and appropriate development scale are priorities.





Variety of types of home  
- duplexes, maisonettes,  
apartments and others

Range of private and  
shared amenity spaces

Space for nature  
as well as people

Local facilities and shops

Getting about by walking  
and cycling is the most  
attractive option

Safe, attractive and green  
public open spaces



## The Street & Ground Floor

The street and public spaces are how most people will experience Spelthorne's town centre neighbourhoods, and are of vital importance to the identity of places, and successful integration with their surrounding town centres and neighbourhoods.



Residential active frontage



Spill-out space for cafe seating on walking route

### DESIGN AIMS

The Street & Ground Floor of Town Centre neighbourhoods **will**:

- Maximise active frontages at ground level, whether they be commercial or residential
- Connect the indoors with the outdoors, with appropriate ground floor uses aligned to the adjoining public realm or outdoor space
- Use the built form and design of the public realm to ensure all space has a positive purpose
- Provide a network of streets that prioritise people and active uses over cars, designed on 'superblock' principles
- Include street trees and planting in the public realm

### TC-S1 ACTIVE FRONTAGES

The ground floor connects the street with the activity within the building, and creates safe and secure environments through passive surveillance. Different frontages and design requirements are set out on the next page.

Development **must**:

- Locate frontages with a higher level of activity on busier streets
- Locate **active frontages** to provide passive surveillance of surrounding areas which lack overlooking from other buildings
- Not have more than 10m length of continuous inactive or low activity frontage
- Have ground-floor entrances to homes, retail or commercial space at least once every 10m

### TC-S2 SPILL-OUT SPACE

Active ground floor uses such as retail, cafes, restaurants, community spaces and leisure uses can further animate the street by providing 'spill-out' public realm space for tables, activities and events.

Development **must**:

- Provide 'spill-out' space of at least 2m width on high activity retail and commercial frontages where there is direct sunlight and shelter from winds
- Demarcate spill-out space, e.g. by a change in surface materials
- Development **should**:
- Align internal uses (e.g. cafes) with external spaces (e.g. squares and open spaces)
- Provide shelter of spill-out space through temporary or retractable awnings



A feeling of safety and security in the public realm at all hours of the day is a key community priority.

## (TC-S1 / TC-S2) Types Of Frontage and Spill-Out Space

This diagram sets out the different types of frontage in town centre neighbourhoods. Proposed frontages **must** achieve these design requirements.

### Retail Frontages & Commercial Entrances

A source of activity and vibrancy on main streets. Allow indoor uses to 'spill out' onto the street with a demarcated space extending the public realm.

**High Activity**

### Commercial

Provides overlooking from ground floor offices and other commercial uses.

**Medium Activity**

### Apartment Entrances

Provide a safe, comfortable and welcoming threshold that feels like home.

**Medium Activity**

### Homes

Promote interaction between neighbours and a welcoming, safe threshold space.

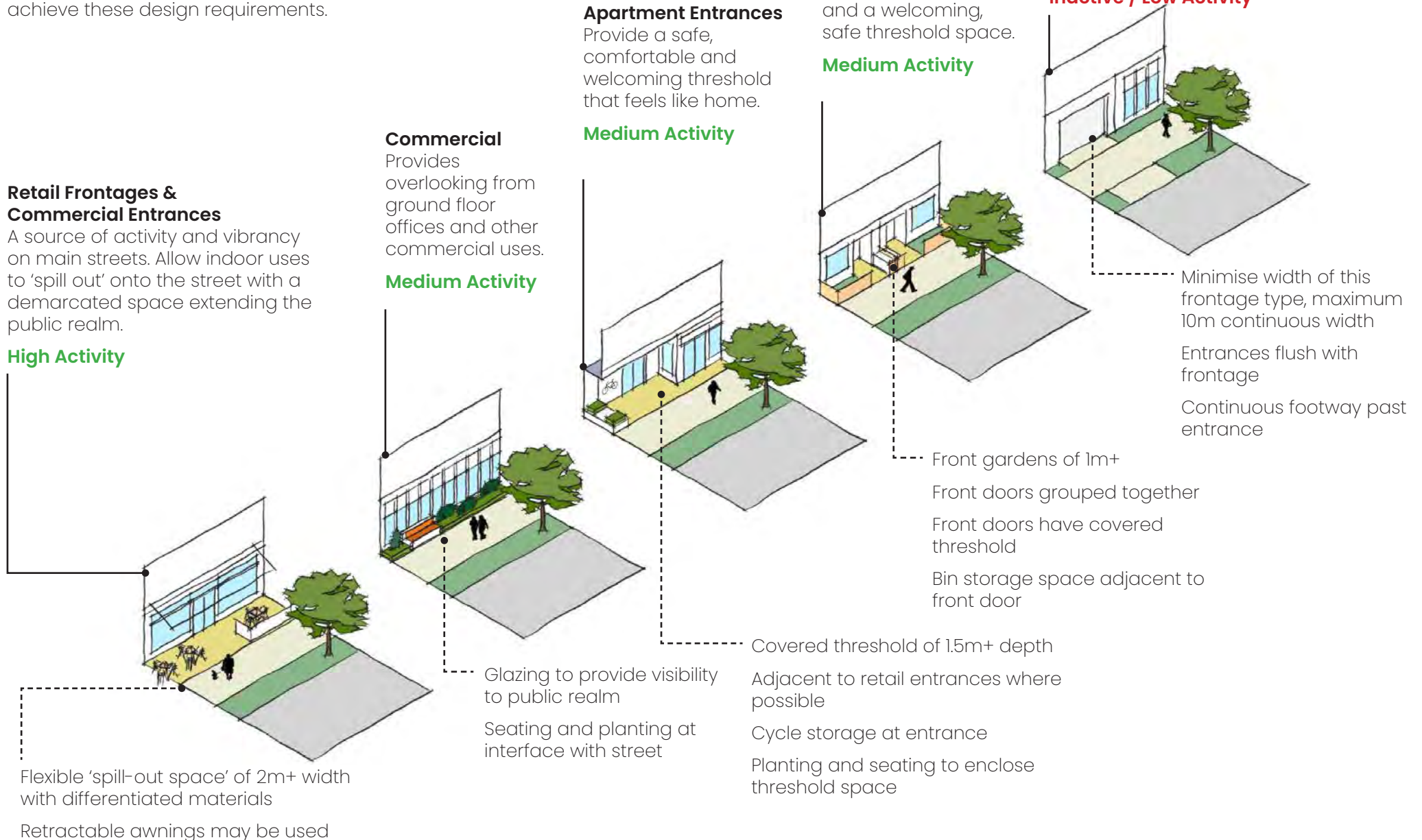
**Medium Activity**

### Service / Parking Access

Minimise the visual impact and lack of activity around these frontages

**Inactive / Low Activity**

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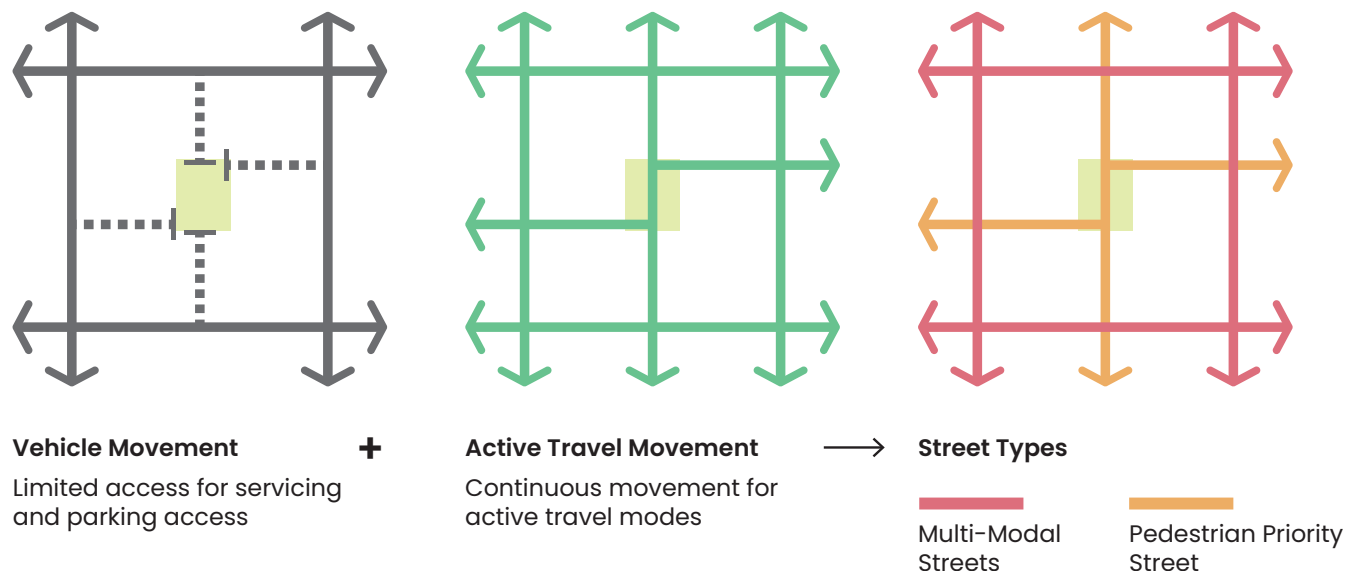


## TC-S3 STREET NETWORKS AND DESIGN

New town centre neighbourhoods **must** be designed on the principle of prioritising the movement of people and active travel within and through the neighbourhood, with vehicle traffic directed onto suitable streets away from people-focused hearts of neighbourhoods.

- Streets within town centre neighbourhoods should be designed as Pedestrian Priority Streets, to prioritise active travel, and the use of the street for play, relaxation, socialising and active uses such as cafes or similar.
- Streets at the edge of town centre neighbourhoods, which are today designed as large traffic-dominated roads, should be re-imagined as Multi-Modal Streets.

Design requirements for these two key street types are set out on the following page. These follow the principles of Surrey's Healthy Streets Design Code.



## TC-S4 STREET TREES AND PLANTING

Street trees provide multiple benefits, including shade, shelter, improvements to air quality, water handling, urban habitats and aesthetic contribution to the character of streets. Other planting can make contributions to some of these aspects and is encouraged.

Where development creates new public realm or streets, they **must** be tree-lined and planting should be integrated throughout the public realm.

Further requirements for landscape and street tree selection are found under TC-O5.



Street trees and planting in a residential street



Trees providing shade in public open space



## (TC-S3) Types Of Street

This diagram sets out design requirements for the different street types within town centre neighbourhoods.



**Pedestrian Priority Street**

**TC-S3a** Pedestrian Priority Streets will vary in character to reflect the surrounding built form and intended vision of the town centre neighbourhood. At a minimum they **must**:

1. Have a shared surface area of at least 5m wide to provide occasional service access or limited access to car parking
2. Have a planting or landscape strip e.g. of at least 3m wide to be able to accommodate street trees, cycle parking, seating, lighting, planted areas, informal play features and surface water management
3. Have a minimum 2m wide footway between the landscape strip and built form front boundary
4. Use differentiated surface material at junctions between pedestrian priority streets

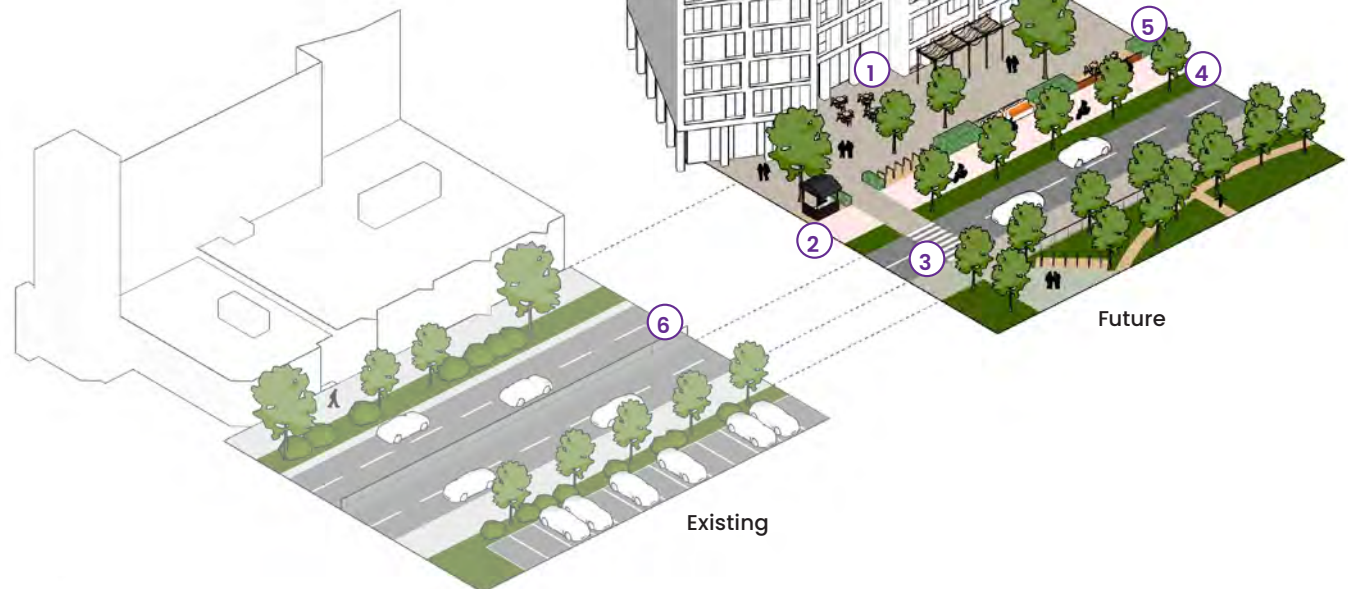
### Multi-Modal Streets

Few (if any) schemes will create new multi-modal streets in town centre neighbourhoods. Most new town centre neighbourhoods will redevelop existing sites and be adjacent to existing major streets. These streets can be redesigned to reduce the dominance of vehicle traffic and provide more space for people and other modes, as shown below.

#### TC-S3b Multi-Modal Streets **must**:

1. Be fronted by built form with active ground floor frontages
2. Include dedicated cycling space to LTN 1/20 standards
3. Have regular pedestrian crossings that connect with desire lines
4. Be tree-lined and include planting areas, retaining existing street trees
5. Include a flexible strip of planting, seating, cycle parking to help delineate movement modes from each other

Reduce the space dedicated to vehicle traffic



## Scale and Massing

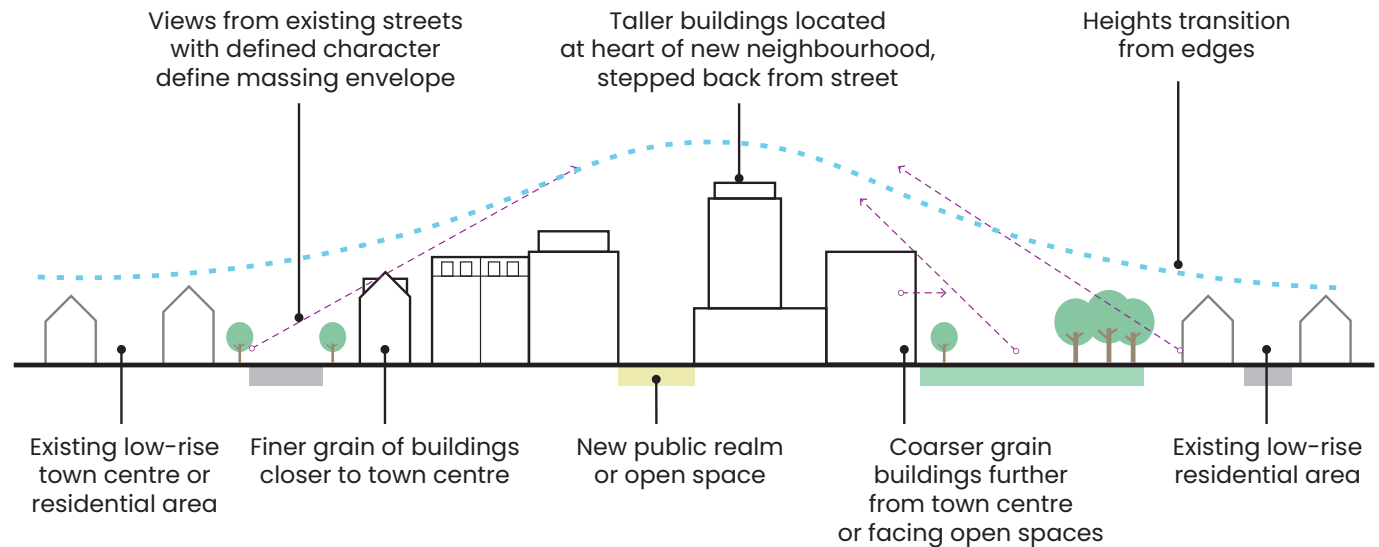
Scale and **massing** must be contextually-sensitive and successfully manage transitions to surrounding areas whilst ensuring an efficient use of land. High density does not need to mean high-rise, and well-designed medium-rise schemes are strongly encouraged in all of Spelthorne's town centre neighbourhoods.

### DESIGN AIMS

The scale and massing of Town Centre Neighbourhoods **will**:

- Consider and minimise the impact on how they will be perceived from the street and areas of local distinctiveness and importance
- Make a positive choice of massing typology that balances integrating with the existing context with the need for efficient use of land in accessible, sustainable locations
- Ensure massing, especially of tall buildings:
  - has a varied elevation over their height
  - does not overwhelm the scale of the surrounding street
  - breaks up elevations through use of materials and facade design
  - keeps a street level microclimate, daylight levels and wind effects that is comfortable

### TC-MI NEIGHBOURHOOD MASSING APPROACH



An overall approach to **massing** of new town centre neighbourhoods is set out in the diagram above. Scale and **massing** includes both the vertical (height) and horizontal (grain) measurements of buildings.

New neighbourhoods may be delivered through a number of different sites. The overall principles will apply across sites, and are defined in detail in Chapter 5, 'Areas of Change'.

New town centre neighbourhoods **must**:

- Transition in height and urban grain from surrounding areas
- Locate taller buildings away from existing low-rise areas



The effect of change on the skyline and character of Staines-upon-Thames is important to the community.

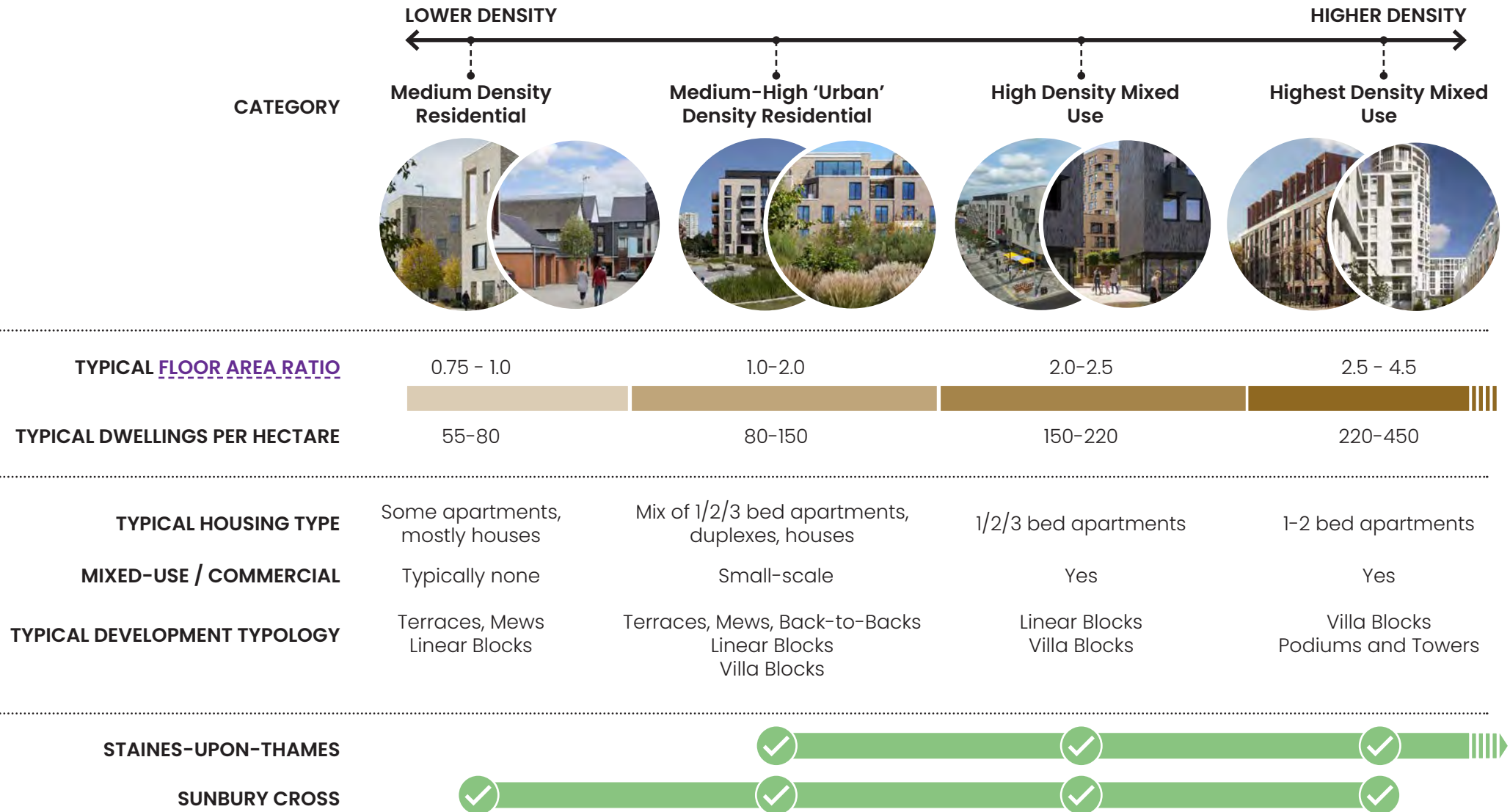


Existing tall buildings in Sunbury Cross create a poor environment which could benefit from mid-rise development.

## DENSITY IN SPELTHORNE'S TOWN CENTRE NEIGHBOURHOODS

Spelthorne's town centre neighbourhoods will vary in prevailing density, measured both in dwellings per hectare and in **floor area ratio**. Anticipated categories of development, their typical density characteristics, and the places in which they are considered appropriate are set out below.

As set out in the 'Design Process' chapter, **the appropriate density for a site will be design-led**, responding to constraints, the existing context, and the requirements set out in this Design Code for Areas of Change.





## TC-M2 DEVELOPMENT TYPOLOGIES

Lower or medium-rise development typologies are preferred in Spelthorne's town centres. These typologies:

- More closely reflect prevailing character
- Can integrate a wide variety of dwelling types
- Retain a more human scale to streets and the built environment
- Retain a closer connection between residents in dwellings and the surrounding public realm

In some locations taller buildings might be appropriate. Due to their sensitivity, additional design requirements apply.

Four different development typologies appropriate to development in Spelthorne are presented on the following pages, including:

- Key dimensional requirements
- Design requirements addressing the design themes of the town centre code
- Where they might be considered and are most appropriate

Within larger sites that may have multiple buildings or character areas, a range of development typologies will be needed to provide a mix of housing types, character and appropriate transitions to surrounding places and spaces. Two applied examples of the use of different development typologies across a larger site are set out later in this section.



## TC-M2a Low-Rise: Terraces, Back-To-Backs And Mews Houses

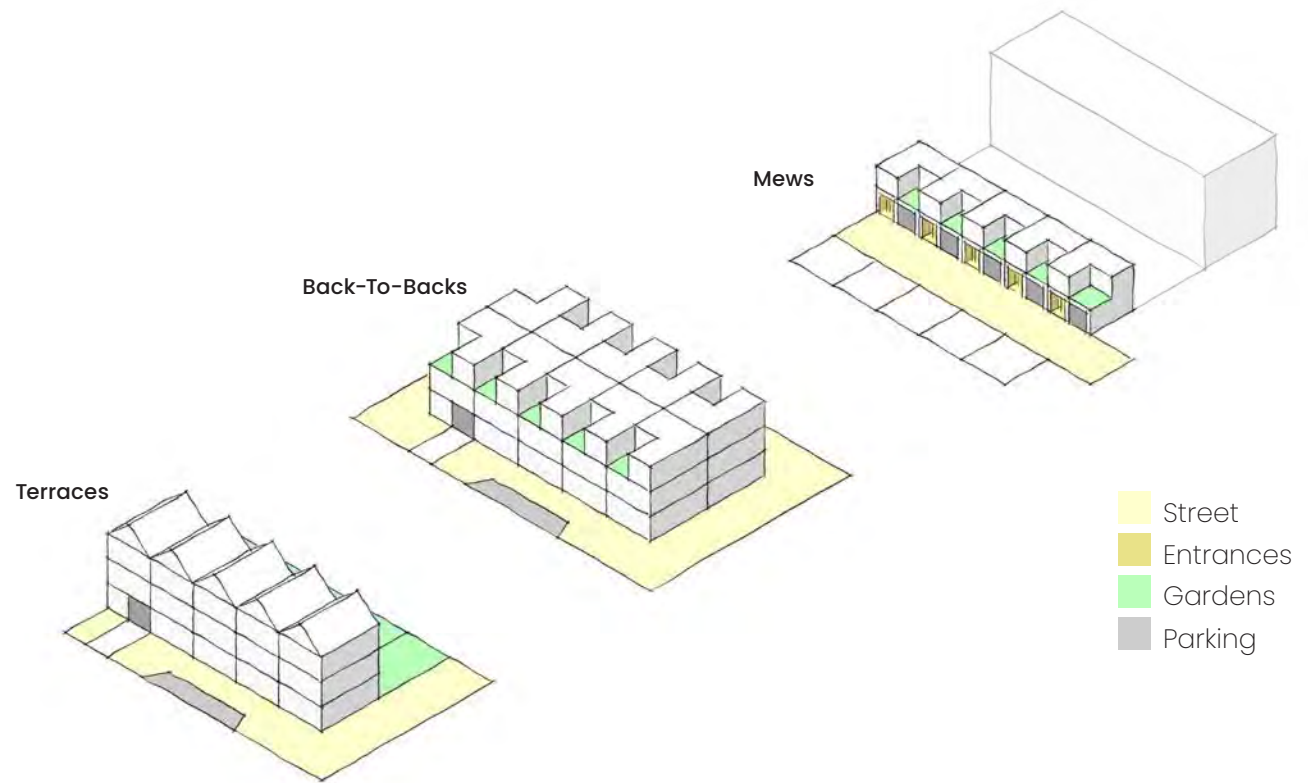
Terraces, back-to-backs and mews houses can provide relatively high densities of houses within low-rise streets that can blend well with the existing character of Spelthorne's town centres and streets. They can help bridge the transition between lower and higher-density development. Care should be taken to ensure sufficient green space, parking and privacy for residents are maintained.

Dimensions this type **must** observe:

- Height up to 3 storeys (approx 15m)
- Typically 2-4 bed homes
- Building depth 8-10m
- Frontage width 5-8m

Most Suitable Locations where this type **should** be located:

- Edges of higher density sites as part of transition to surroundings
- Quieter side streets
- Near areas with strong existing character to be preserved
- Mews streets inserted into centre of perimeter blocks in new neighbourhoods



### DESIGN REQUIREMENTS

Terraces, back-to-backs and mews **must** include:

#### Street and Public Realm

- Typically no commercial ground-floor frontages or uses
- Shared space or pedestrian-priority streets

#### Scale and Massing

- Limited variations in height
- Mix of contemporary and traditional pitched roof types

### Open Spaces

- Private gardens and terraces, including roof terraces
- Any shared open spaces delivered as part of public realm

### Homes and Practicalities

- Front doors on street for all homes
- Car parking on-street, in off-plot parking areas or integral to homes
- Single-aspect types not located facing primarily north or south, to prevent overheating or lack of access to daylight



Providing family homes within town centre neighbourhoods is strongly supported.

TC-M2b Medium-Rise: Linear Blocks

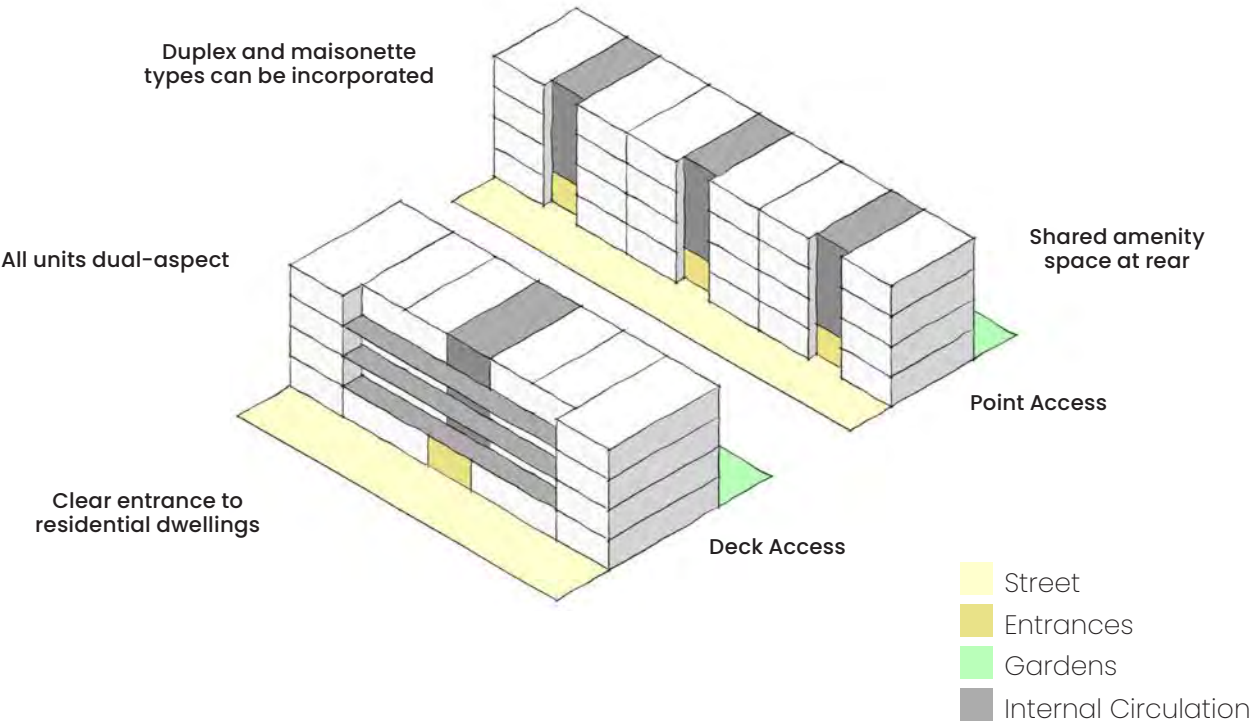
At medium densities and heights, linear blocks can accommodate a range of housing types within a mid-rise town centre neighbourhood. These can often fit into relatively narrow blocks and create a legible urban environment with a mix of quieter and busier frontages and streets.

Dimensions this type **must** observe:

- Height typically 4-6 storeys (approx 18m)
- Deck access heights limited to 5 storeys (approx 15m).
- Mix of duplexes, maisonettes and apartments
- Building depth 8-12m to ensure dual-aspect dwellings throughout
- Frontage widths flexible, typically articulated at 6-10m intervals to align with dwellings
- For point access, a maximum of two dwellings served on each floor per core
- For deck access, a maximum of six dwellings served on each floor per core

Most Suitable Locations where this type **should** be located:

- Edges of higher density sites as part of transition to surroundings
- Larger infill sites on the peripheries of town centres
- Near areas with strong existing character to be preserved
- Sites that have a lower proportion of commercial uses



DESIGN REQUIREMENTS

Linear blocks **must** include:

Street and Public Realm

- Residential active frontages
- Any commercial ground-floor active frontages or uses located only on key corners or facing onto busier streets

Scale and Massing

- Mix of roof types to differentiate buildings
- A minimum of 2 hours direct sunlight on 21st December to all units, ensured by the spacing of buildings

Open Spaces

- Private gardens and terraces, including roof terraces for maisonettes and duplexes
- Shared gardens for duplexes and apartments

Homes and Practicalities

- Both deck access or internal (point/walk-up) access possible.
- Front doors on street for maisonettes ground floor units
- Car parking on-street or integral to homes for maisonettes, shared off-plot provision required for other forms



## TC-M2c Medium-Rise: Villa Blocks

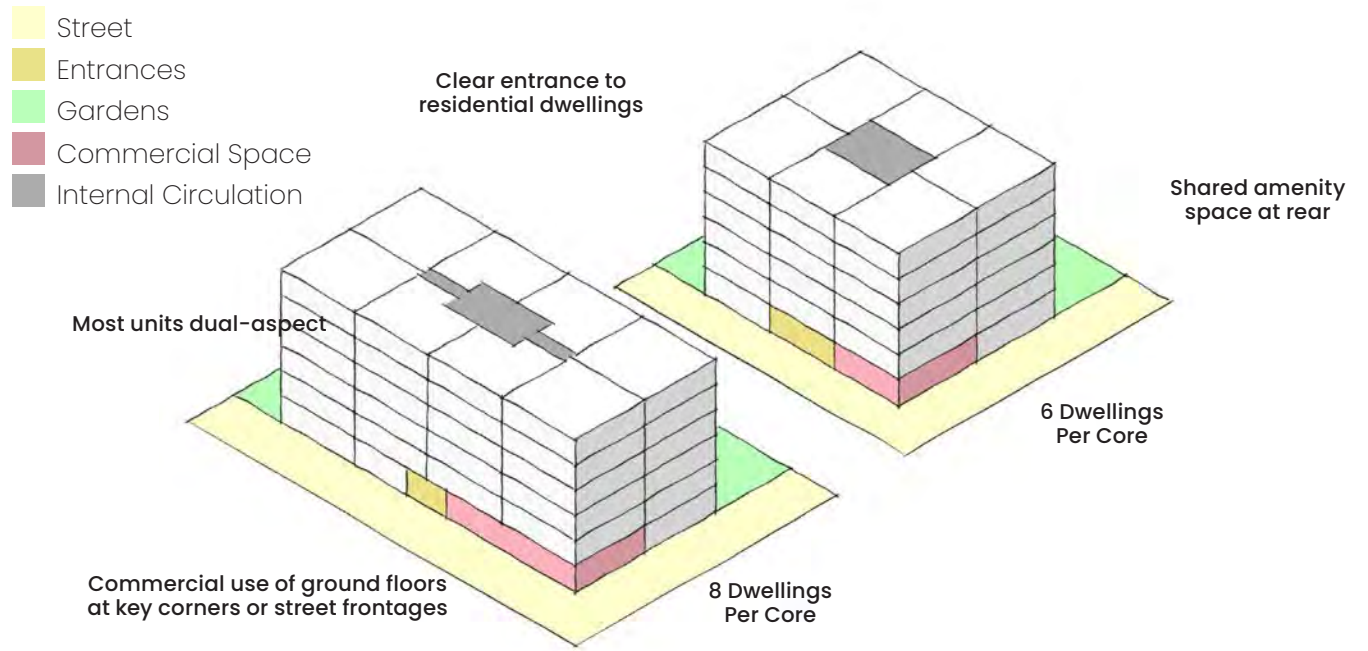
This typology can provide a flexible mix of higher density apartment buildings and ground-floor commercial uses set within a network of open space or as part of other typologies. They are typically double-fronted at ground level enabling them to be flexibly used, but care needs to be taken to ensure frontages are active and servicing is located in the right places.

Dimensions this type **must** observe:

- Height typically 5-10 storeys (approx 15-30m)
- Floors above 8 storeys (approx 24m) should be set back
- Mix of 1, 2 and 3 bed apartments
- Building depth 16-20m
- Building width 20-30m
- Subdivision of frontage to create finer visual grain on larger buildings
- No more than eight dwellings on each floor served by a single core

Most Suitable Locations where this type **should** be located:

- On key corners and busier streets
- Adjacent to green open spaces, ensuring views for residents
- As the main component in many medium-high density developments away from sensitive edges



### DESIGN REQUIREMENTS

Villa blocks **must** include:

#### Street and Public Realm

- Commercial ground-floor **active frontages** or uses at key nodes

#### Scale and Massing

- Within larger sites with multiple buildings, lower heights used on southern sides of sites to ensure daylight access to open spaces
- Incorporate plant within the roof design

#### Open Spaces

- Private balconies and terraces
- Shared amenity open spaces between buildings need to be on a podium or have some change in levels from the public realm to provide separation from the street

### Homes and Practicalities

- Shared entrances for apartments can be provided on both sides of building
- At least half of units dual-aspect
- Central core providing access to all upper-floor units
- Shared refuse and cycle storage at ground level
- Individual entrances to units located on the ground floor
- Private gardens for units fronting onto shared amenity spaces
- Car parking either underground or housed off-plot

## TC-M2d Medium To High-Rise: Podiums And Towers

Towers with apartments provide the highest-density option for new residential development. They can also be the most impactful on surrounding areas due to their scale, can deliver a large number of high-quality new homes in town centres, and can have impacts on the street due to overshadowing and wind effects. As such they require careful design.

Due to safeguarding restrictions for Heathrow Airport, the maximum height of any development in Spelthorne is 45m (approximately 15 storeys).

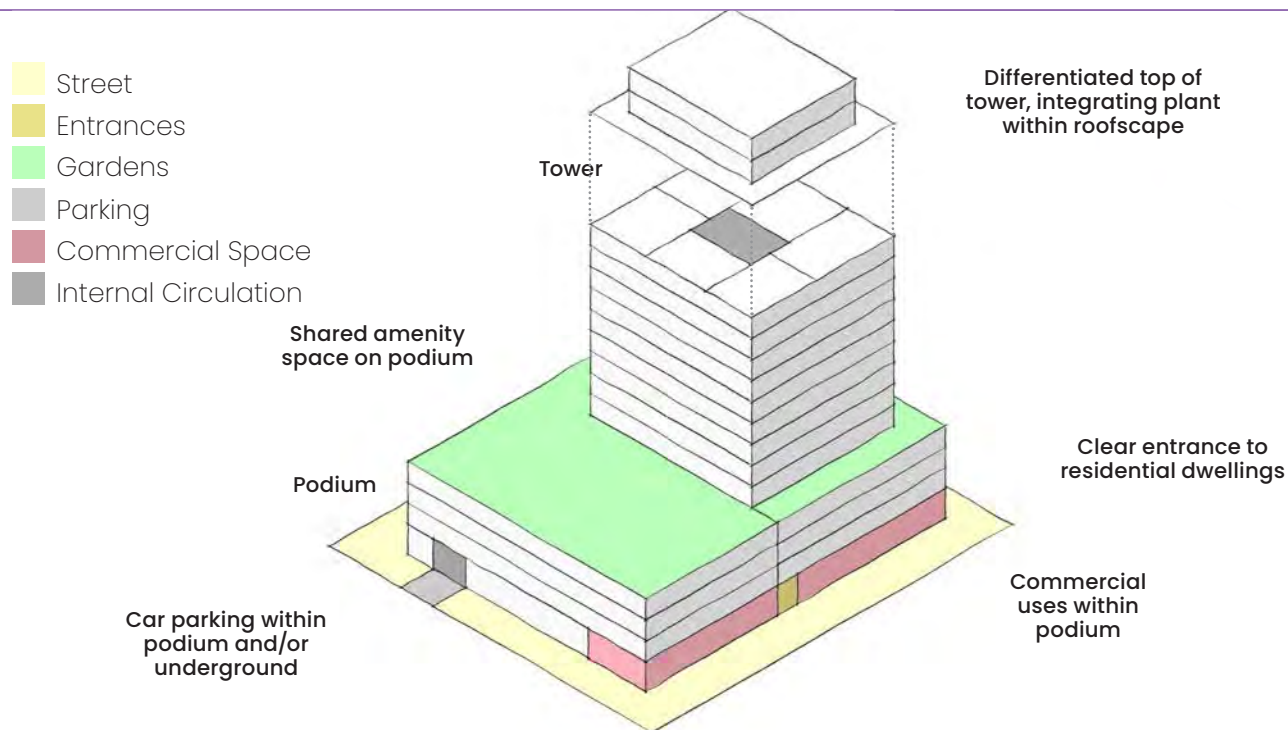
Further requirements for tall buildings are set out under 'Tall Building Design Requirements' later in this section.

Dimensions this type **must** observe:

- Towers sit on and are set back from a podium or base building
- Heights: towers of between 10-15 storeys, lower base/podium up to 6-8 storeys
- 1-2 bed apartments, some 3-bed apartments possible
- Tower width and depth typically 20-30m
- No more than eight dwellings on each floor served by a single core

Most Suitable Locations where this type **should** be located:

- The highest density town centre areas
- As part of town centre neighbourhoods where an appropriate transition has been achieved, in line with the overall approach to **massing** set out earlier in this section



### DESIGN REQUIREMENTS

Podiums and towers **must** include:

#### Street and Public Realm

- Commercial ground-floor uses within podium
- Podium to provide strong edge to street, with no open space without a clear use or ownership (public/private) around the tower base

#### Scale and Massing

- Towers to comply with the tall building requirements set out on the following pages

### Open Spaces

- Private balconies and terraces, including roof terraces
- Balcony types to comply with tall building requirements on following pages
- Shared podium gardens to provide amenity space for residents

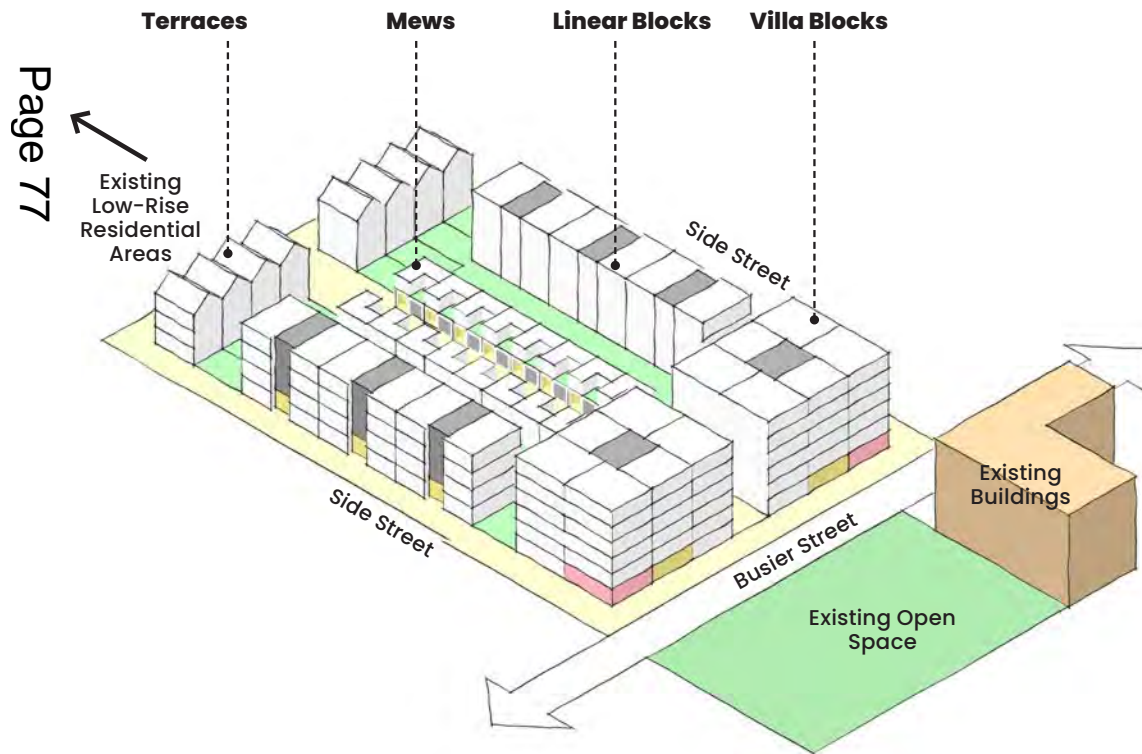
### Homes and Practicalities

- Shared entrances for apartment blocks located on the street, with a connection through to any shared podium garden
- Car parking underground, within podium, or a combination of both.

## Applied Example 1

### Approx 200dph, Floor Area Ratio 1.85

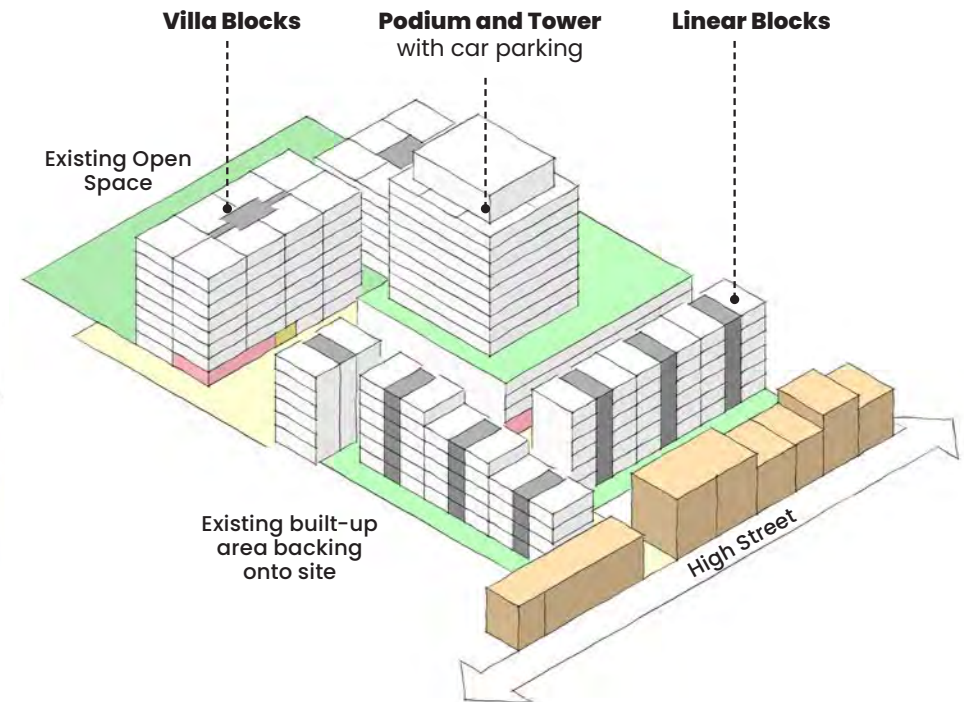
The site below is 100m x 60m, adjacent to a busy commercial street and surrounded by lower-rise side streets, that transition to existing residential areas. The example maximises the potential of the site by selecting typologies on side streets that provide moderate intensification of built form, and locate the densest typologies along the main street. To increase the variety of provision a mews street is inserted into the centre of the block.



## Applied Example 2

### Approx 330dph, Floor Area Ratio 3.2

The site below is 105 x 80m, adjoining an existing High Street of 3-4 storeys linking through to an existing open space. The example transitions from the existing area by building heights from the High Street with fine-grain buildings into the centre of the new neighbourhood. A tower is located adjacent to new public realm at the centre of the neighbourhood. Coarser grain Villa Blocks overlook the open space.





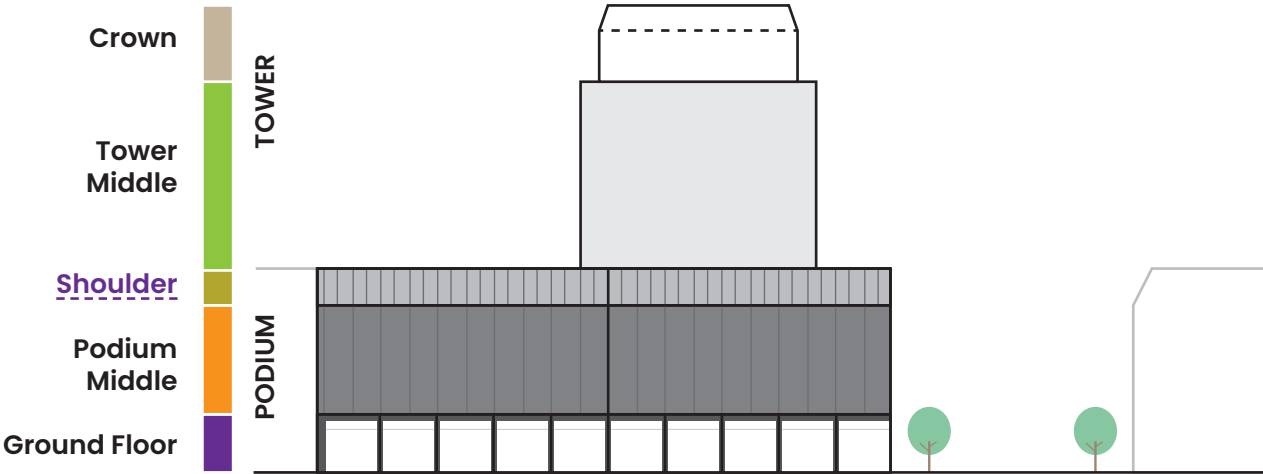
TC-M3 TALL BUILDING DESIGN REQUIREMENTS

When designed well, tall buildings can make valuable contributions to the character of a place, can accommodate significant numbers of new dwellings and can be positive new landmarks.

Tall buildings are defined in Staines-upon-Thames and Sunbury Cross as buildings over 8 storeys (around 24m).

The generally acceptable locations for tall buildings in Staines-upon-Thames and Sunbury Cross are set out in the heights plans in the Areas of Change coding later in this chapter. They are located so as to:

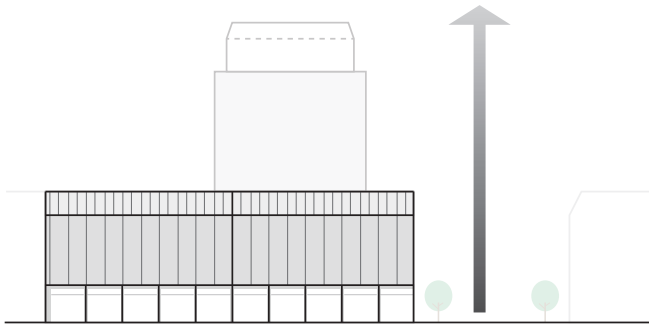
- Respect the scale of existing streets and areas that are valued and form part of the identity of place
- Be within coherent new neighbourhoods, following the overall approach to **massing** set out earlier in this section, creating an overall rhythm of height variation across the town centre, with taller heights grouped together
- Be adjacent to spaces that can ‘absorb’ their scale



Each of the distinct parts of a tall building **must** be designed as set out below and in the principles set out on the following page..

Ground Floor	Podium Middle	Shoulder	Tower Middle	Crown
Activated ground floor relating strongly to street/ public realm	Main part of street facade elevation	Visibly differentiated top of street facade	Set back mass of tower, arranged to not dominate street	Visibly differentiated top of the building for longer views
Taller heights (typically 4.3m) to provide future flexibility of use	Balconies facing street should be inset or partially inset	Reduced storey height or levels of glazing to ‘cap’ facade	Balconies should be inset	Plant integrated within roof design
High levels of glazing when commercial uses specified	High levels of detailing and articulation to ensure visual interest	Maximum height should ensure visual connection between floor and street (typically 6 storeys, appx 18m)	Set tower back from plot edge	‘Penthouse’ accommodation can be incorporated within crown

Taller buildings have their place but must not overwhelm streets with an existing traditional character.

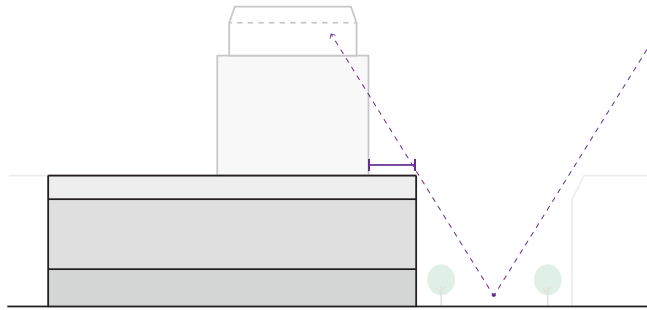


### TC-M3a Breaking Up Massing

Facades for podium/base buildings **must** be broken up through vertical articulation such as pillars, changes in material and other architectural features to match the prevailing grain of the street and area.

Material choices **must** ensure that the tower is visually subservient to the base. This is usually achieved through selecting a paler material that recedes when viewed from the street, and the design of façades to be less visually prominent.

If needed to fit the prevailing urban grain, podium/bases **should** be designed so as to appear to be two or more independent buildings,

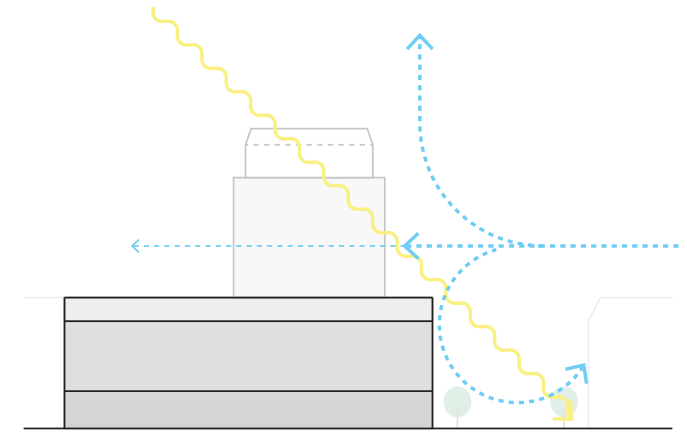


### TC-M3b Scale Of The Street

The use of a setback and 'shoulder' for the tower element of a taller building creates a street environment that is not overwhelmed by the massing of the tower.

Shoulder heights on existing streets **must** be set at or one storey above the prevailing heights. For new streets, shoulder heights are a maximum of 6 storeys (approx 18m).

Setbacks **must** be a minimum of 3m but larger setbacks are encouraged, and the impact of a tower on the existing street scale and townscape will be assessed on a site-by-site basis.



### TC-M3c Microclimate

Taller buildings can have significant effects on wind and sunlight at street level and in the surroundings.

Sunlight analysis **must** be undertaken to demonstrate that surrounding public spaces and dwellings retain access to sunlight for at least two hours per day during winter.

With an appropriate setback above a podium, 'wind tunnel' effects on public spaces are likely to be limited. Stepped, rounded or chamfered corners and massing are approaches that can be used to further reduce these effects at ground level.

Open Spaces

Open spaces in town centre neighbourhoods are vital relief for relaxation, socialising, nature and general health and wellbeing. Spelthorne’s town centres generally have few public open spaces, and new development should maximise opportunities to include them, as well as providing space for residents to use.

DESIGN AIMS

Open Spaces in Town Centre Neighbourhoods will:

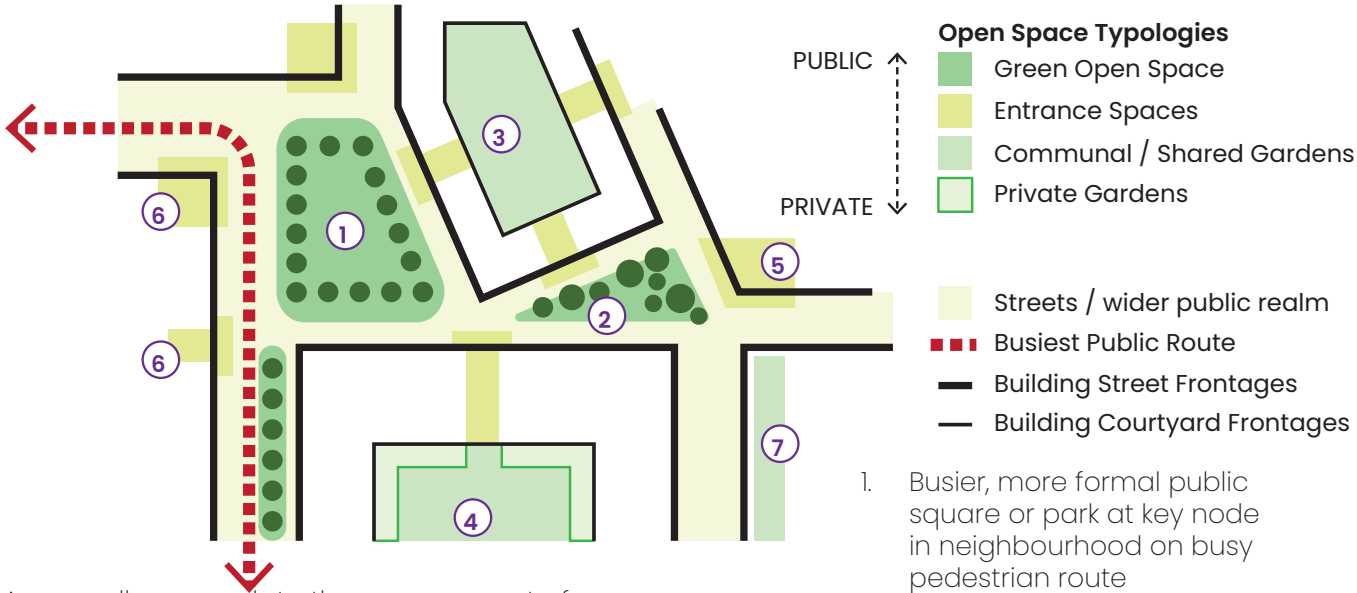
- Be safe and secure (and perceived to be so) for all user groups at all times of day
- Encourage physical activity, enable social interaction, provide access to nature and be inclusive
- Be of a variety of types appropriate to a town centre context and their intended use
- Include provision of shared amenity space for residents to use regardless of tenure

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Green spaces form a vital part of Spelthorne’s identity, but the town centres lack good provision.

TC-O1 NEIGHBOURHOOD OPEN SPACE APPROACH



An overall approach to the arrangement of open spaces in relation to each other and built form is set out in the diagram above. Each typology has detailed requirements on following pages.

New neighbourhoods may be delivered through a number of different sites. The overall principles will apply across sites, and are defined in detail in Chapter 5, ‘Areas of Change’.

New town centre neighbourhoods **must**:

- Clearly define public and private spaces
- Ensure the intended character of open spaces reflects the activity of connecting streets, with busier spaces on busier streets

1. Busier, more formal public square or park at key node in neighbourhood on busy pedestrian route
2. Quieter neighbourhood incidental ‘pocket park’ away from main routes, with seating, informal play areas and a less formal character
3. Ground-level communal garden
4. Podium-level communal garden with private gardens for dwellings at edges
5. Building entrances at key corners and onto open spaces
6. Regular entrances to activate street
7. Shared roof terrace in setback area above street



## TC-O2 SAFETY AND SECURITY

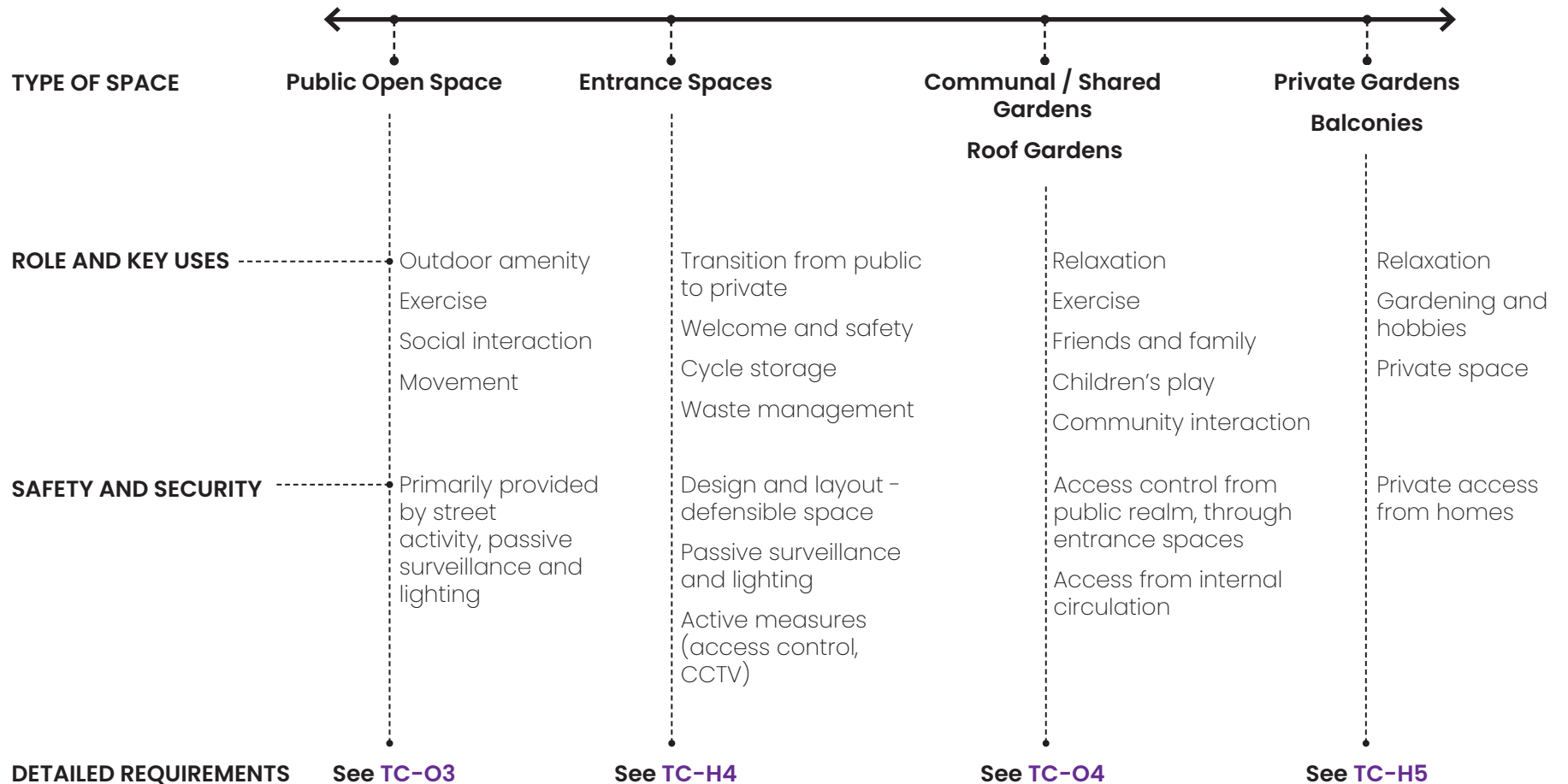
Open space typologies are defined on a continuum of public to private use, which defines their role and key uses within an open space network.

All open space **must** implement the principles set out in Secured by Design.



PUBLIC

PRIVATE



## TC-O3 PUBLIC OPEN SPACES

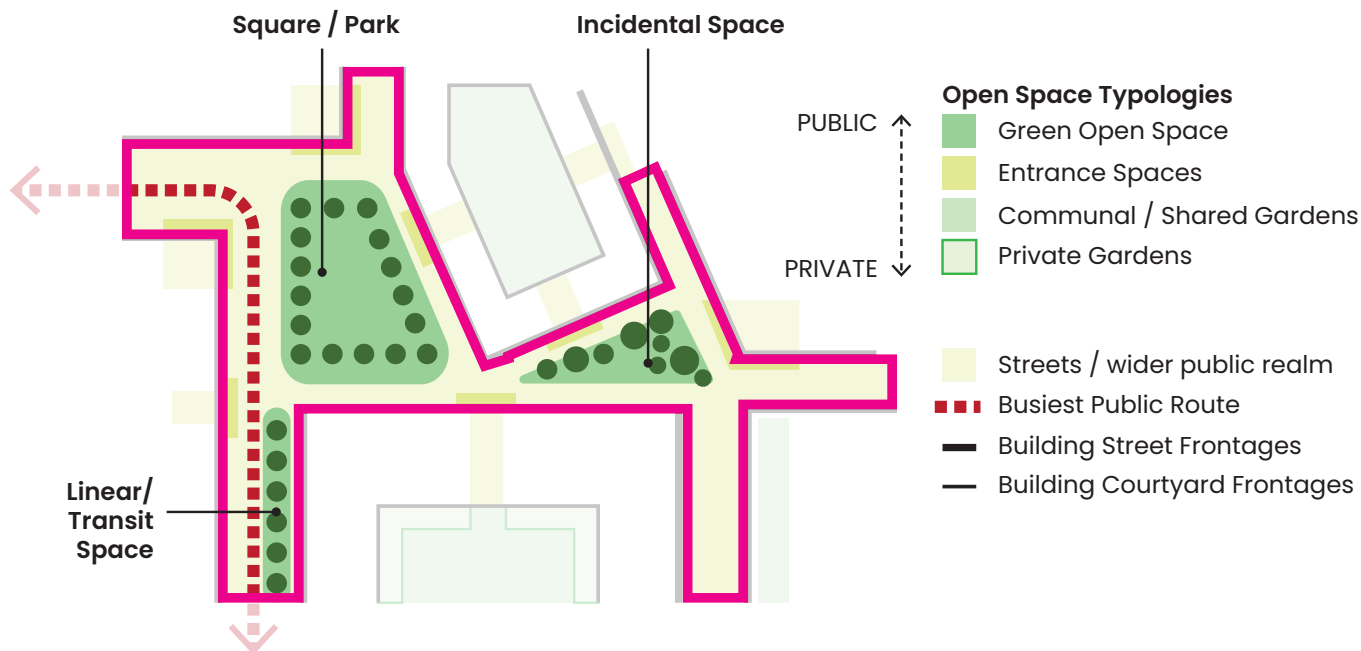
Public open spaces **must**:

- Be overlooked by surrounding built form, with **active frontages** at ground level
- Provide seating at least once every 50m along active travel routes
- Provide visitor cycle parking
- Be universally accessible to all abilities
- Be well-lit with no concealed spaces
- Include planting and trees for shade and shelter
- Include surface water management systems and solutions including permeable paving, permeable planted areas, rills, drains and other water management features
- Connect to and extend active travel routes through the space to LTN 1/20 standards
- Be protected from vehicle traffic through bollards or other boundary treatments

Depending on character and intended use, public open spaces **could** include:

- Events space in larger, busier open spaces
- Gathering and socialising spaces
- Community garden space in quieter, neighbourhood spaces
- Informal and designated play areas, outdoor gyms and trim trails

Suitable public open space typologies and key design requirements within the Town Centre Neighbourhoods area type are set out on the following page.



'Play on the way' in streets



Seating, movement, gathering and relaxation space as an integral part of landscape design



Flexible use of existing open space for events and temporary seating



### TC-O3a Squares and Parks



Located at key nodes within the town centre active travel movement network. Squares and parks **must** have:

- High levels of enclosure by surrounding built form, with a width:height aspect ratio of between 1.5:1 and 3:1
- A mix of hard and soft landscaping, seating, trees for shade and a focal point of interest
- Typical maximum dimensions of around 50-70m along the edges

### TC-O3b Courtyards, Incidental Spaces and Pocket Parks



Courtyards, incidental spaces and pocket parks enclosed by built form **must**:

- Be used positively for functions such as cycle parking, surface water management, informal play space and biodiversity enhancement
- Be smaller in size than squares and parks, and typically quieter in feel and character
- Have passive surveillance and good lighting
- Have sufficient daylight to avoid spaces become dark and unpleasant

### TC-O3c Linear and Transit Spaces



Linear parks and extended green open spaces can successfully bring together development along key active travel routes. They **must**:

- Have high levels of enclosure by surrounding built form with **active frontages** throughout
- Have a maximum width of around 30-40m
- Be varied in character along their length to ensure legibility
- Clearly delineate through active travel routes



## TC-04 SHARED/COMMUNAL OPEN SPACES

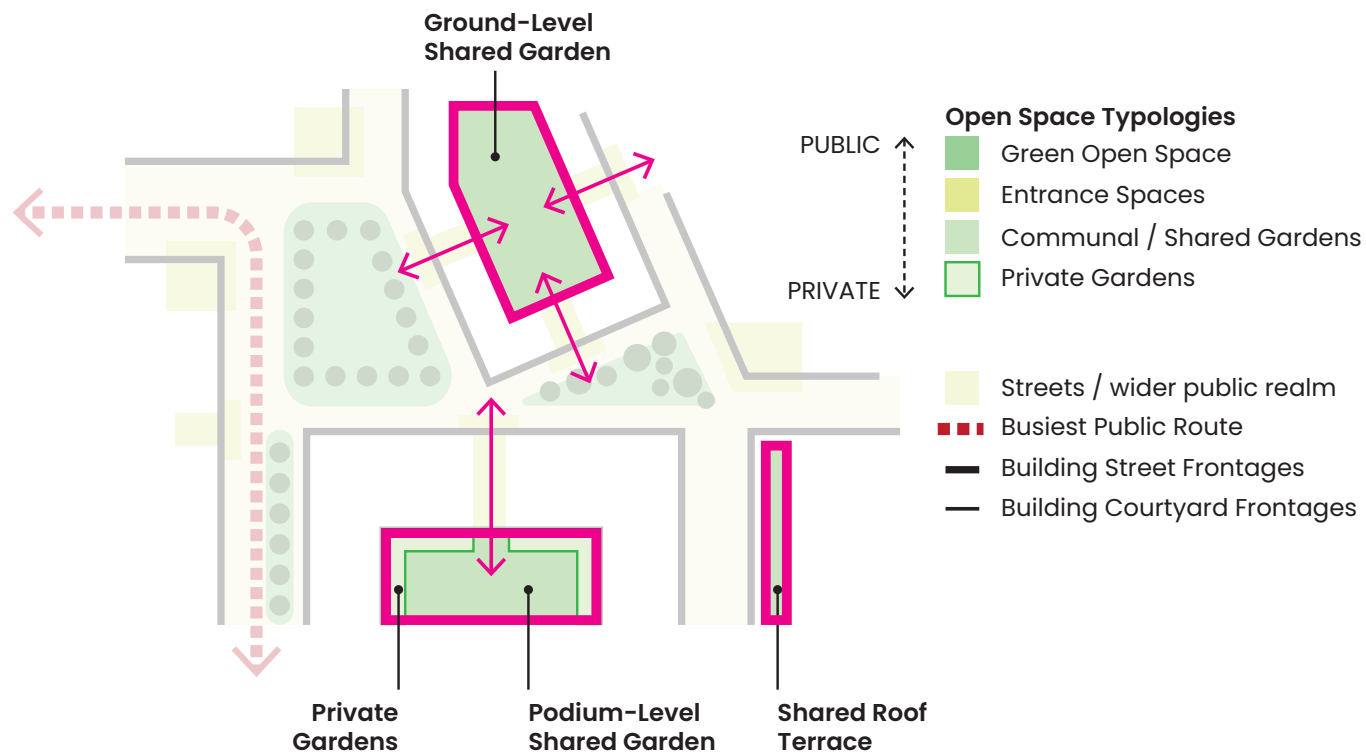
Shared open spaces **must**:

- Be universally accessible to all abilities
- Be overlooked by surrounding dwellings, with levels of lighting so as not to disturb residents
- Be separated from residential units by a minimum of 1.5m of defensible buffer space (e.g. a private garden) or boundary treatment to provide privacy
- Provide regular seating, mixed in groups for socialising and individually for relaxation
- Include planting and trees for shade and shelter, and to provide permeable surfaces
- Achieve a minimum of 2hrs sunlight covering at least 50% of their usable area on March 21st
- Be a minimum of 21m wide to provide privacy between dwellings. Subject to daylight requirements, this may be reduced if windows and built form are arranged in a manner so as to provide privacy between facing dwellings.

Depending on character and intended use, shared open spaces **could** include:

- A mix of different gathering and socialising spaces, and more secluded spaces for relaxation
- Community gardens and food production
- Informal play areas, outdoor gym equipment
- New habitats and natural spaces

Shared open space typologies and requirements within the Town Centre Neighbourhoods area type are set out on the following page.



Private garden space facing shared community garden



Shared roof terrace space sheltered from prevailing winds



Seating arranged in groups to encourage socialising

### TC-O4a Ground-Level Gardens



Ground-level gardens can be both communal or provide access to the public with sufficient design consideration. They **must**:

- Be separated from the public realm by built form or other features that provide privacy, access control and visual separation
- Have a clear buffer and boundary treatment of up to 1.2m high to homes fronting or backing onto the space

### TC-O4b Podium Gardens



Communal gardens sat on top of podiums that house parking, servicing or other development are an efficient way of using space. They **must**:

- Be planted with plants with smaller rooting requirements
- Be connected directly to vertical circulation and entrances in surrounding buildings
- Have a clear buffer and boundary treatment of up to 1m high to homes fronting or backing onto the space

### TC-O4c Roof Gardens and Terraces



Roof gardens and terraces can be both communal or private spaces. They **must**:

- Be sheltered (by surrounding built form or other features in their design) from prevailing and northern winds, and make the most of solar gain through their aspect
- Be planted with species of a lower height that can survive at height and with less soil



## TC-O5 LANDSCAPE CHARACTER

Town centre neighbourhoods will typically have a more urban, managed character than suburban areas. Busier areas will have a high proportion of hard landscape. There will be opportunities to incorporate softer landscape and planting throughout, and smaller, incidental spaces should be greener and calmer than busier spaces.

The integration of street furniture so as to avoid clutter and complement the overall landscape scheme is an important component of the landscape character of town centre neighbourhoods.

Material selection in the adopted public realm **must** be in compliance with the Surrey Healthy Streets Design Code.

### TC-O5a Hard Landscape

Hard landscape materials **must** be selected to be long-life, attractive and delineate different uses effectively.



Changes in material can help to delineate movement areas from seating, utility, gathering and other spaces.



Patterns within public realm surfacing at key nodes can reinforce importance of location and memorability

### TC-O5b Soft Landscape

Soft landscape features **must** be incorporated throughout the public realm to provide new habitats, shade, cooling, surface water absorption and to soften the appearance of the built environment.



Planted beds can separate different spaces within the public realm.



Planted strips can provide flexible areas to accommodate uses that activate a street, such as seating or informal play equipment

Species **should** be varied to ensure resilience to climate change and invasive species.

Management and maintenance **should** be minimised where possible.

### TC-O5c Street Furniture



Seating can be integrated with planting beds



Changes in level can be informal seating areas



Low-level bollard lighting performs multiple functions



Ensure street furniture is installed where it does not block movement



## TC-O5d Street Trees

All streets **must** be tree-lined. In general trees will be integrated with hard landscape or planted beds and associated street furniture within the street scene. Suitable approaches include:



Trees installed within hard landscape



Trees installed within small planted areas



Trees installed within planting beds and street furniture

Trees **must** have sufficient space to grow and thrive, following guidance set out by the Trees Design Action Group (see reference in Chapter 6).

Using a variety of street tree species ensures resilience to climate change and invasive species.

## TC-O5e Surface Water Drainage Features

All development **must** manage surface water through the use of Sustainable Drainage Systems (SuDS). Suitable design features include:



Source Control / Initial Absorption Features

- Street 'rain gardens'
- Planted verges and general soft landscape cover
- Green roofs and walls
- Permeable surfaces and details



Conveyancing Features

- Rills and other channels
- Planted street swales
- Incorporation into public realm features



Attenuation Features

- Larger rain garden features as part of incidental spaces
- Underground crate storage (where no other options available)

Homes and Practicalities

Town centre dwellings should be welcoming, safe and convenient places to live, with all the pleasures and conveniences of modern urban living to hand. Enough space, a mix of homes and well-designed essentials will make new homes in town centres built to last.

DESIGN AIMS

Homes in Town Centre Neighbourhoods will:

- Be sized to Nationally Described Space Standards and include a flexible mix of storage space
- Be of a mix of dwelling types to create inclusive and balanced new places
- Be mostly dual-aspect dwellings
- Have safe, welcoming entrances from the street with essentials such as waste, recycling and cycle storage close to the front door
- Provide sufficient private outdoor amenity space for all residents
- Accommodate vehicle and cycle parking

TC-HI SPACE STANDARDS

All homes **must** be at least the sizes specified in the Nationally Described Space Standards ([Local Plan](#) policy HI). The March 2015 standards are set out below for reference.

Dwellings **must** offer a mix of flexible internal storage and secure outdoor areas of storage for items such as pushchairs. In apartment buildings such larger storage areas are best accommodated adjacent to dwelling front doors.

Table 1 - Minimum gross internal floor areas and storage (m<sup>2</sup>)

Number of bedrooms(b)	Number of bed spaces (persons)	1 storey dwellings	2 storey dwellings	3 storey dwellings	Built-in storage
1b	1p	39 (37) *			1.0
	2p	50	58		1.5
2b	3p	61	70		2.0
	4p	70	79		
3b	4p	74	84	90	2.5
	5p	86	93	99	
	6p	95	102	108	
4b	5p	90	97	103	3.0
	6p	99	106	112	
	7p	108	115	121	
	8p	117	124	130	
5b	6p	103	110	116	3.5
	7p	112	119	125	
	8p	121	128	134	
6b	7p	116	123	129	4.0
	8p	125	132	138	



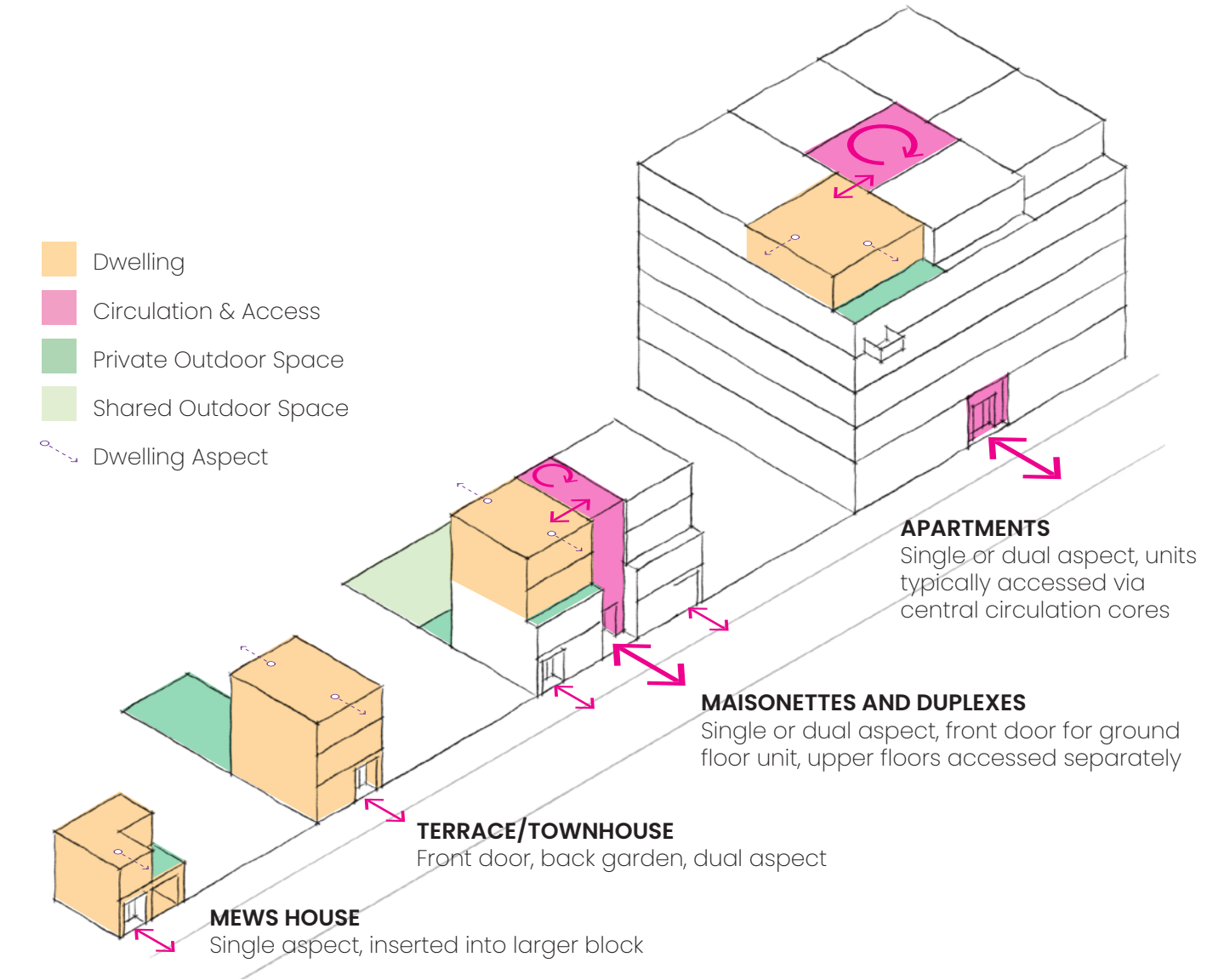
Spelthorne’s communities want to see spacious and high quality homes provided for new residents.

## TC-H2 MIX OF HOMES

A wide mix of types of dwelling are encouraged in all developments. These are closely related to the Development Typologies in 'Scale and **Massing**'.

Dwellings **must**:

- Be designed to be tenure-blind with no differentiation between affordable and market tenures.
- Not have 'poor doors' or other selectively gated forms of development.



Types of Dwelling, Aspect, Access and Arrangement

## TC-H3 DWELLING ASPECT

Dwellings which have dual aspects have several benefits for residents, including:

- Sunlight at multiple times of day for passive heating and access to natural daylight
- Passive cooling in hotter weather from breezes through the building

Development **must**:

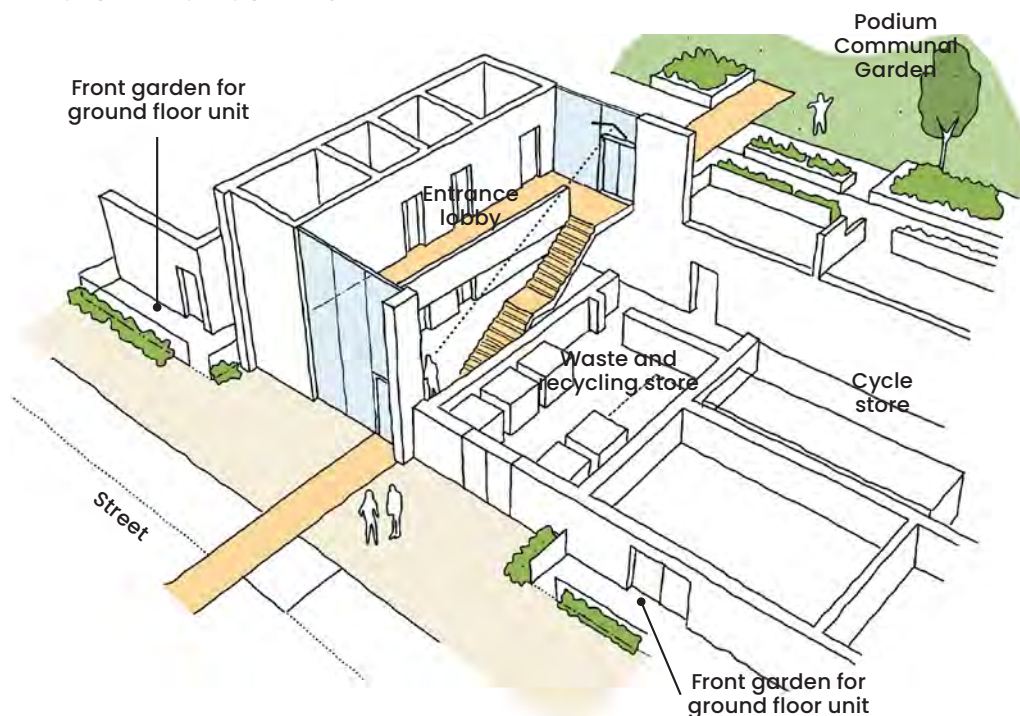
- Maximise dual aspect dwellings
- Size dwellings with aspects on opposing sides to be no more than 12m deep
- Size dwellings with aspects at right angles to be no more than 8m deep from a window
- Size single-aspect dwellings to be no more than 8m deep
- Orient single-aspect dwellings along an east/west direction to ensure access to daylight without excessive build-up of heat.



## TC-H4 RESIDENTIAL ENTRANCES AND CIRCULATION

Entrances to residential buildings are frequently used, functional spaces that also set the tone for a development. They should be safe, welcoming and convenient, with daily uses such as cycle storage and waste disposal close by.

Within fluvial flood risk areas, all entrances, shared or private, **must** have a universally accessible dry pedestrian evacuation route.



Entrance example – Podium Garden



### TC-H4a Shared Entrances

Shared residential entrances **must**:

- Be located with a front door onto the street for legibility and to activate the street.
- Have an accessible level access entrance area which is safe, welcoming, durable, well-lit and at least partially glazed onto the street.
- Have lifts and stairs within sight of the entrance area or clearly signposted.
- Have external windows for daylight and ventilation.
- Include facilities for deliveries that does not require giving access to the whole building

Shared entrances **should** locate a range of key uses close by.

- Secure residents' cycle parking, which may be accessed from the communal entrance lobby as long as there is a direct access off the lobby, or from a direct secure link to the street
- Waste and recycling stores should be located as close to residential entrances as possible, with an access to the street to enable possible collection directly from the store, rather than as a managed collection.
- Waste and recycling stores should not connect to the core internally as this poses a security risk and allows smells to enter the building.

### TC-H4b Private Entrances

Single-dwelling residential entrances **must**:

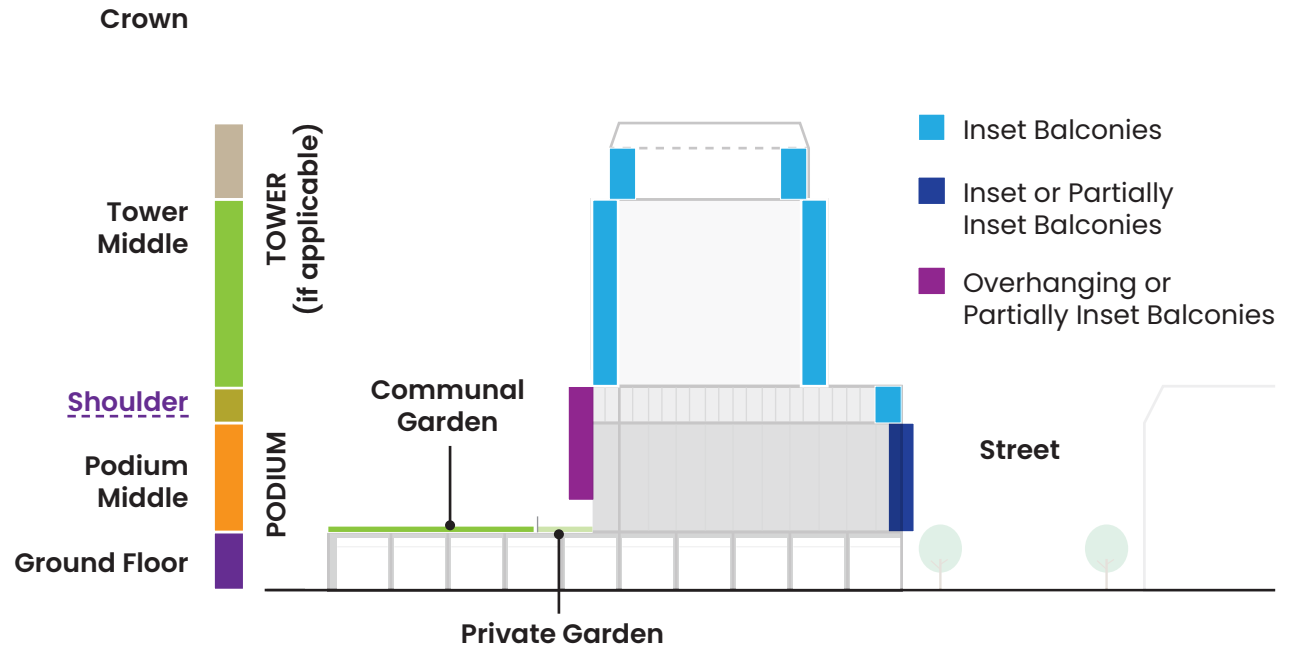
- Face the street or shared courtyard space for legibility and to activate the street/space.
- Be provided for ground-floor apartments and maisonettes from the street.
- Provide a sheltered, inset defensible space adjacent to the front door of at least 1m deep by 1.5m wide
- Include a covered space to store bins
- Include secure space to store cycles
- Be accessible to users of all abilities with a variety of mobility needs

## TC-H5 PRIVATE AMENITY SPACES

### TC-H5a Balconies

Balconies **must:**

- Be provided for all dwellings that do not have other forms of private outdoor space
- Have a minimum depth of 1500mm
- Have a minimum of 5m<sup>2</sup> of private outdoor space for all 2 person dwellings and an extra 1m<sup>2</sup> provided for each additional occupant.
- Have level access from a habitable room, ideally a living room or living area
- Comply with the acceptable locations for different forms of balcony set out on the right
- Where inset, be able to be at least partially closed from wind and rain
- Where overhanging, include a privacy screen between dwellings of 1.8m high



Private amenity space in town centre neighbourhoods may be provided by balconies (or roof terraces), or in private gardens, which may back onto communal gardens.

### TC-H5b Private Garden Space

Private garden spaces **must:**

- Directly adjoin and have level access from the dwelling's living area
- Have a minimum depth of 2m
- Have a minimum of 5m<sup>2</sup> of private outdoor space for all 2 person dwellings and an extra 1m<sup>2</sup> provided for each additional occupant.
- Be the same width of the dwelling it serves
- Be clearly identified by boundary treatments, including railings, low wall, a hedge
- Have a privacy screen between dwellings of up to 1.8m



Privacy screens on protruding balconies



Private gardens between dwellings and communal garden



## TC-H6 VEHICLE AND CYCLE PARKING

Living close to the town centre offers an opportunity to encourage active travel as a regular choice over frequent use of the car. In town centre neighbourhoods the aim should be to have fewer than one resident parking space per dwelling.

Car parking, where it needs to be provided, needs to be accommodated using an appropriate typology that limits the impact on the surrounding area and street scene.

All town centre development **must**:

- Include visitor cycle parking
- Include convenient and secure cycle parking for residents, at a provision level of 1 space per 1-2 bed dwelling or 2 spaces per 3+ bed dwelling
- Include car parking designed to the requirements for the appropriate typology as set out on the following page
- Provide at least 10% of car parking spaces as disabled spaces of at least 3.6m x 5.0m within 50m of the relevant building entrance
- Provide a fast EV charging point for each dwelling

All town centre development **should**:

- Identify space of at least 2.5m x 6.0m close to shared residential entrances for delivery vans to park and drop off items
- Identify cycle parking space close to retail units that may host cafes, restaurants and food takeaways for delivery cycles to park when picking up items

## TC-H6a Visitor Cycle Parking

Visitor cycle parking **must**:

- Be located close to entrances of residential and commercial buildings, or at accessible points within courtyards
- Be overlooked and well-lit
- Be covered when not on the street
- Be of typical Sheffield stand construction



Visitor cycle parking within the public realm



Visitor cycle parking within a residential courtyard

## TC-H6B Residents' Cycle Parking

Residents' cycle parking **must**:

- Be located close to entrances of residential and commercial buildings, or at accessible points within courtyards
- Be secure and lockable, with no visibility into the parking area from the street
- Be overlooked and well-lit
- Be enclosed, dry and protected from the weather
- Be more convenient to access for daily journeys than the car park
- Be accessible to users of all abilities with a variety of mobility needs



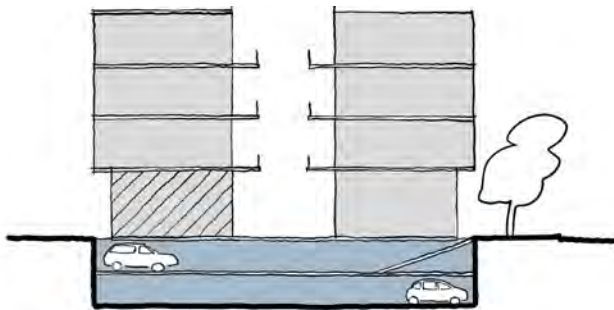
Secure cycle parking garage



### TC-H6c Underground Parking

Underground parking offers a space-saving option for town centre car parking. They **must**:

- Be well-lit, ideally with some natural light
- Be well-ventilated
- Not compromise the provision of high-quality trees and planting in communal gardens above them
- Be accessed from an entrance on a side or service street, rather than from a main street
- Have direct, secured access to internal circulation cores
- Have no negative impact on groundwater flows through an evidenced engineering solution, demonstrated as part of the planning application
- Be accessible to users of all abilities with a variety of mobility needs



### TC-H6d Podium Parking

Podium parking offers a flexible option for town centre car parking within higher-density developments. They **must**:

- Be well-lit, ideally with some natural light, and secure
- Be well-ventilated
- Not compromise the provision of high-quality trees and planting in communal gardens above them
- Be accessed from an entrance on a side or service street, rather than from a main street
- Have direct, secured access to internal circulation cores
- Have no negative impact on groundwater flows through an evidenced engineering solution, demonstrated as part of the planning application
- Be accessible to users of all abilities with a variety of mobility needs



### TC-H6e Integrated Parking



For typologies such as mews or terrace houses, integrated parking within the building can be a good option. Homes with integrated parking **must**:

- have no more than 50% of the frontage used for parking access
- at least one street-facing window on the ground floor to provide passive surveillance

### TC-H6f Surface or On-Street Parking

Surface or on-street car parking **must** only be used in very limited circumstances, for example to provide disabled, visitor or accessible parking spaces.

Any surface or on-street parking **must**:

- Incorporate trees or planting at least once every five parking spaces
- Be surfaced with permeable paving



Detail and Richness

Town centre buildings are part of the identity of a place. New buildings can complement their surroundings by being attractive, having visual richness at all scales and fitting with the materiality of the context.

DESIGN AIMS

Town Centre Neighbourhoods **will**:

- Be designed to enhance the townscape of Spelthorne’s places, providing interest, legibility and identity
- Have buildings with façades and elevations with richness, depth and detail, adopting an approach appropriate to the chosen architectural language of the building

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Rich, detailed building façades are popular with the community.

TC-D1 TOWNSCAPE

‘Townscape’ is a term that characterises the richness and quality of the built environment, and how it can be successfully tied together.

Town Centre Neighbourhoods will have a rich and attractive townscape. They **should** make use of features either seen in Spelthorne or that would be suitable for the town centre context.

Screened & Terminated Vistas



The end of a view is terminated by a marker building. Using trees to screen the vista increases the sense of distance.

Projection & Recession



A building line with bays, variation and intricacy rather than a single flat frontage.

Enclosure



Spaces which are surrounded by built form, providing a quieter ‘escape’ from adjacent busier streets that are more open.

Incident & Punctuation



Features breaking up the street view or interrupting the alignment of the street to create interest and separation.

Deflection



Views partially terminated by a building set at an angle, suggesting a further space round the corner.

Narrowing and Views Through



Differentiating between two spaces by emphasising the transition through constricting the width between the two.

## TC-D2 DISTINCTIVE BUILDINGS

The street environment **must** be easy to navigate for those who may be less familiar with it, and easy to remember for those who are. The arrangement and prominence of buildings relative to one another make a valuable contribution to the legibility, memorability and cohesiveness of the overall townscape of Spelthorne's town centres.

'Legibility' is the ability of people to 'read' a street environment to understand how to navigate a place successfully without resorting to signage or maps.

Two key types of building should be considered to aid legibility and townscape in important locations: **marker buildings** and **landmark buildings**.

Marker and landmark buildings will be important and long-lasting parts of the overall townscape and should be considered with care. The **design process** for such buildings **could** make use of:

- Architectural design competitions
- Design review panels that include community representatives

## TC-D2a Marker Buildings

Marker buildings are memorable buildings that stand out from the surrounding built form. They can help people to navigate and make the townscape more distinctive and interesting.

Marker buildings **should** be located:

- To terminate key views along streets
- At nodes, public spaces or meeting points
- To draw attention to key entrances or uses within an arrangement of buildings

Marker buildings **must**:

- Be of similar grain and dimensions to surrounding built form and complement the wider townscape.
- Be differentiated and distinctive from surrounding built form through the use of detailing, materiality, architectural treatment or orientation.



## TC-D2b Landmark Buildings

Landmark buildings are prominent buildings that are easily recognisable and have significant cultural or historical value.

Landmark buildings should be used sparingly in development.

Landmark buildings **should** be located:

- At major nodes or public spaces within a town centre
- As an anchoring focal point within new neighbourhoods, housing distinctive uses that define the new neighbourhood

Landmark buildings **must**:

- Include distinctive, town-wide uses of wider importance, not just residential use
- Be of distinctive and exceptional architectural quality, materiality and **massing** approach, differentiated from their surroundings



Landmark building in prominent location



## TC-D3 DESIGN OF ELEVATIONS

Buildings need to have variation, depth and texture on their elevations, as well as façades that are proportioned well and reflect features in the local context. This can be accomplished in traditional and contemporary ways.

Key elevation aspects for town centre neighbourhood buildings are:

- Overall facade composition
- Arrangement of windows
- Window detailing and reveal depths
- Treatment of balconies
- Corners of buildings
- Roofs and building tops

Example implementation of design code elevation requirements



String course detail to separate roof

Visually subservient top of the building, shorter window heights

Unified middle of elevation, prominent window surrounds

Textured and differentiated base

Boundary treatment matching overall materiality

## TC-D3a Bases, Middles and Tops

Buildings **must**:

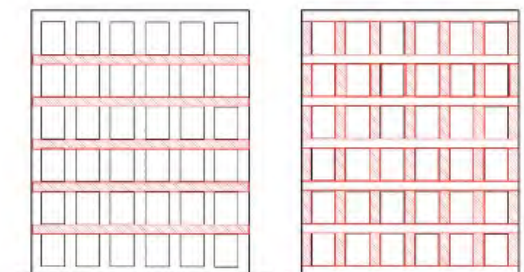
- Have a base, middle and top floors that are differentiated visually through the use of materials and proportional heights
- Have a base that is traditionally treated with a highly textured materiality, e.g. rough-hewn brick or other visually textured materials, of one or two storeys, with proportionally taller storey heights
- Treat each elevational aspect of the building according to its setting, which may differ across the same building



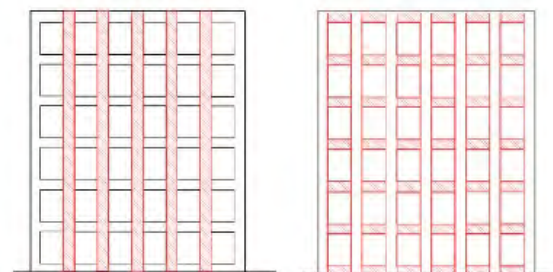
## TC-D3b Proportions

The overall composition of an elevation **must**:

- Have proportions informed by the general context of the site and the immediate surrounding environment. For example, buildings in a fine grained immediate context should utilise a tall, narrow, vertically proportion.
- Emphasise horizontal or vertical components of the facade to reflect context and grain of building
- Have component parts (**massing** volumes, doors, windows etc) that clearly relate to each other in terms of size, position and separation



Emphasis on horizontal components of facade, visually widening and shortening building



Emphasis on vertical components of facade, visually narrowing building

### TC-D3c Building Tops and Roofs

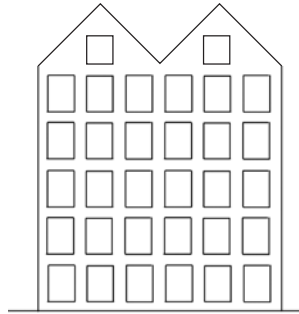
The roofscape has an effect on both long-distance views of the town centre and the perception of buildings from the street. They terminate the elevation vertically and balance the overall composition. They can include dwellings and building plant.

Buildings in town centre neighbourhoods **must**:

- Distinguish the top of the building from the rest of its facade using one of the roof types specified on this page
- Adopt a roof that reflects the grain of the building as specified
- Incorporate any building plant within the design of the roof, so it is hidden from the street and distance views

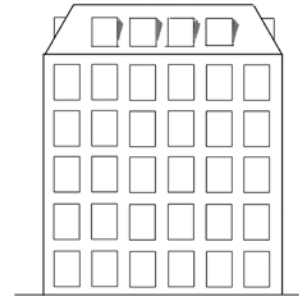
Buildings in town centre neighbourhoods **could**:

- Include roof terraces within setbacks
- Include balconies within gable end roofs



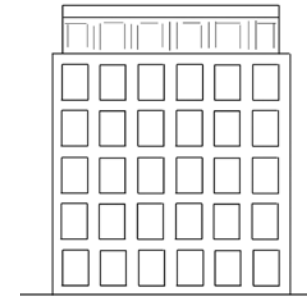
**Pitched gable-end roof** form potentially incorporating accommodation

Most appropriate for fine-grained buildings <10m wide



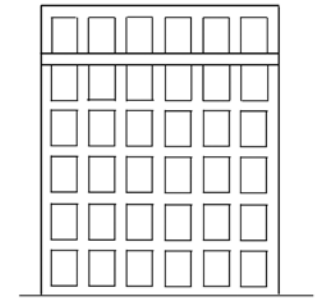
**Mansard roof** with or without dormers

Most appropriate for medium-grained buildings <20m wide



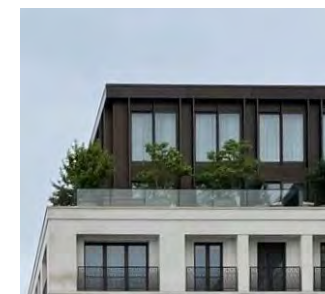
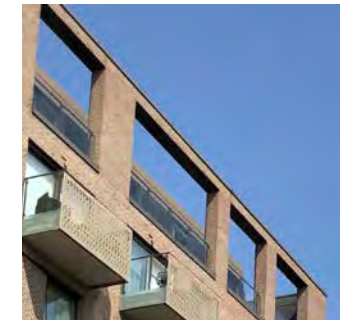
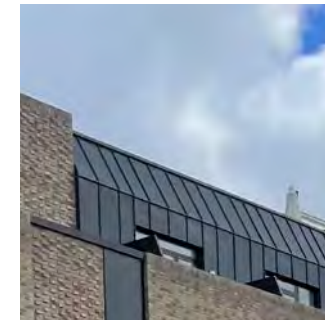
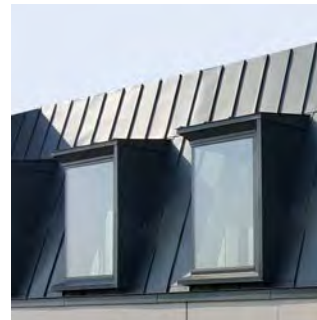
**Setting back of upper storeys**, with change in materials or window expression

Appropriate for coarser-grained buildings 15m+ wide



**Separating horizontal element** – cornice, string course, change of material or texture

Appropriate for coarser-grained buildings 15m+ wide



The skyline and tops of buildings are important to the community, especially when viewed from a distance.



### TC-D3d Balconies

Balconies for apartments have a significant effect on how the elevation and resulting street scene are perceived.

In addition to the dimensional requirements set out under **TC-H5**, detailed design of balconies **must**:

- Ensure adequate daylight levels within the home when inset or partially inset balconies are used
- Have edge treatments that balance privacy for occupants, views out, and also provide screening of furniture etc when viewed from the public realm.
- Ensure shading to windows below, assisting with the avoidance of overheating issues.
- Be clad to all sides including the underside, to maintain a high quality appearance from all aspects.



Use of rich, complementary materials to unify balconies to facade



Corner inset balconies



Use of privacy screen that also provides shading to apartment and balcony



Use of inset balconies to break up facade

### TC-D3e Corners

Buildings addressing street corners **must** have:

- Passive surveillance and **active frontages** facing both elevations

Street corners **could** include:

- Inset balconies
- Shared residential entrances
- Retail corner units
- Different material treatments to surrounding elevations
- A single storey rise in height
- A distinctive roof form



Differentiated corner



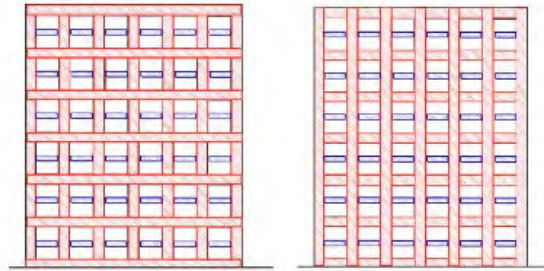
Rise in heights, distinct roof and corner entrance



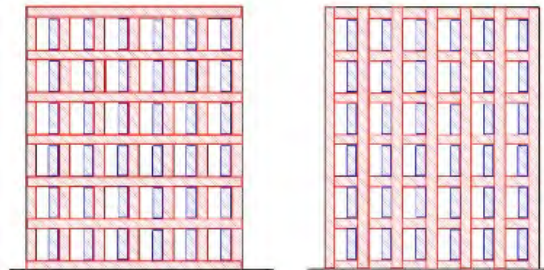
## TC-D3f Windows and Fenestration

Windows, window surrounds and other fenestration within an elevation **must**:

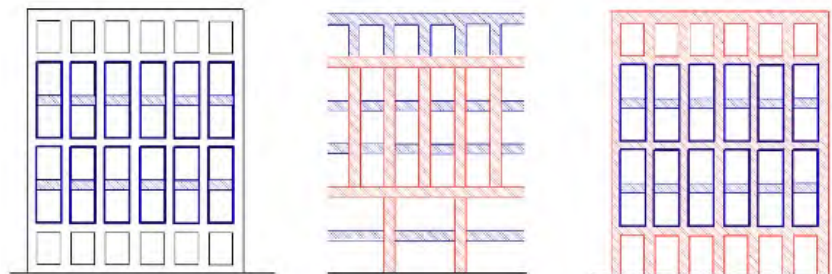
- Only use flush windows for ground floor retail
- Have other window reveals at least 75mm in depth to provide richness to the elevation, and deeper if the surrounding context has deeper depth of façades
- Balance the need for internal daylight penetration with thermal performance, overheating, privacy and views out.
- Have glazed coverage on façades that complies with the proportions set out in **TC-C1** under Climate Change and Sustainability. To achieve a well-proportioned facade, use of a secondary grid of features within overall window reveals can be used.



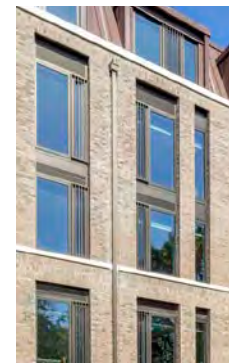
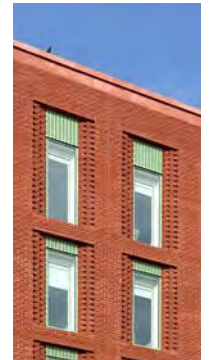
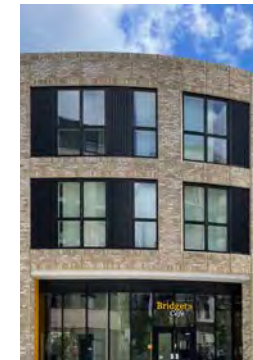
Secondary grid within window reveal to emphasise horizontal components of facade



Secondary grid within window reveal to emphasise vertical components of facade



Grouping of windows to reduce perceived height of building



## Climate Change & Sustainability

Town centre dwellings should be of the highest standards of environmental sustainability, in construction and operation. This includes mitigating their impact on climate change and the local environment, and also adapting to a future with more frequent and more extreme weather events.

Further guidance is set out in Spelthorne's *Climate Change Supplementary Planning Document (SPD)*.

### DESIGN AIMS

Climate Change & Sustainability in Town Centre Neighbourhoods will be ensured through:

- Layout and orientation to minimise energy needs by orienting for solar gain and passive ventilation
- Absorption and slowing of surface water runoff by the use of Sustainable Drainage Systems (SuDS) and achieving a high Urban Greening Factor (UGF)
- Demonstrating no impact to groundwater flows through an appropriate engineering approach



The Climate Emergency will particularly affect Spelthorne and high standards of sustainability are expected.

### TC-C1 MITIGATION: REDUCING ENERGY USE

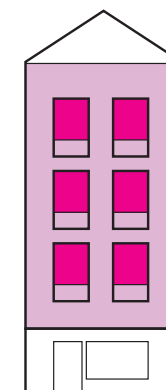
Building energy use is a significant contributor to carbon emissions.

New buildings **must**:

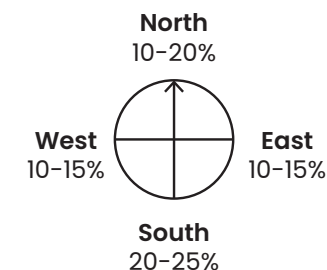
- Be heated by electricity, and not include gas boilers or other carbon-emitting heat sources
- Achieve a 31% reduction on the Dwelling Emission Rate (DER) against the Target Emission Rate (TER) based on the 2013 Edition of the Building Regulations (Part L), as per [Local Plan](#) policy PS1.
- Orientate buildings as much as possible within +/- 30° of a south-facing aspect to maximise solar gain and passive heating
- Include a form of shading on windows, ideally external, and ensure windows can be opened
- Target a window coverage for residential dwellings as set out on the right

New developments **should**:

- Include on-site photovoltaic (PV) energy generation where possible
- Use low-temperature heat networks powered by zero-carbon heat sources such as Air Source Heat Pumps or Ground Source Heat Pumps
- Use heat sources within or adjacent to the development such as supermarket fridges to augment heat network sources
- Locate Air Source Heat Pumps away from areas where the noise could cause nuisance to other users. When located on roofs, they should be enclosed within the roof design.



Window Area  
Wall Area = Coverage %



Flush photovoltaic panels incorporated into a roof



Energy Centre co-located with supermarket fridges and designed as marker building in street



## TC-C2 MITIGATION: REDUCING EMBODIED CARBON

Embodied carbon is the emissions generated by the construction of buildings.

New development **should**:

- Prioritise the re-use of existing buildings and parts of buildings, such as foundations, frames and other carbon-intensive components
- Use locally-sourced recycled materials
- Minimise the use of high-carbon materials such as aluminium, steel, glass and concrete
- Maximise the use of low-carbon and reusable materials such as brick, cross-laminated and soft timber



For more detailed technical guidance on reduction of embodied carbon, consult the LETI Climate Emergency Design Guide

## TC-C3 ADAPTATION: PREPARING FOR A CHANGING CLIMATE

Increased frequency of extreme weather events will mean:

- More intense storm events, with associated surface water runoff management requirements
- More intense heat events, which will particularly affect built-up areas

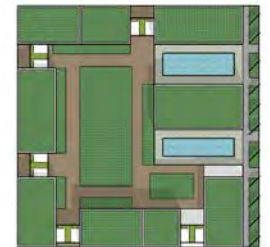
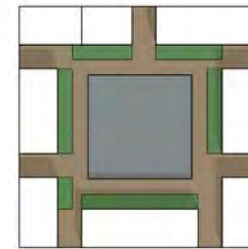
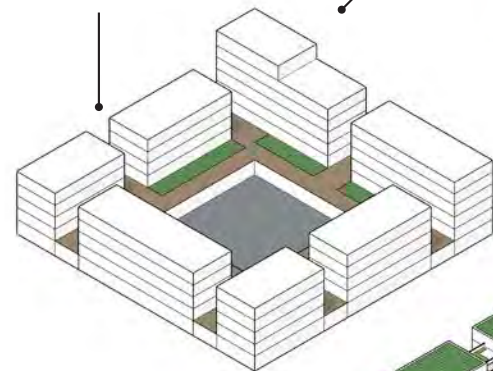
An increase in permeable surfaces, tree cover and planting can make a substantial contribution to mitigating these effects.

New development **must**:

- Achieve an Urban Greening Factor of at least 0.4, calculated using Natural England's Green Infrastructure Framework standards, through the use of green roofs and walls, planting and permeable surfacing, and urban sustainable drainage system features such as swales and rain gardens
- Provide an increase in tree canopy cover within the public realm when compared to the existing situation
- Include a safe pedestrian evacuation route from all circulation cores to a dry gathering area in the event of fluvial flood events
- Demonstrate no harm on groundwater flows from foundations or underground levels through an evidenced engineering solution

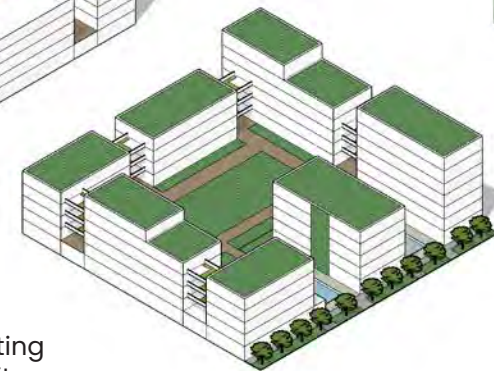
### Low Urban Greening Factor

Mostly impermeable surfaces



### High Urban Greening Factor

Green roofs / Green walls  
Street trees, swales and planting  
Rain gardens and water on site  
Permeable paving  
Shared, drained gardens on internal podiums





# Inner Suburban

## OVERVIEW

The Inner Suburban areas of Spelthorne were developed predominantly pre-WWI and in the early inter-war period. They have comparatively high densities compared to the rest of the borough, and are closely related to their nearby town centre.

These areas have a distinctive and replicable urban form, with tight gridded streets, mostly semi-detached homes on relatively narrow plots, and a variety of architecture along streets.



Intensification in Inner Suburban areas is supported but it must be done carefully, respecting the context.

## AREAS OF CHANGE

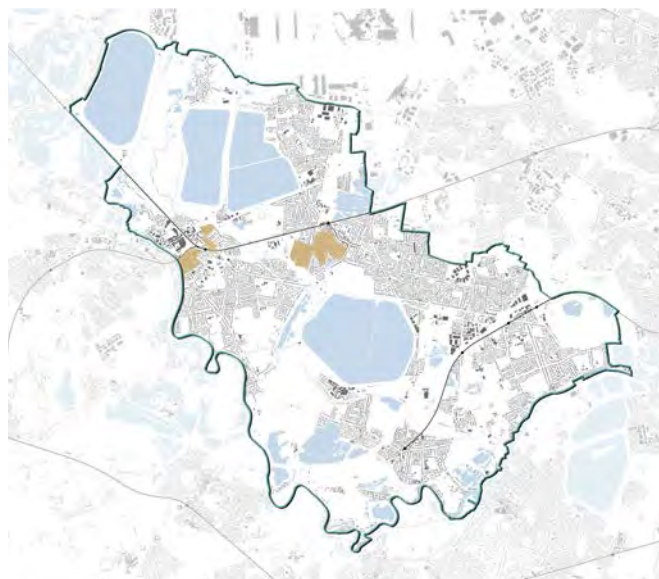
There are no areas of significant planned change in the Inner Suburban area type. Development is expected to be small-scale, incremental change governed by the codes for the Development Types above.

## DESIGN AIMS

Development in Inner Suburban areas will:

- Respect the existing street grid
- Reduce the visual and functional impact of car parking on the public realm
- Prioritise the walking and cycling potential of these areas
- Retain the rhythm and key dimensional characteristics of streets and buildings
- Sensitively intensify residential density without compromising the existing character of the area

## LOCATIONS



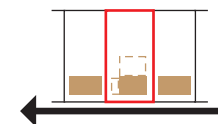
## WHAT CODE SHOULD I USE?

The design requirements you need to apply will depend on the type of development you are proposing.



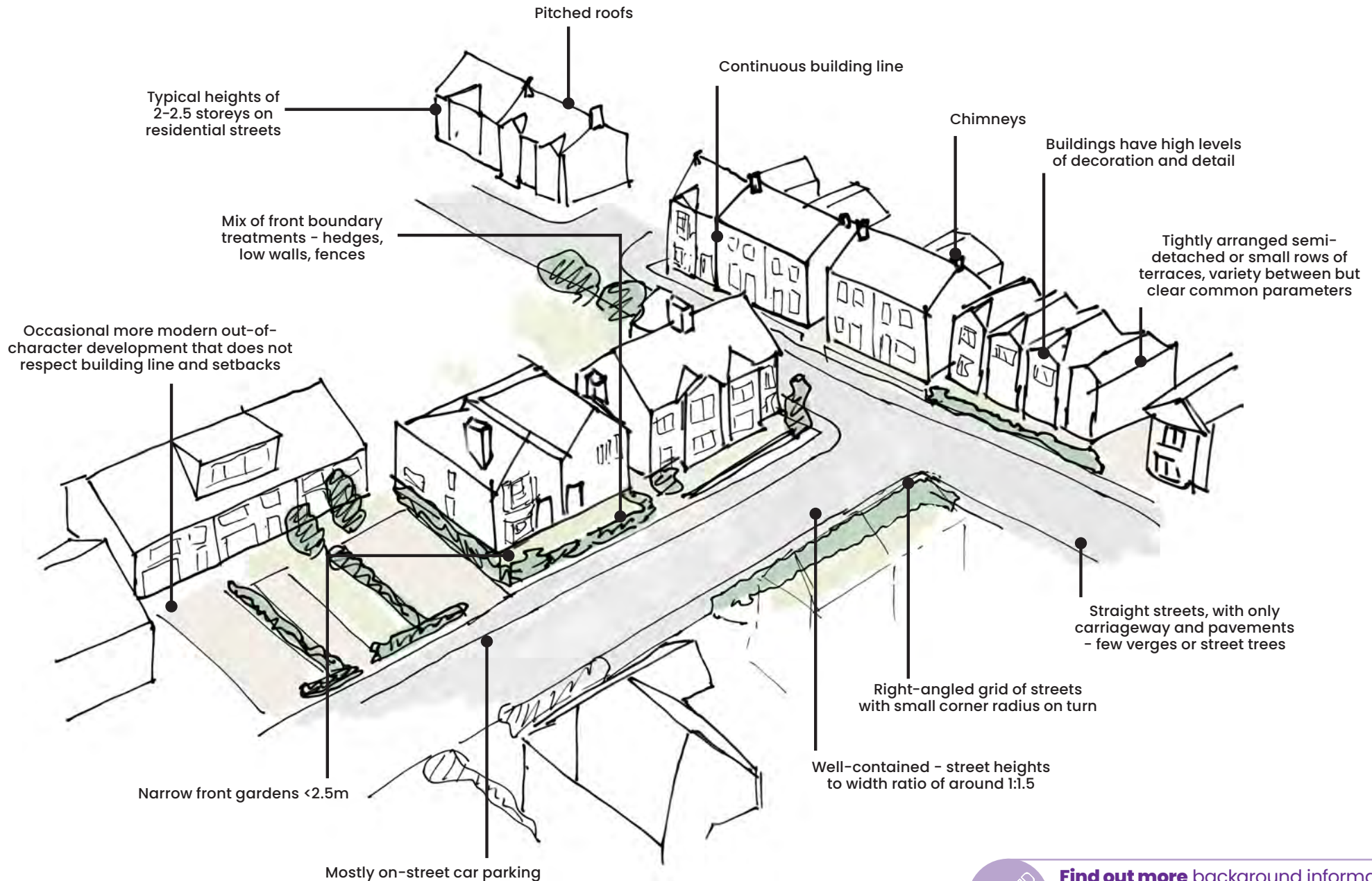
**New homes or apartments on existing streets**

**IS-A1 > IS-A8**



Key requirements for **Residential Extensions** are coded, with further guidance contained in Appendix C.

**IS-X1 > IS-X5**



Existing characteristics of inner suburban areas



**Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.

# New Homes or Apartments on Existing Streets

New apartments within existing Inner Suburban residential streets are a common form of development in Spelthorne. When designed well they can complement the existing street scene while delivering high quality new homes in locations close to High Streets and other facilities.

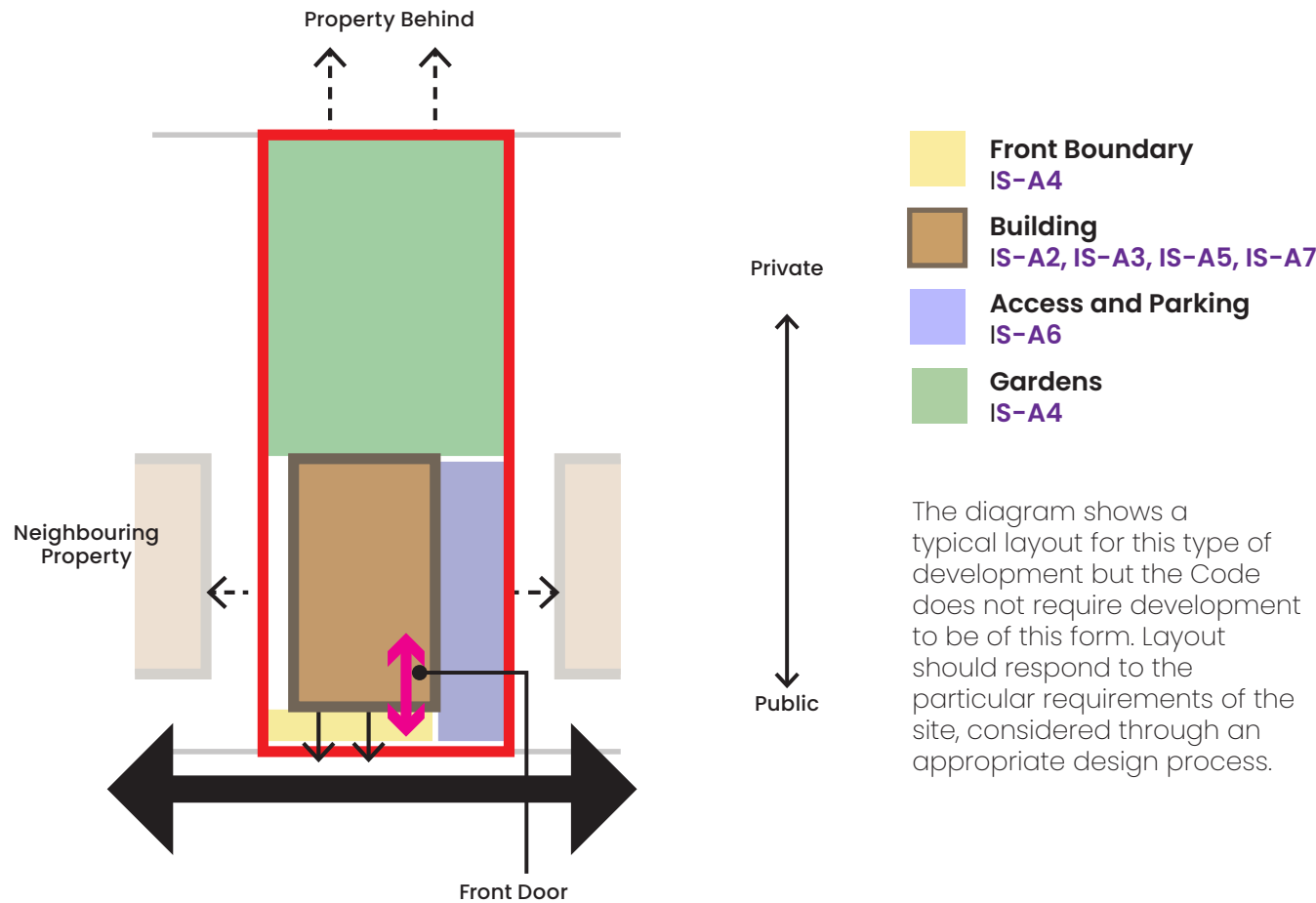
## DESIGN AIMS

All Inner Suburban apartment development on existing streets **will**:

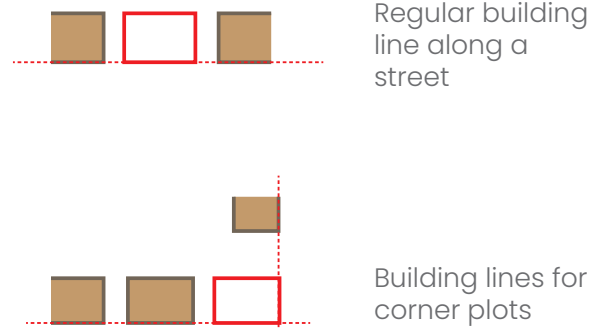
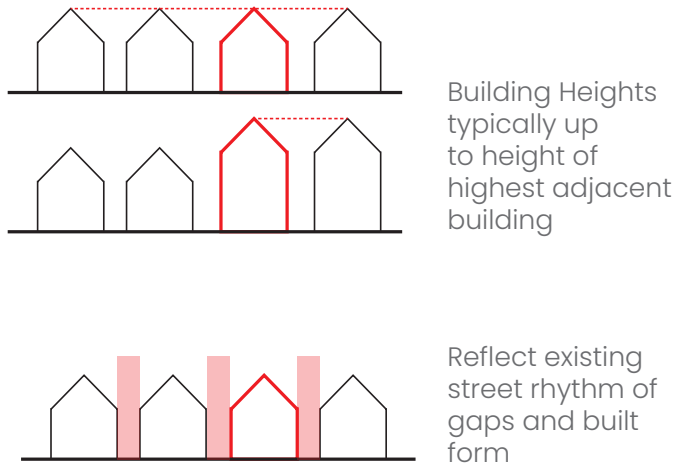
- Comply with Nationally Described Space Standards
- Address the needs of different design zones for street frontage, access, servicing and gardens
- Respect the existing street scene by observing the key design parameters, including:
  - the existing building line, rhythm of windows and separation distances, and the existing visual grain of the street
  - car parking placed to the rear of the built form, with vehicle access not dominating the frontage
  - adding any additional height sensitively
- Use materials and articulation to provide richness to the street scene

## IS-A1 LAYOUT PRINCIPLES

New development on existing streets in Inner Suburban Area Types **should** follow the overall layout principles set out below. Coding requirements for different areas are set out on the following pages.







Front boundary treatments in Inner Suburban areas are typically small front gardens with formal edges, such as railings, low walls or fences. Planting can be used to enhance the street scene and soften built form.



### IS-A2 BUILT FORM PARAMETERS

New development on existing streets **must** observe the following key built form parameters:

- Roofline up to height of highest immediately neighbouring building
- **Plot coverage** that is broadly within the range of the existing area, typically 30-50%
- Match neighbouring building line on streets with regular building line
- Sites on street corners to match the building line of both adjoining streets and provide passive surveillance to both aspects
- Reflect the existing street rhythm of gaps and built form



### IS-A3 ROOF FORM

New development on existing streets **must**:

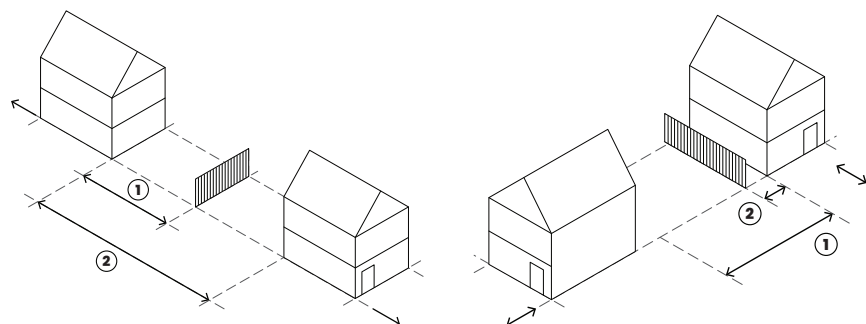
- Have pitched roof forms, reflective of surrounding prevailing form, e.g. gable ends or street-facing pitch
- Avoid flat roof forms facing streets on main roofs
- Ensure dormers are set in a minimum of 1m from the roof edge, down 0.5m from the ridge and up 1m from the eaves, and not be dominant and out of proportion
- Flat-roofed dormers facing the street may be acceptable if the overall architectural design language of the development is **contemporary**, otherwise they must incorporate a roof which is compatible with the main roof



### IS-A4 FRONT BOUNDARY TREATMENT

New development on existing streets **must** have:

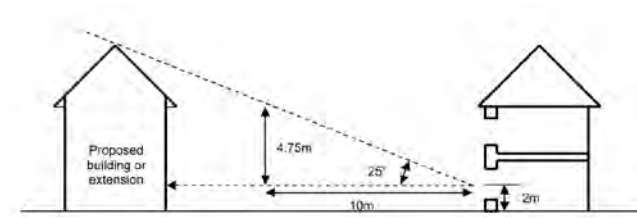
- A defined front boundary that separates public realm from private space
- A clear pedestrian path to the front door
- A boundary treatment such as a low wall or railings, making reference to prevailing styles on the street
- A front door that faces the street
- Sheltered, defensible threshold space at the front door of at least 0.5m x 1m
- An accessible covered space to store waste and recycling
- Accessibility to users of all abilities with a variety of mobility needs



Maintain appropriate distances to existing properties

Left: Back to Back 21m (30m for 3 storey) (distance 2 on left diagram)

Right: Back to Flank 13.5m (21m for 3 storey) (distance 1 on right diagram)



Ensure a 25° vertical line of sight to neighbouring properties to ensure daylight



### IS-A5 DAYLIGHT, PRIVACY AND OVERLOOKING

New development on existing streets **must**:

- Have a minimum back to back distance to properties at the rear of 21m (30m for 3 storey buildings)
- Ensure built form of two storeys or above is clear of a 45° line drawn from the centre of a habitable room in neighbouring properties, both horizontally and vertically
- Ensure a 25° vertical line of sight to neighbouring properties to ensure daylight
- Ensure a minimum back to flank distance 13.5m (21m for 3 storeys)
- Ensure a minimum boundary set-in distance 1m (2m for 3 storeys), or more to suit the context and prevailing street scene



### IS-A6 ACCESS, CYCLE AND VEHICLE PARKING

New development on existing streets **must** have:

- Secure and integrated cycle parking within building, e.g. for apartments within a circulation core on ground floor
- One of side, rear (shared) or integrated car parking.
- Brick paving or permeable gravel for car parking
- Planting and permeable surfaces within shared car parking areas (for apartments)
- Pedestrian access to rear gardens



### IS-A7 APARTMENT DEVELOPMENT

All new apartment development on existing streets **must** ensure:

- Dual aspect apartments are maximised
- Single aspect apartments are no deeper than 6m from an external window
- There are no single aspect apartments on north-facing aspects
- Balconies face the street and rear, avoiding balconies facing towards adjacent properties to sides
- That recessed or partially projecting balconies are used

Where no other private outdoor space is provided, balconies **must**:

- Have a minimum depth of 1500mm
- Have a minimum of 5m<sup>2</sup> of private outdoor space for all 2 person dwellings and an extra 1m<sup>2</sup> provided for each additional occupant.
- Have level access from a habitable room, ideally a living room or living area

## IS-A8 DETAIL, RICHNESS AND MATERIALITY

New buildings on existing streets **should** demonstrate how they have incorporated common features seen in Inner Suburban areas into their detailed design to enhance richness and variety in the street scene.



Roof line features



Bay windows and projections

To enhance the richness of the street scene, new buildings on existing streets **could** incorporate features such as sheltered seating with a boundary to the street, projecting bay windows (both traditional and contemporary in form), and integrated bin and cycle storage as part of the front garden.



Roof dormers



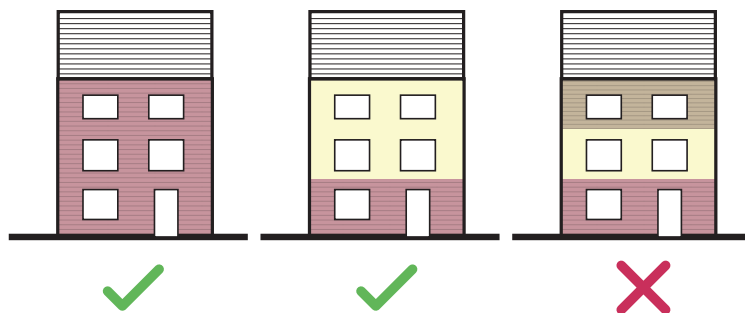
Gable ends



Changes in brickwork



Sheltered seating



New development on existing streets **must**:

- Use a single material for the elevation or;
- Have one clear change in materials between the ground floor and upper floors
- Use materials of high quality and long life, ideally with visible texture such as brick



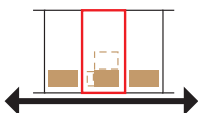
Projecting windows



Integrated bin and cycle storage



## Residential Extensions



### OVERVIEW

The key design considerations for residential extensions on existing plots in Inner Suburban areas are set out on this page.

All new residential extensions **must** comply with these requirements.

This section sets out an overview of the key dimensional requirements for residential extensions. More detailed guidance on design for this type of development is contained in Appendix C, drawn from the previously adopted *“Design of Residential Extensions and New Residential Development Supplementary Planning Document (SPD)” (April 2011)*.

### IS-X1 CONTEXT & CHARACTER

Designs **should** be mindful of key dimensions of the wider context that will ensure an extension fits within and complements that character of the area. These include:

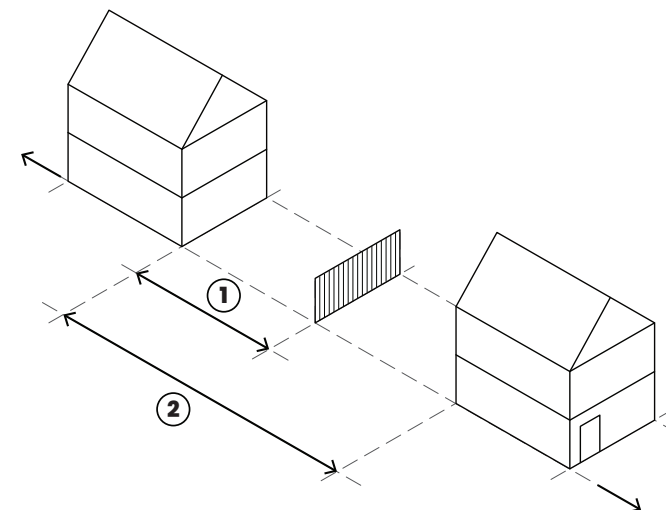
- Set-in distance: the distance from plot edge to the flank side of the building. It defines the characteristic width between properties along a street. Care should be taken to reflect the existing street scene.
- Set-back distance and prevailing building line

Minimum requirements for key dimensions are set out on these pages.

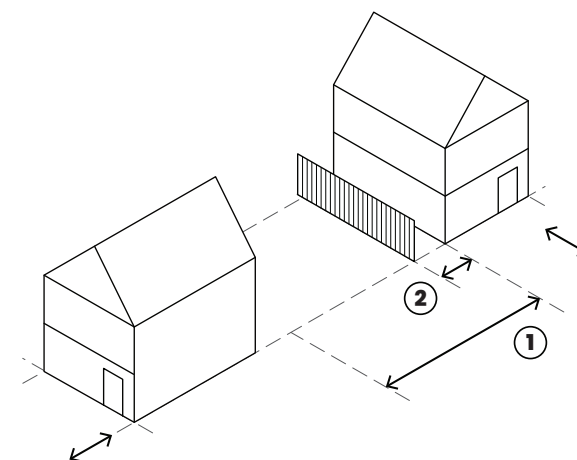
Key characteristics to observe that extensions **should** respond to in architectural design include:

- Prevailing materials of the area
- Prevailing roof forms and features
- Rhythm of windows and location of front doors on façades

### IS-X2 PRIVACY & OUTLOOK



1. Minimum garden length 10.5m (15m for 3 storeys)
2. Minimum back to back distance 21m (30m for 3 storeys)

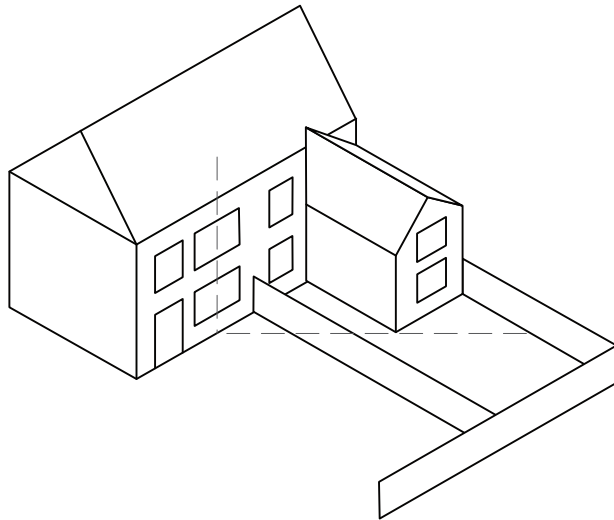


1. Minimum back to flank distance 13.5m (21m for 3 storeys)
2. Minimum boundary set-in distance 1m (2m for 3 storeys), or more to suit the context

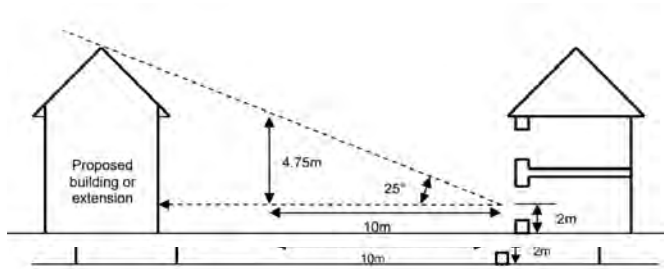


**Find out more** background information about the borough in Appendix A ‘Understanding Spelthorne Today’.

### IS-X3 DAYLIGHT

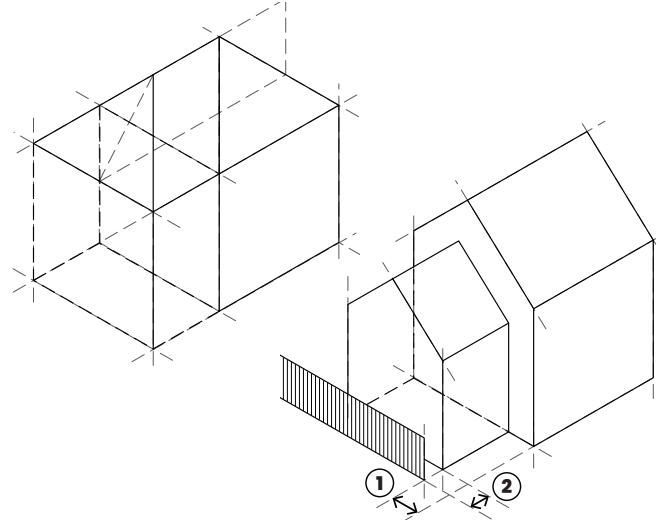


- Two-storey extensions **must** be clear of a 45° line drawn from the centre of a habitable room in neighbouring properties, both horizontally and vertically

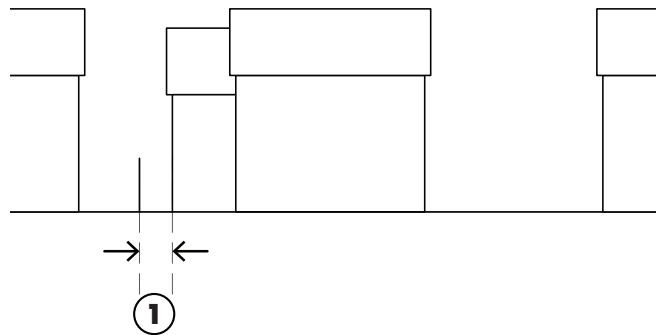


- Development **must** maintain a 25° vertical line of sight to neighbouring properties to ensure daylight

### IS-X4 SIDE EXTENSIONS

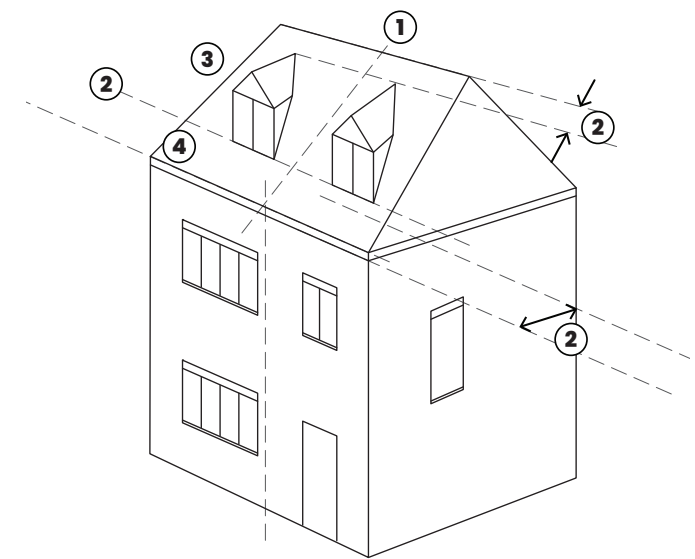


- Inline side extensions are acceptable
- Subordinate multi-storey side extensions **must** be set back by at least 1m (1) and set in from the plot boundary (2)



- Subordinate multi-storey side extensions **must** be set in from the plot boundary by a minimum of 1m (2m for 3 storey development), or more to suit the context

### IS-X5 DORMERS



Dormers **must**:

- Be located centrally or symmetrically on a roof
- Be set in a minimum of 1m from the roof edge, down 0.5m from the ridge and up 1m from the eaves
- Incorporate a roof which is compatible with the main roof
- Not be dominant and out of proportion

# Suburban

## OVERVIEW

Spelthorne's post-war suburban areas, typically laid out in the period 1945-1970, are characterised by lower-density housing on a street grid. While semi-detached houses are most common; terraced and detached homes as well as bungalows are found across the borough.

Many of Spelthorne's suburban locations benefit from proximity to green spaces and larger plot sizes. There is often less distinctive identity between the areas. Because they tend to be further from their core town centres, they often host small local parades of shops at key nodes.

At the edge of existing built-up areas or within larger infill sites, new residential neighbourhoods are anticipated of between 15-200 homes at a range of potential densities, with a mix of homes or apartments.

 New residential neighbourhoods must provide well-designed homes that are integrated into their surroundings.

## AREAS OF CHANGE

Development in existing suburbs is expected to be small-scale, incremental change governed by the codes for the relevant Development Types.

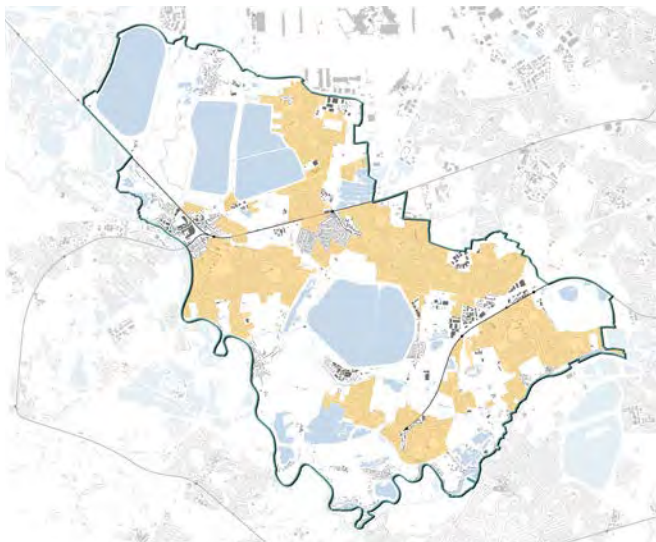
Development of New Residential Neighbourhoods will be a change in character and is governed by the Coding set out in this chapter.

## DESIGN AIMS

### Development in Suburban Areas will

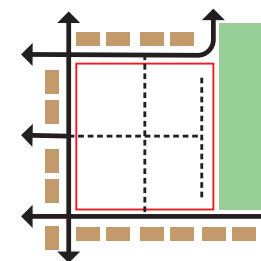
- Integrate new development into existing places positively
- Create new residential neighbourhoods with green spaces and attractive streets
- Maximise opportunities for green infrastructure on street and frontages
- Prioritise walking and cycling potential
- Retain the rhythm and key dimensional characteristics of streets
- Take opportunities to sensitively intensify residential density without compromising the existing character of the area

## LOCATIONS



## WHAT CODE SHOULD I USE?

The design requirements you need to apply will depend on the type of development you are proposing.



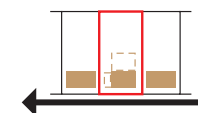
**New Residential Neighbourhoods**, either on the edge of the existing built up area or as larger sites within the existing built-up area.

**S-U1 > S-U6**



**New buildings on existing streets**

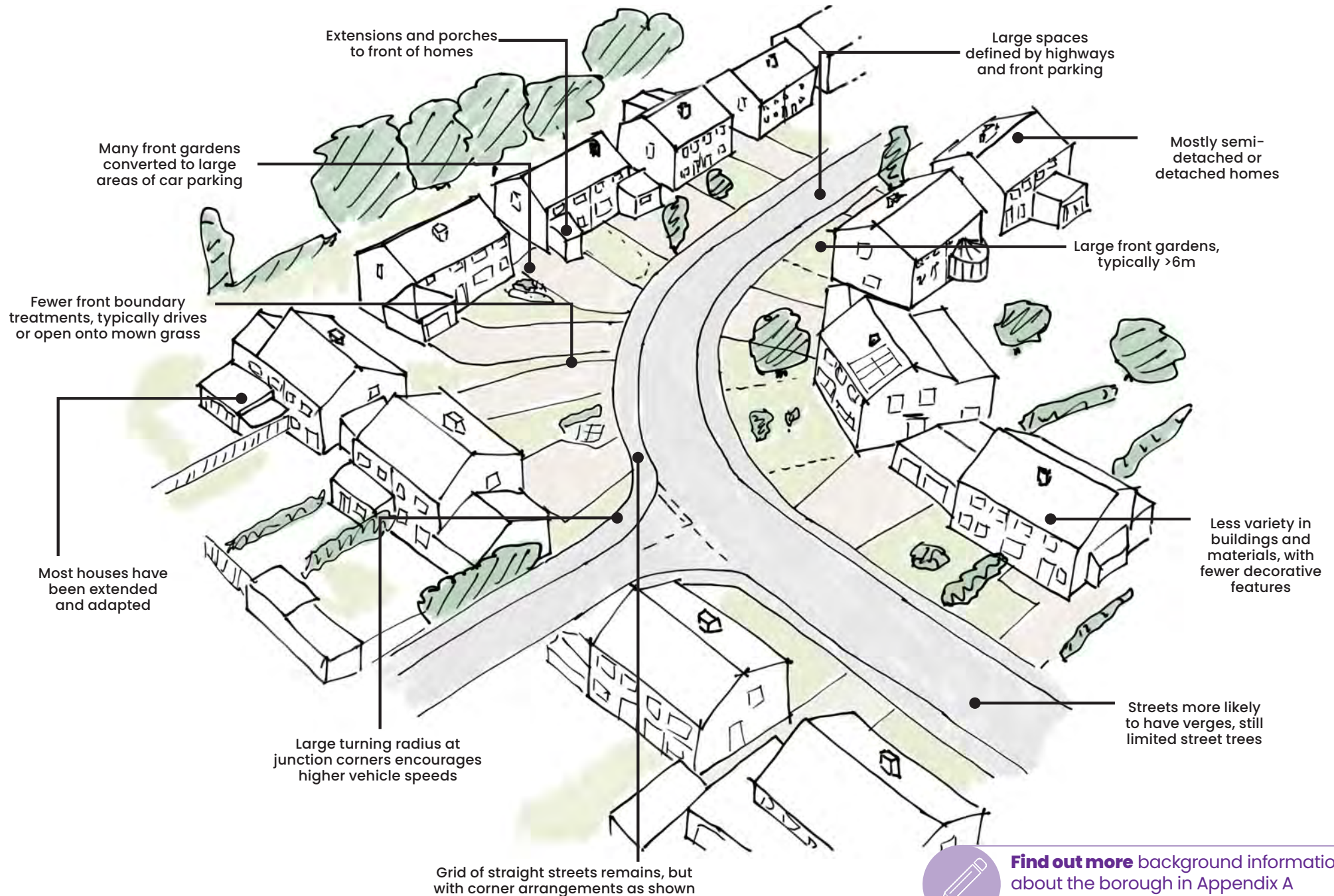
**S-A1 > S-A8**



Key requirements for **Residential Extensions** are coded.

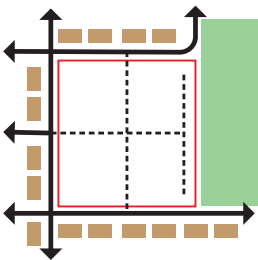
**S-X1 > S-X3**





Existing characteristics of suburban areas

## New Residential Neighbourhoods



Coding for larger areas of residential development in, or on the edge of the existing suburban area, is set out in this section. This may include:

- Allocated small sites that have been released from the Green Belt as part of the **Local Plan**.

- Other sites adjacent to existing built-up area
- Larger infill sites within existing built-up area

It is anticipated this coding is to be applied typically for developments of between around 15–200 homes.

New residential neighbourhood sites that are not allocated in the **Local Plan** **must** also comply with the requirements set out in this section.

### DESIGN AIMS

All new residential neighbourhoods will:

- Be inspired by and reflective of the place
- Integrate with and complement their surrounding areas through the consideration of edges and looking beyond the site boundary
- Have legible, connected streets that prioritise walking and cycling, and with car parking integrated so that cars do not dominate the street scene
- Have public green open spaces that are safe, well-managed, ecologically rich and complementary in scale and design to the surrounding built form

## S-U1 ENSURING DISTINCTIVENESS

New residential neighbourhoods will be clearly inspired and influenced by their surroundings.

New residential neighbourhoods **should** demonstrate as part of the **design process** how they have observed, studied and responded to:

- Typical local block structure, dimensions and grain of built form
- Scale, character, use and built form enclosure of local open spaces
- Typical materials and architectural features such as roof forms and elevational treatments used in the local area
- Distinctive local landscape features (e.g. Spelthorne’s distinctive cedar trees)
- Historic uses and users of the site and context
- Absences from the local area that could enhance it (e.g. a need for more play provision, different approaches to housing provision, open space, food production or access to nature)

**Find out more** background information about the borough in Appendix A ‘Understanding Spelthorne Today’.

Safe, well-managed car parking approaches are an important part of what makes places successful.

## S-U2 EDGES

On sites extending the existing built up area, how edges are considered will make a significant difference to how the scheme integrates with its surroundings. Addressing edge conditions successfully can:

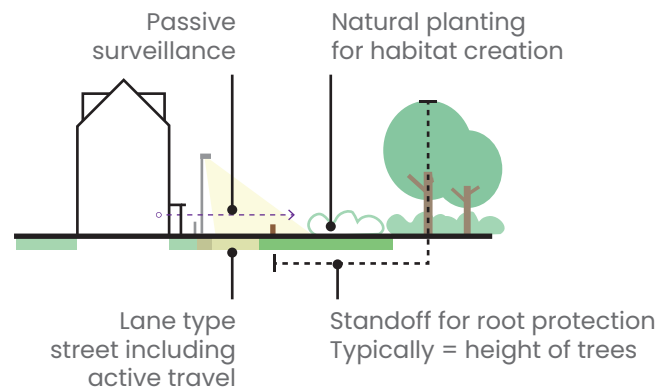
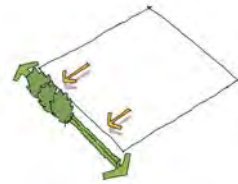
- Make new developments healthier by enabling new walking and cycling connections
- Integrate the scheme visually and physically so that in future it becomes a coherent part of the surroundings
- Reduce any impacts on surrounding buildings
- Minimise ongoing management issues
- Maximise site capacity by using land at the edges productively



Example green field development site west of Sunbury Cross with edges highlighted

## Edges: Green And Blue

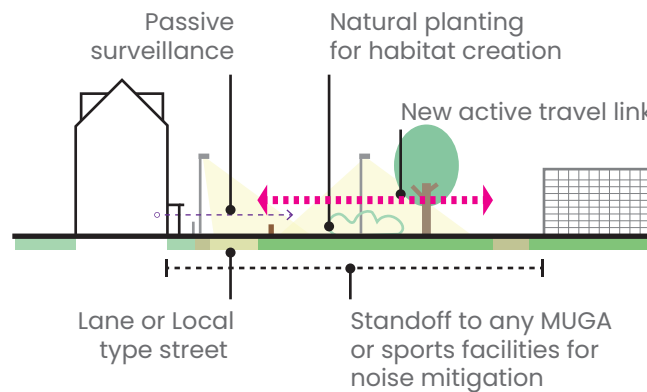
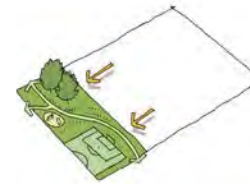
### S-U2a Trees, Woodland and Hedgerows



#### Development **must:**

- Retain and protect existing green infrastructure already on site
- Limit removal of existing green infrastructure to enable safe, overlooked access points
- Retain hedgerows and trees within public space, accessible for management
- Enhance existing hedgerows with additional diverse native planting
- Not place existing hedgerows or tree belts at the back of new properties. This will hinder access for future maintenance.

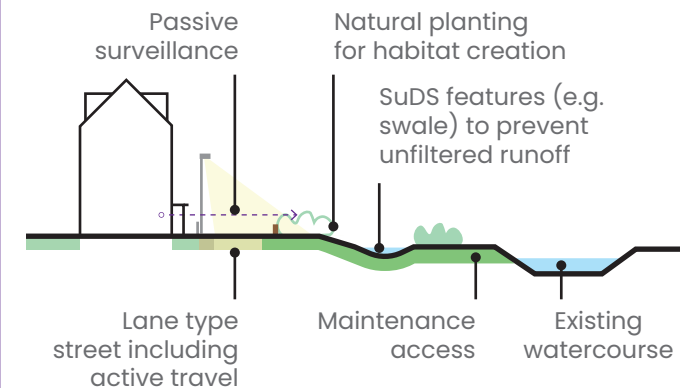
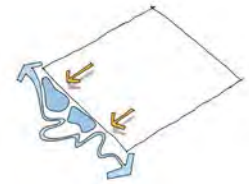
### S-U2b Open Spaces



#### Development **must:**

- Front new development to face towards existing open spaces
- Provide views towards existing open spaces from key nodes or spaces
- Connect open spaces to new development through active travel links, and provide new active travel links along the edge of existing open spaces with enough lighting to ensure safe use all year round
- Increase biodiversity by providing planted edges such as wildflower areas.

### S-U2c Watercourses and Water Bodies



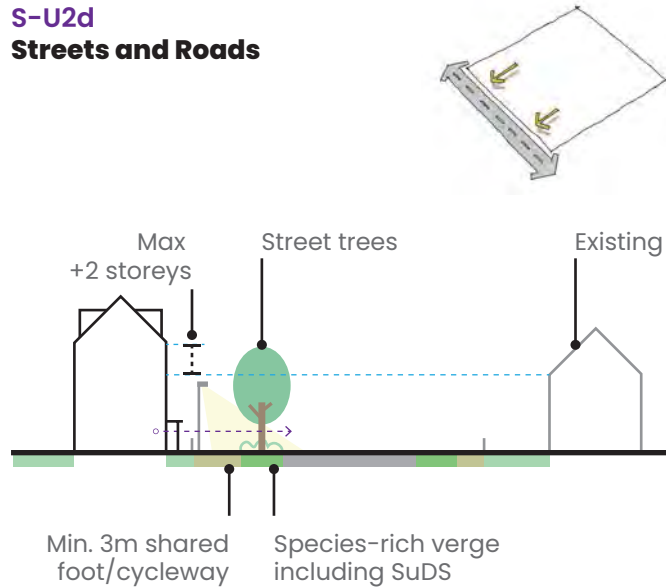
#### Development **must:**

- Provide new active travel links along the edge of existing watercourses with enough lighting to ensure safe use all year round
- Front development towards watercourses or drainage features so they are accessible for management
- Ensure surface water runoff is filtered by at least one stage of SuDS before entering the watercourse
- Increase biodiversity by providing planted edges such as wildflower areas, or integrated with SuDS features



## Edges: Transport And Movement

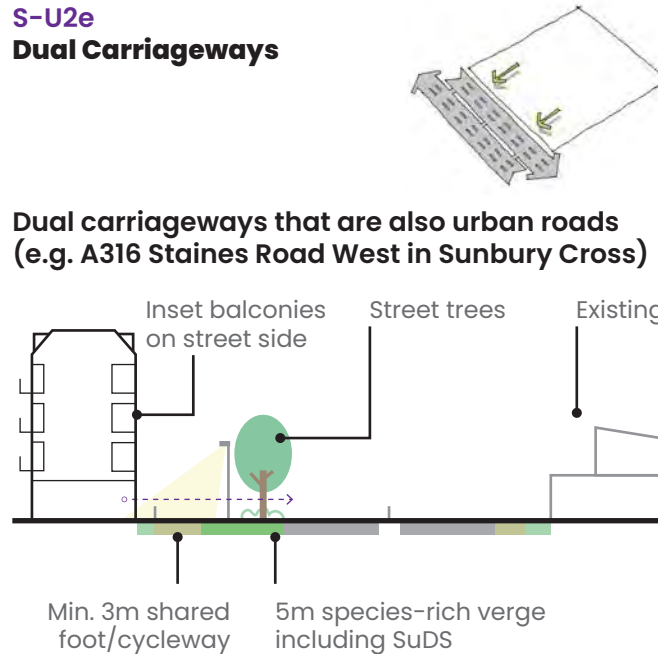
### S-U2d Streets and Roads



#### Development **must:**

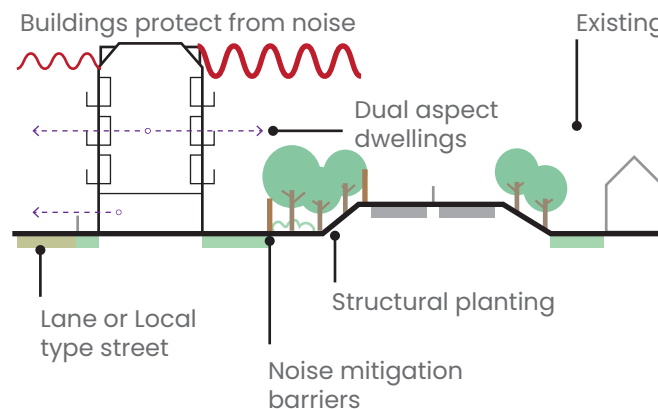
- Front new development towards existing streets and roads
- Set new development back in a way that respects the existing characteristic dimensions of the street or road
- Include street trees and planting along the street edge, and provide linear ecological habitats such as wildflower verges
- Improve pedestrian and cycling provision on existing roads adjacent to the site, potentially providing a new off-road connection through the site

### S-U2e Dual Carriageways

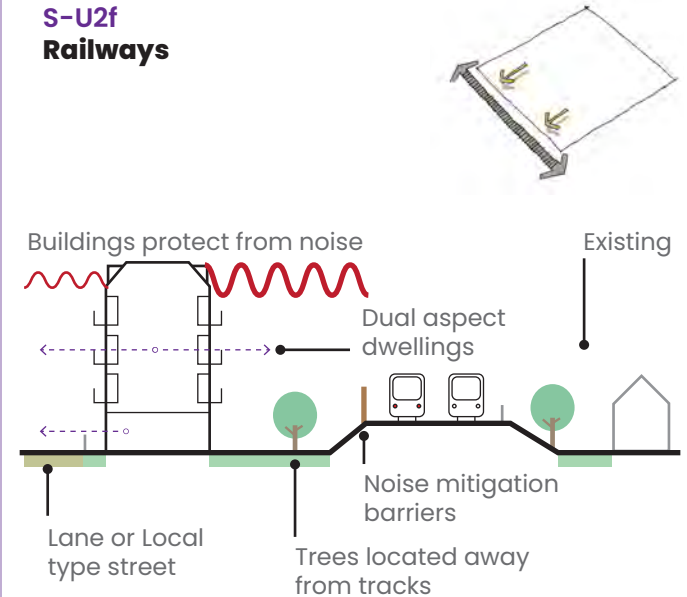


#### Dual carriageways that are also urban roads (e.g. A316 Staines Road West in Sunbury Cross)

#### Dual carriageways designed as bypasses (e.g. M3 and Upper Halliford bypass)



### S-U2f Railways

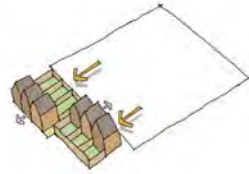


#### Development **must:**

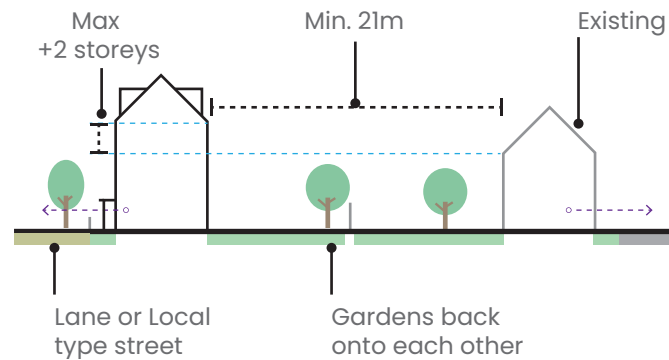
- Concentrate higher density development close to railway stations
- Provide lighting and overlooking towards any pedestrian footbridges or underpasses that enter or are adjacent to the site
- Place new tree planting at a sufficient distance from the railway tracks so as to not create autumnal leaf-fall impacts on the rail network
- Reduce access between development and the railway line, but where this is not possible or appropriate, ensure good lighting and passive surveillance to prevent anti-social behaviour

## Edges: Existing Built Form

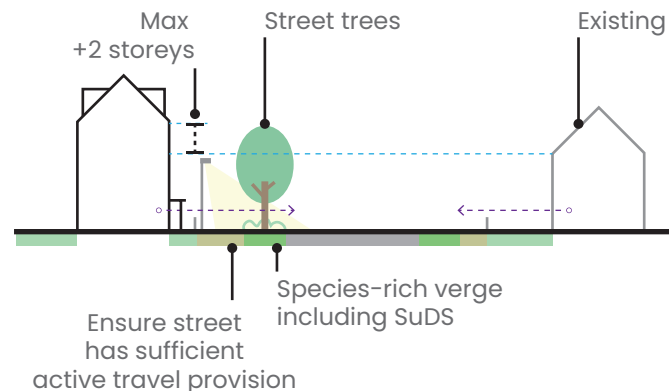
### S-U2g Residential (Backing onto and Facing onto)



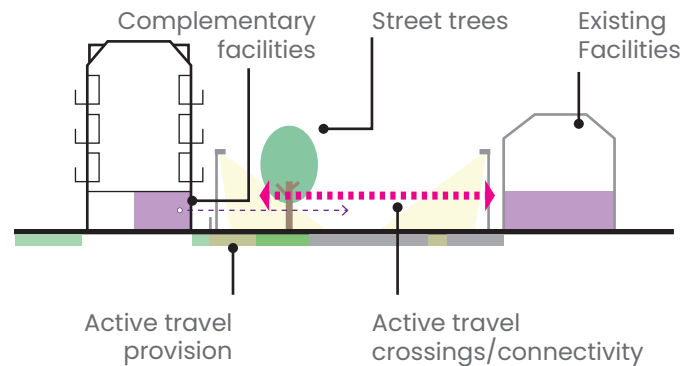
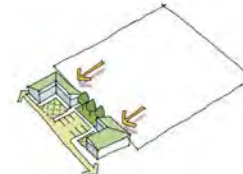
#### Existing Residential Backing onto the Site



#### Existing Residential Facing the Site across an Existing Street



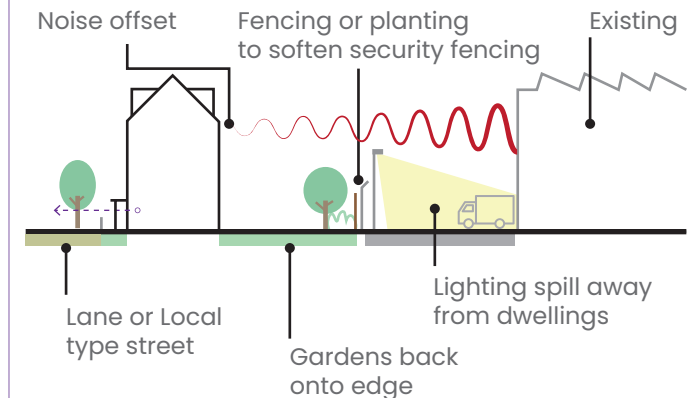
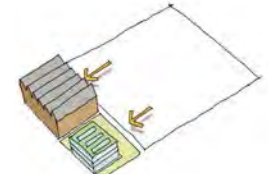
### S-U2h Local Facilities



#### Development **must**:

- Make walkable connections to nearby local facilities
- Co-locate any new facilities or uses adjacent to existing facilities
- Concentrate development density close to relevant local facilities, e.g. local shopping
- Prevent overspill parking near local facilities through the design of streets or enforced parking restrictions

### S-U2i Industry and Commercial Uses



#### Development **must**:

- Set new homes a sufficient distance from noise-emitting uses to ensure a maximum outdoor residential noise level of 55dB during the day and 45dB at night
- Provide screen planting to prevent industrial light sources from spilling into homes

## S-U3 MOVEMENT: LEGIBLE, CONNECTED STREETS



New streets will be designed in a way that provides a sense of place as well connectivity and accessibility to surrounding areas.

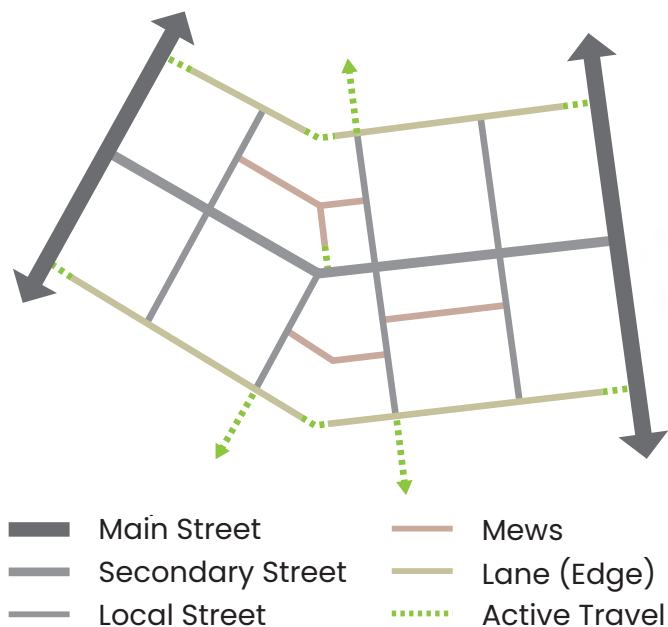
Streets will be designed around people, not vehicles. They should bring communities together and enhance their quality of life. Streets will be designed with flexibility and sustainability in mind, so that they will last for future generations.

Street types are determined by the importance of their place and movement functions, not their desired capacity or design speed. The decision on street typology is a collective decision with designers, planners, transport engineers and the local community. It must not be the sole decision of transport engineers.

Streets should be accessible to all abilities and ages through the use of drop kerbs, tactile paving, regular seating and clear sightlines and sufficient lighting for visibility and safety.

Street layout and design **must** be in compliance with the Surrey Healthy Streets Design Code.

## S-U3a Street Layout Approach



Street layouts **must**:

- Have a clear street hierarchy drawing on the types set out in this Code
- Create blocks of between 60-100m, with crossroads arrangements supported to align blocks
- Use filtered permeability, with active travel prioritised and having a continuous grid of routes, and private vehicles required to take more circuitous routes to access homes

## S-U3b Main Streets



Main streets **must**:

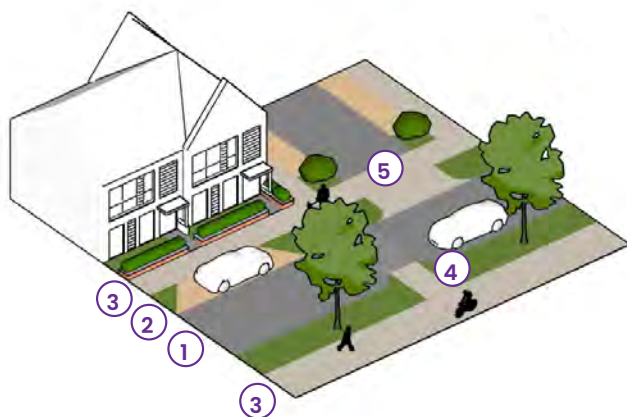
1. Have a carriageway between 5.5m and 6.5m
2. Have verges at least 2.5m wide, to incorporate street trees, planting and bus stop laybys
3. Have a footway on each side at least 2m wide, and a cycleway on each side at least 2m wide
4. Have continuous footways across junctions with streets lower in the hierarchy
5. Have a width:height enclosure ratio of between 2:1 (more urban) and 4:1 (minimum)

Where development fronts onto existing main streets, it **must**:

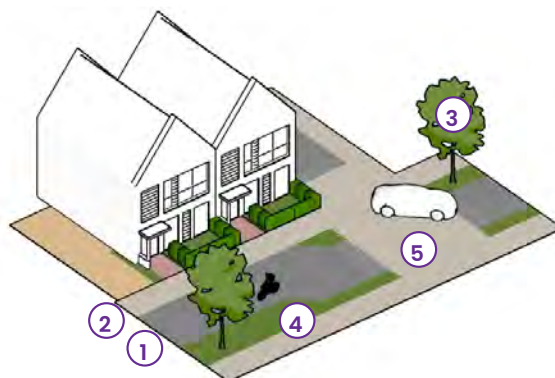
6. Safeguard sufficient land for future walking and cycling improvements
7. Align active travel links with existing or planned crossings



### S-U3c Secondary Streets



### S-U3d Local or Residential Streets



### S-U3e Mews and Lanes



Secondary streets **must:**

1. Have a carriageway of between 4.8m and 6.0m
2. Have verges of at least 2.5m wide, to incorporate street trees, planting and occasional on-street parking bays
3. Have a footway of at least 2m wide, and a shared footway/cycleway of least 3m wide
4. Include occasional build-outs for pedestrian crossings to slow vehicles
5. Have continuous footways across junctions with streets lower in the hierarchy
6. Have a width:height enclosure ratio of between 1:1 (ideal) and 3:1 (minimum)

Local or Residential streets **must:**

1. Have a carriageway of between 3.5m and 4.8m, with local widening to allow passing of vehicles
2. Have footways at least 2m wide
3. Be lined with street trees
4. Include occasional planting, rain garden and change in direction of the carriageway to slow vehicles
5. Use raised tables with brick paving at junctions with other Local or Mews Streets
6. Have a width:height enclosure ratio of between 1:1 (ideal) and 3:1 (minimum)

Mews and Lanes **must:**

1. Be a minimum of 6m wide
2. Be a shared surface for pedestrians and vehicles, of brick, paved or permeable paving construction
3. Include occasional planting, trees and features to slow vehicles
4. Include in-street drainage features such as rills and rain gardens
5. Have a width:height enclosure ratio of between 1:1 (ideal) and 2:1 (minimum)
6. Provide continuous active travel connectivity between edge lanes, with no gaps created between 'private drive' type arrangements

## S-U4 MOVEMENT: CAR PARKING



Developments must be designed around people not the car. When poorly-designed, car parking can have a significantly detrimental effect on the quality of a street and place.

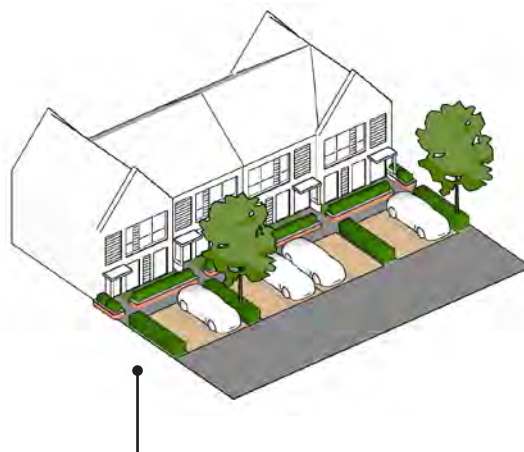
The parking typologies set out on this page are supported in New Residential Neighbourhoods in Spelthorne, provided they are designed in compliance with the Design Requirements.

### Vehicle parking **must:**

- Be provided at a level that complies with Surrey County Council's parking standards
- Have bay parking spaces at least 5.0m x 2.5m
- Have on-street parking spaces of at least 6.0m x 2.5m, with additional space on the ends of runs to allow for a kerb return
- Have at least 10% of parking spaces as disabled spaces, of 3.6m wide, located within 50m of the relevant building entrance
- Provide at least 0.2 visitor spaces per dwelling in on-street or otherwise unallocated spaces

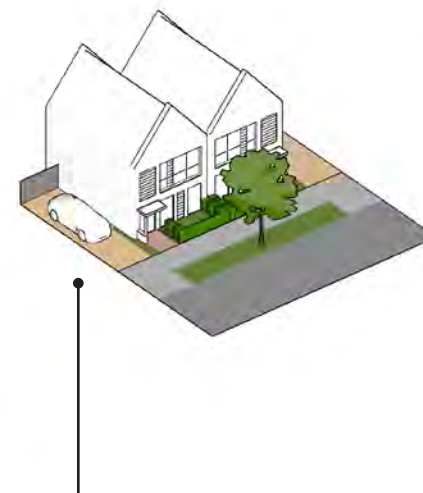
### S-U4a On-Plot Parking

On-plot parking **should** be used in lower-density areas of new residential neighbourhoods, typically of 35 dwellings per hectare or lower.



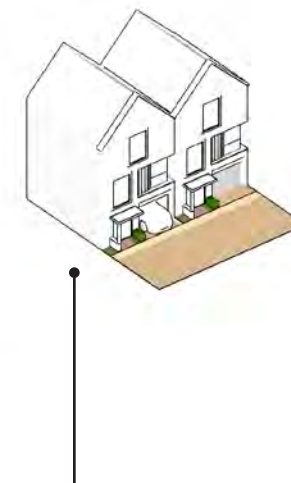
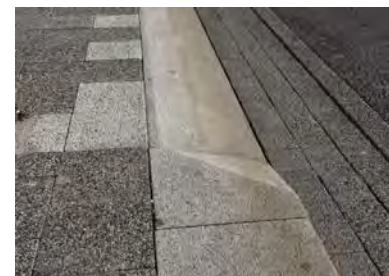
#### Frontage parking **must:**

- Have planting at least every four spaces in a run
- Include trees to soften views along longer runs of parking
- Be differentiated in surface material from the carriageway
- Retain footway or planted front garden area of least 1.5m behind the parking space



#### Side parking **must:**

- Be a minimum of 3.3m wide
- Maintain level footways and cycleways when accesses cross, using quadrant kerbs to provide a drop to the carriageway, as shown below



#### Integral parking **must:**

- Be a minimum of 6.0 x 3.0m internally
- Have a garage opening of at least 2.7m wide
- Have a garage door no more than 50% of the building frontage width
- Ensure a ground floor window is provided in addition to the front door and garage door

## S-U4b On-Street and Shared Parking

On-street parking and shared parking approaches **should** be used in higher-density areas of new residential neighbourhoods, typically between 40-75 dwellings per hectare.

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On-street parking **must**:

- Have runs of no more than four spaces
- Leave no unallocated space to prevent nuisance parking
- Have kerb returns of between 45° or 90°
- Be differentiated in surface material from the carriageway
- Be contained within verge/planted areas at the edge of carriageways, with planting or street trees at the ends of runs



Parking squares **must**:

- Have planting at least every four spaces in a run
- Must not exceed 12 spaces
- Include trees within some planted areas
- Be overlooked from surrounding dwellings
- Be differentiated in surface material from the surrounding streets, with permeable paving recommended



Rear parking courts **must**:

- Be overlooked from dwellings
- Be lit to provide security at all times of day. Lighting spread should be designed so as not to disturb neighbours. Motion-sensitive lighting is supported for its reduced energy use and impact on ecology
- Have planting or trees at least every five spaces in a run
- Be mostly surfaced with permeable paving
- Provide overlooked, safe access to apartment circulation cores via an overlooked route



Safe, well-managed car parking approaches are an important part of what makes places successful.



## S-U5 SAFE, ATTRACTIVE AND MULTIFUNCTIONAL OPEN SPACES



All development of New Residential Neighbourhoods in the Suburban area type **must** provide public open space at the levels, standards and accessibility specified in the latest Open Space Assessment, currently:

- Amenity Green Space: 0.6ha / 1000 people, within 480m of all homes
- Parks and Recreation Grounds: 0.8ha / 1000 people, within 400m of all homes
- Provision for Children and Teenagers: 0.1ha / 1000 people, within 400m of all homes
- Natural Green Space: 1.0ha / 1000 people for new development including amenity green space, within 500m of all homes
- Allotments: 0.25ha / 1000 people, within 800m of all homes

Additional Code requirements for two different types of open space are set in this section.

## S-U5a Meeting Points: Open Spaces Amongst Homes



Open spaces amongst homes **must**:

- Have high levels of enclosure from surrounding built form
- Have traffic calmed surrounding streets with a change in carriageway materials
- Be overlooked from surrounding homes
- Include sufficient lighting for safety
- Include cycle parking and seating
- Be accessible to and inclusive of all users

Open spaces amongst homes **could** include:

- Planting and habitat creation
- Traffic-free active travel links
- Rain garden and surface water management features
- Community garden and food production
- Small events space
- Childrens play features

## S-U5b Getting Outdoors: Open Spaces on the Edge of the Built-up Area



Open spaces at the edge of built-up areas **must**:

- A. Be overlooked from surrounding homes
- B. Have a transition in character from managed to natural, with uses such as play areas closer to homes
- C. Include sufficient lighting for safety on any active travel routes that pass through the space
- D. Include features such as bollards that prevent vehicles from entering or parking on the space from surrounding streets
- E. Be accessible to and inclusive of all users

Open spaces at the edge of built-up areas **could** include:

- 1. Natural habitat creation
- 2. Surface water management features that also function as natural habitats
- 3. Traffic-free active travel links and connections to surrounding open spaces and other destinations
- 4. Childrens play areas
- 5. Seating along footpaths
- 6. A distinctive built form edge with views across the open space, with the potential for taller heights to address the space



Being connected to the outdoors and wider green networks is an important part of living in Spelthorne.



## S-U6 LANDSCAPE CHARACTER

New residential neighbourhoods will blend built form with planting, soft landscape and green infrastructure, to create a softer, less formal environment than town centres and inner suburbs. There will be proportionally more soft landscape than hard landscape.

### S-U6a Hard Landscape

Hard landscape features will typically be within streets, including footways, cycleways and carriageways. It will also include incidental hard landscape features and squares within open spaces or at key nodes within the street network.



Brick paving can provide a cohesive and traffic-calmed environment on smaller residential streets and key nodes or junctions



Resin-bound gravel can provide an attractive and practical surface for informal leisure paths through open spaces

Material selection in the public realm **must** be in compliance with the Surrey Healthy Streets Design Code.

### S-U6b Soft Landscape

Soft landscape features play an important part in the quality of the built environment.

In more formal areas, such as busier streets and areas with more hard landscape, a more ornamental palette is appropriate.



A mix of grasses and low-maintenance evergreen species



Evergreen shrub species planted within verges to prevent verge parking



Including mown amenity grass ensures open spaces can be flexible for different uses

Closer to the edge of the built-up area and larger open spaces, a more informal mix of native species, including trees and hedgerows, **should** be used, maximising habitat creation opportunities.



Wildflower planting within verges or larger areas of open space



'Play on the way' features within retained mature tree corridor

Species selection **should** be varied to ensure resilience to climate change and invasive species.

Management and maintenance **should** be minimised where possible for most areas, saving more maintenance-intensive species for small areas of high impact.



### S-U6c Street Trees

All streets **must** be tree-lined. In general trees will be integrated within verges and as part of the street green infrastructure, as set out under S-U3 and S-U4. Suitable approaches include:



Lines of trees within verges



Trees installed to aid traffic calming features



Trees installed to break up frontage parking or parking squares

Trees **must** have sufficient space to grow and thrive, following guidance set out by the Trees Design Action Group (see reference in Chapter 6).

Using a variety of street tree species ensures resilience to climate change and invasive species.

### S-U6d Surface Water Drainage Features

All development **must** manage surface water through the use of Sustainable Drainage Systems (SuDS). Suitable design features include:



Source Control / Initial Absorption Features

- Street 'rain gardens'
- Planted verges and general soft landscape cover



Conveyancing Features

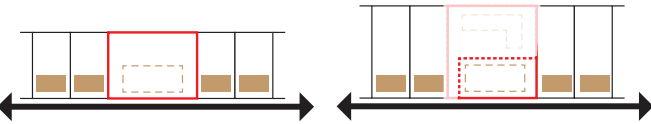
- Planted street swales
- Other overland flow features with minimal culverting or piping



Attenuation Features

- Surface attenuation basins, planted to create new habitats
- Attenuation ponds with permanent water

# New Homes or Apartments on Existing Streets



Development of new dwellings or apartments on plots on existing Suburban residential streets will be designed to complement the existing street scene and character of the area, while delivering high quality new homes throughout the borough.

In contrast to Inner Suburban locations, Suburban areas have more space between buildings and typically larger front garden areas, allowing more flexibility for the design of new development.

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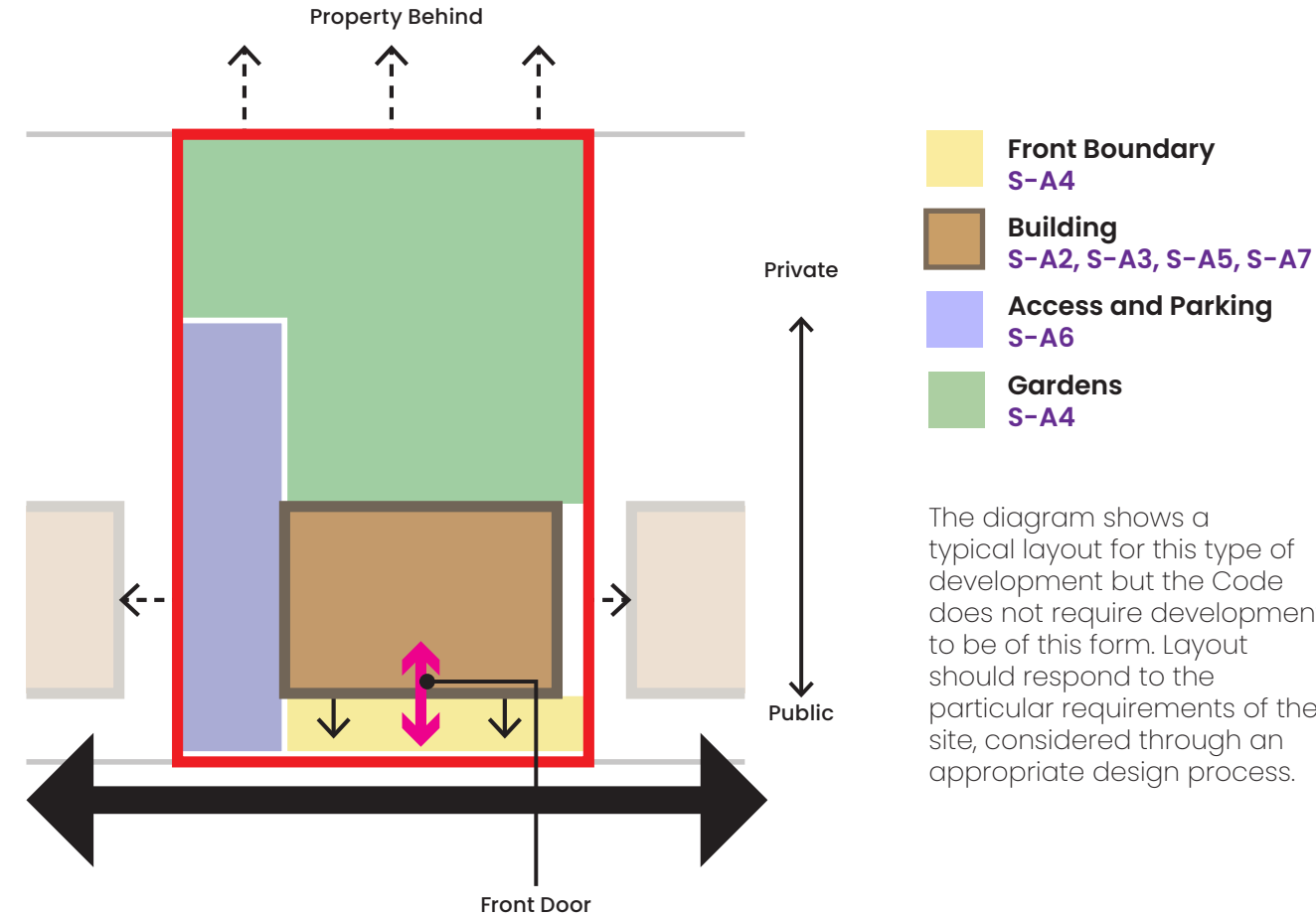
## DESIGN AIMS

All Suburban development on existing streets will:

- Comply with Nationally Described Space Standards
- Address the needs of different design zones for street frontage, access, servicing and gardens
- Respect the existing street scene by observing the key design parameters, including:
  - the existing building line, rhythm of windows and separation distances
  - car parking not dominating the frontage
  - respecting heights and scale of streets
- Use materials and articulation to provide richness to the street scene

## S-A1 LAYOUT PRINCIPLES

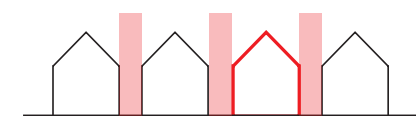
New development on existing streets in Suburban Area Types **should** follow the overall layout principles set out below. Coding requirements for different areas are set out on the following pages.







Building Heights typically up to height of highest adjacent building



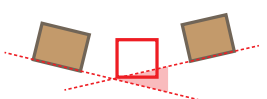
Reflect existing street rhythm of gaps and built form



Regular building line along a street



Building lines for corner plots



Irregular building line establishes zone for new building front



Front boundary treatments in suburban areas typically include planting and landscape to soften the built form and parking arrangements



## S-A2 BUILT FORM PARAMETERS

New development on existing streets **must** observe the following key built form parameters:

- Roofline up to height of highest immediately neighbouring building
- **Plot coverage** that is broadly within the range of the existing area, typically 25-40%
- Match neighbouring building line on streets with regular building line
- Where building line is irregular, use neighbouring buildings to establish zone for building line
- Sites on street corners to match the building line of both adjoining streets and provide passive surveillance to both aspects
- Reflect the existing street rhythm of gaps and built form



## S-A3 ROOF FORM

New development on existing streets **must**:

- Have pitched roof forms, reflective of surrounding prevailing form, e.g. gable ends or street-facing pitch
- Avoid flat roof forms facing streets on main roofs.
- Ensure dormers are set in a minimum of 1m from the roof edge, down 0.5m from the ridge and up 1m from the eaves, and not be dominant and out of proportion
- Flat-roofed dormers facing the street may be acceptable if the overall architectural design language of the development is **contemporary**, otherwise they must incorporate a roof which is compatible with the main roof

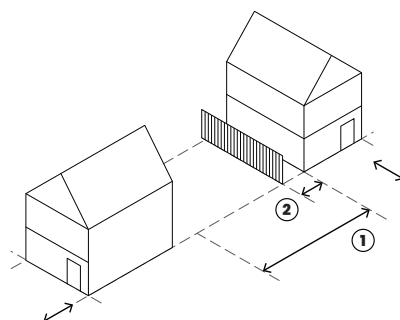
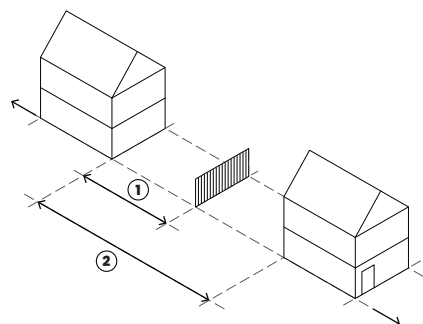


## S-A4 FRONT BOUNDARY TREATMENT

New development on existing streets **must** have:

- A defined front boundary that separates public realm from private space
- A clear pedestrian path to the front door, clearly defined from any frontage parking
- A boundary treatment such as a low wall, ornamental hedge or railings, making reference to prevailing styles on the street
- A front door that faces the street
- Sheltered, defensible threshold space at front door of at least 1m depth and 1.5m width
- An accessible covered space to store waste and recycling
- Accessibility to users of all abilities with a variety of mobility needs

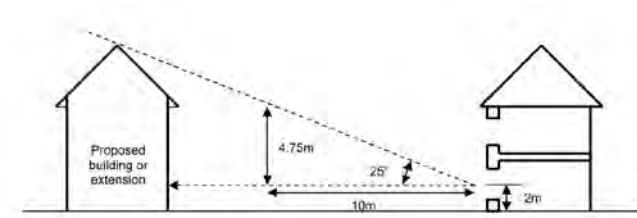




Maintain appropriate distances to existing properties

Left: Back to Back 21m (30m for 3 storey) (distance 2 on left diagram)

Right: Back to Flank 13.5m (21m for 3 storey) (distance 1 on right diagram)



Ensure a 25° vertical line of sight to neighbouring properties to ensure daylight



### S-A5 DAYLIGHT, PRIVACY AND OVERLOOKING

New development on existing streets **must**:

- Have a minimum back to back distance to properties at the rear of 21m (30m for 3 storey buildings)
- Ensure built form of two storeys or above is clear of a 45° line drawn from the centre of a habitable room in neighbouring properties, both horizontally and vertically
- Ensure a 25° vertical line of sight to neighbouring properties to ensure daylight
- Ensure a minimum back to flank distance 13.5m (21m for 3 storeys)
- Ensure a minimum boundary set-in distance 1m (2m for 3 storeys), or more to suit the context and prevailing street scene



### S-A6 ACCESS, CYCLE AND VEHICLE PARKING

New development on existing streets **must** have:

- Secure and integrated cycle parking within building, e.g. for apartments within a circulation core on ground floor
- One of side, rear (shared), integrated or frontage car parking
- Hardstanding for frontage car parking that occupies no more than 50% of frontage
- Brick paving or permeable gravel where car parking is on frontage
- Planting and permeable surfaces within shared car parking areas (for apartments)
- Pedestrian access to rear gardens



### S-A7 APARTMENT DEVELOPMENT

All new apartment development on existing streets **must** ensure:

- Dual aspect apartments are maximised
- Single aspect apartments are no deeper than 6m from an external window
- There are no single aspect apartments on north-facing aspects
- Balconies face the street and rear, avoiding balconies facing towards adjacent properties to sides
- That recessed or partially projecting balconies are used

Where no other private outdoor space is provided, balconies **must**:

- Have a minimum depth of 1500mm
- Have a minimum of 5m<sup>2</sup> of private outdoor space for all 2 person dwellings and an extra 1m<sup>2</sup> provided for each additional occupant.
- Have level access from a habitable room, ideally a living room or living area

## S-A8 DETAIL, RICHNESS AND MATERIALITY

New buildings on existing streets **should** demonstrate how they have incorporated common features seen in Suburban areas into their detailed design to enhance richness and variety in the street scene.



Softening built form with planting



Gable ends



Roof dormers



Hip roofs



Brickwork edge detailing

To enhance the richness of the street scene, new buildings on existing streets **could** incorporate features such as integrated garages and terraces, inset balconies for upper-floor apartments, and a variety of textures within elevation design.



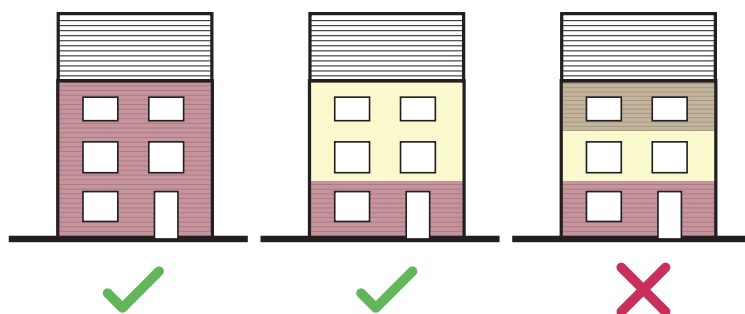
Integrated garages and roof terrace design



Inset balconies on maisonette upper floors



Variety of textures and finishes on facade



New development on existing streets **must**:

- Use a single material for the elevation or;
- Have one clear change in materials between the ground floor and upper floors
- Use materials of high quality and long life, ideally with visible texture such as brick

## Residential Extensions



### OVERVIEW

The key design considerations for residential extensions on existing plots in Suburban areas are set out on this page.

All new residential extensions **must** comply with these requirements.

This section sets out an overview of the key dimensional requirements for residential extensions. More detailed guidance on design for this type of development is contained in Appendix C, drawn from the previously adopted *“Design of Residential Extensions and New Residential Development Supplementary Planning Document (SPD)” (April 2011)*.

### S-X1 CONTEXT & CHARACTER

Designs **should** be mindful of key dimensions of the wider context that will ensure an extension fits within and complements that character of the area. These include:

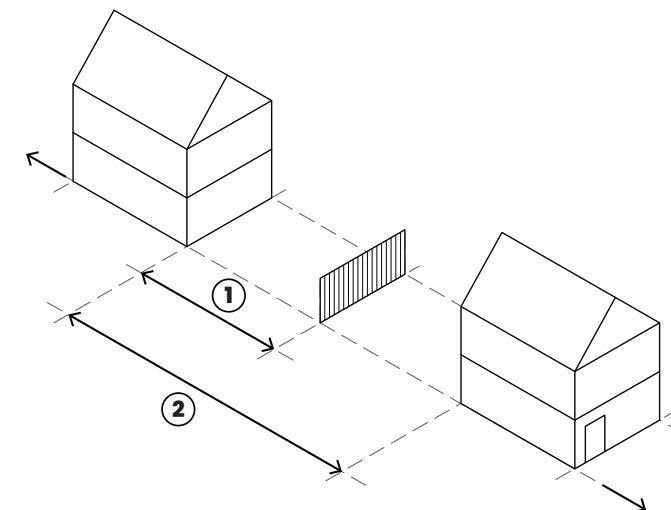
- Set-in distance: the distance from plot edge to the flank side of the building. It defines the characteristic width between properties along a street. Care should be taken to reflect the existing street scene.
- Set-back distance and prevailing building line

Minimum requirements for key dimensions are set out on these pages.

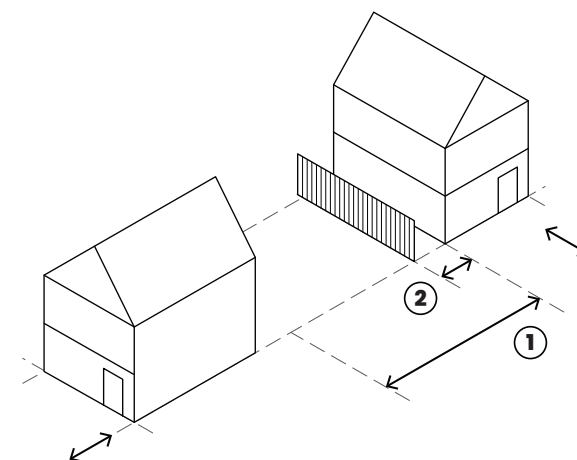
Key characteristics to observe that extensions **should** respond to in architectural design include:

- Prevailing materials of the area
- Prevailing roof forms and features
- Rhythm of windows and location of front doors on façades

### S-X2 PRIVACY & OUTLOOK



1. Minimum garden length 10.5m (15m for 3 storeys)
2. Minimum back to back distance 21m (30m for 3 storeys)



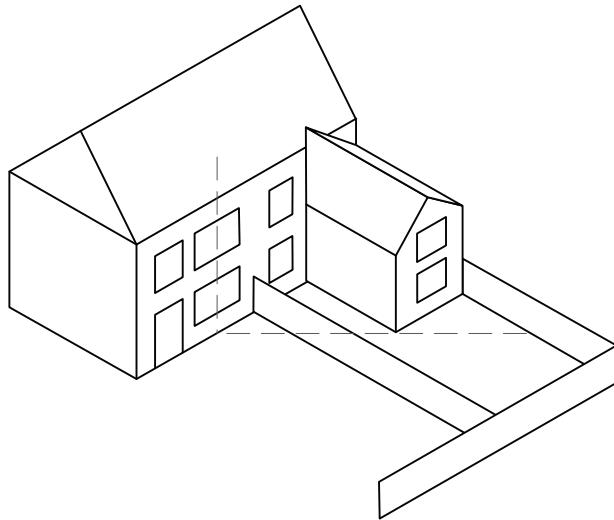
1. Minimum back to flank distance 13.5m (21m for 3 storeys)
2. Minimum boundary set-in distance 1m (2m for 3 storeys), or more to suit the context



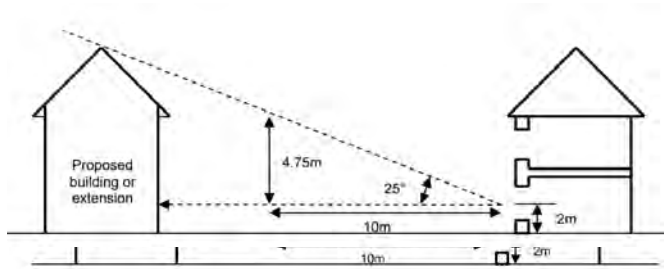
**Find out more** background information about the borough in Appendix A ‘Understanding Spelthorne Today’.



### S-X3 DAYLIGHT

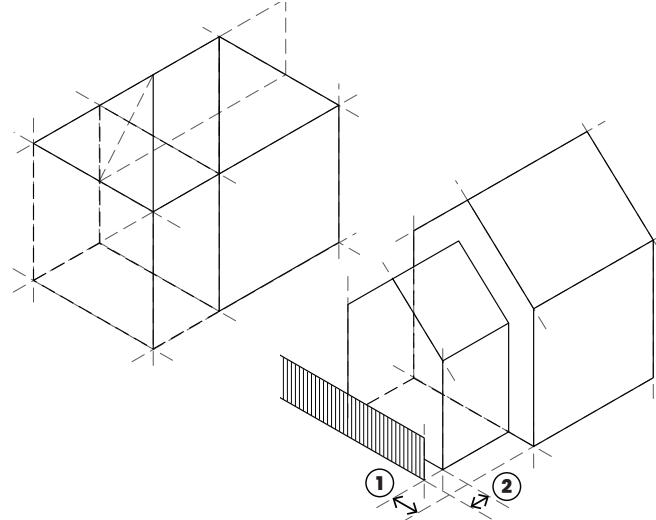


- Two-storey extensions **must** be clear of a 45° line drawn from the centre of a habitable room in neighbouring properties, both horizontally and vertically

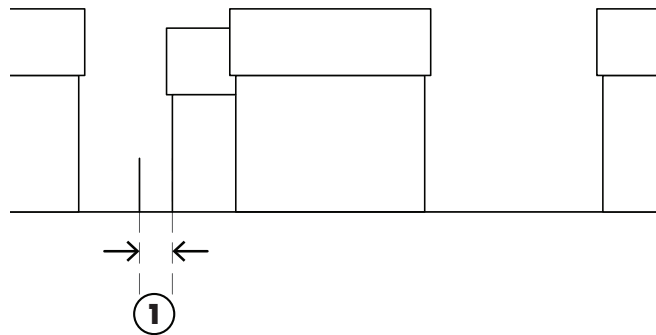


- Development **must** maintain a 25° vertical line of sight to neighbouring properties to ensure daylight

### S-X4 SIDE EXTENSIONS

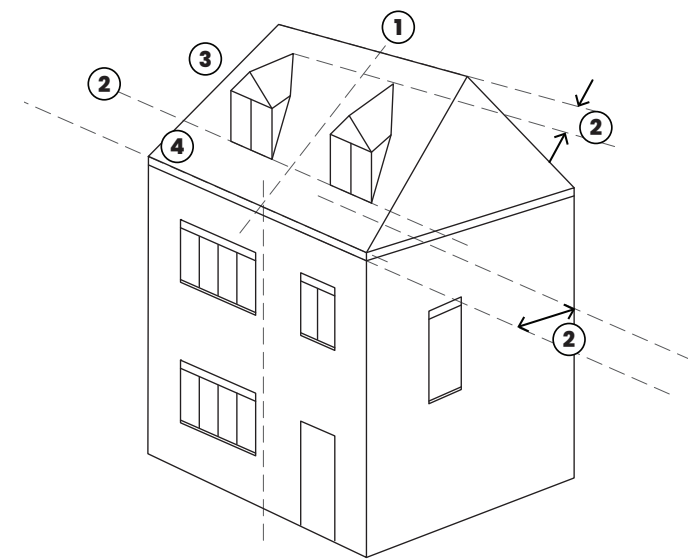


- Inline side extensions are acceptable
- Subordinate multi-storey side extensions **must** be set back by at least 1m (1) and set in from the plot boundary (2)



- Subordinate multi-storey side extensions **must** be set in from the plot boundary by a minimum of 1m (2m for 3 storey development), or more to suit the context

### S-X5 DORMERS



Dormers **must**:

- Be located centrally or symmetrically on a roof
- Be set in a minimum of 1m from the roof edge, down 0.5m from the ridge and up 1m from the eaves
- Incorporate a roof which is compatible with the main roof
- Not be dominant and out of proportion





5



# Areas of Change

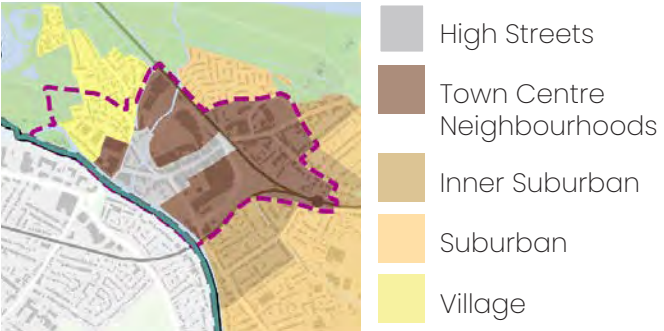


# Staines-upon-Thames Town Centre

## OVERVIEW

This section sets out detailed Design Requirements and guidance for development in Staines-upon-Thames town centre.

## EXTENT AND CONTEXT



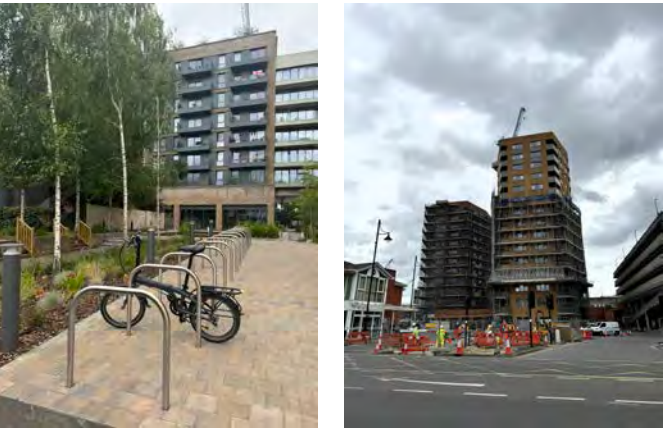
 **Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.

 **Find out more** about the Conservation Area in the Staines Village Conservation Area Appraisal (2023).

 A clear vision for town centre neighbourhoods and the future 'look' of the town centre is a community priority.

## DEVELOPMENT CONTEXT

Staines-upon-Thames town centre will see significant change in coming years. The [Local Plan](#) allocates around 3,500 new homes to be built, mostly at higher densities and resulting [floor area ratios](#) than the prevailing built form. The Design Code sets out the requirements for these to be delivered as part of coherent, well-designed Town Centre Neighbourhoods, that complement and enhance the existing High Street.



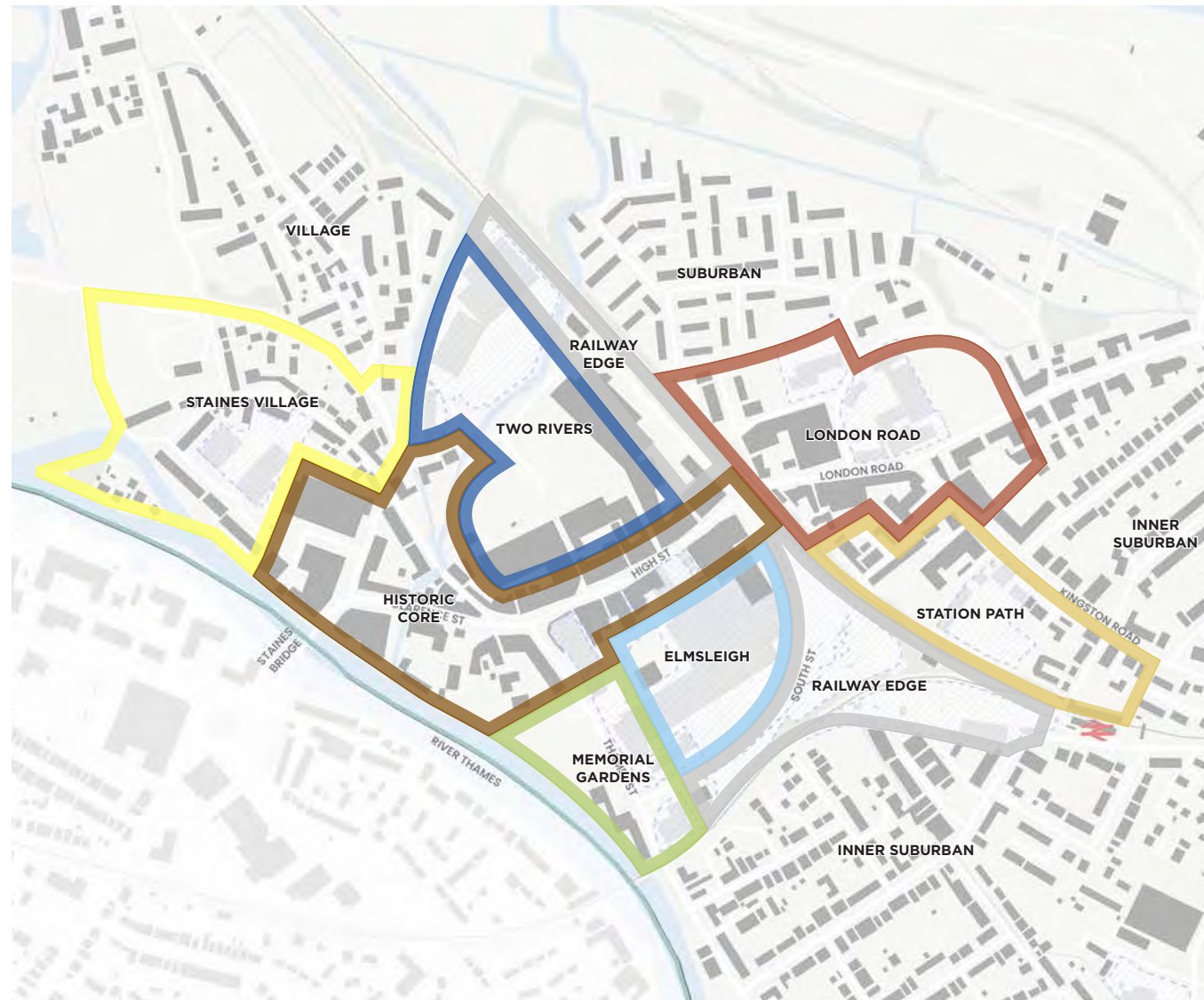
Recent development in Staines town centre: London Square (left), River Town (right)

## DESIGN AIMS

New development in Staines-upon-Thames town centre **will**:

- When within the 'Historic Core', respect and complement the context of the High Street, Clarence Street, Conservation Area and river frontage
- When within new town centre neighbourhoods, form part of coherent new places for people to live and enjoy that encourage connection, health and sustainable living
- Prioritise the quality, safety and attractiveness of the public realm, open spaces and streets
- Make the most of the river frontage and help to connect the town to the Thames, Colne and Wraysbury
- Be resilient to the anticipated effects of climate change, and ensure that new places do not adversely affect existing places in terms of flooding, microclimate and quality of life

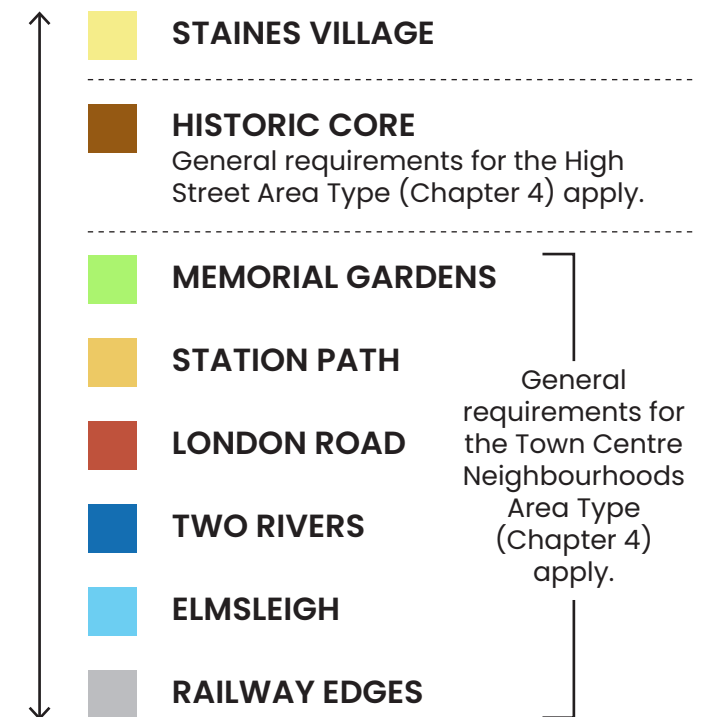
## Area Types



Within the Area of Change, more detailed requirements are set out by finer-grain Area Types. Each Area Type in the town centre is considered by whether it will largely retain its existing character and contribution to overall place identity, or whether it is likely to change substantially in character and has the opportunity to newly contribute to the town's identity.

### Incremental Change

Retaining existing character and place identity  
Design requirements strongly reflect context.



### Transformative Change

Defining a new character and place identity.  
Design requirements set key parameters only.



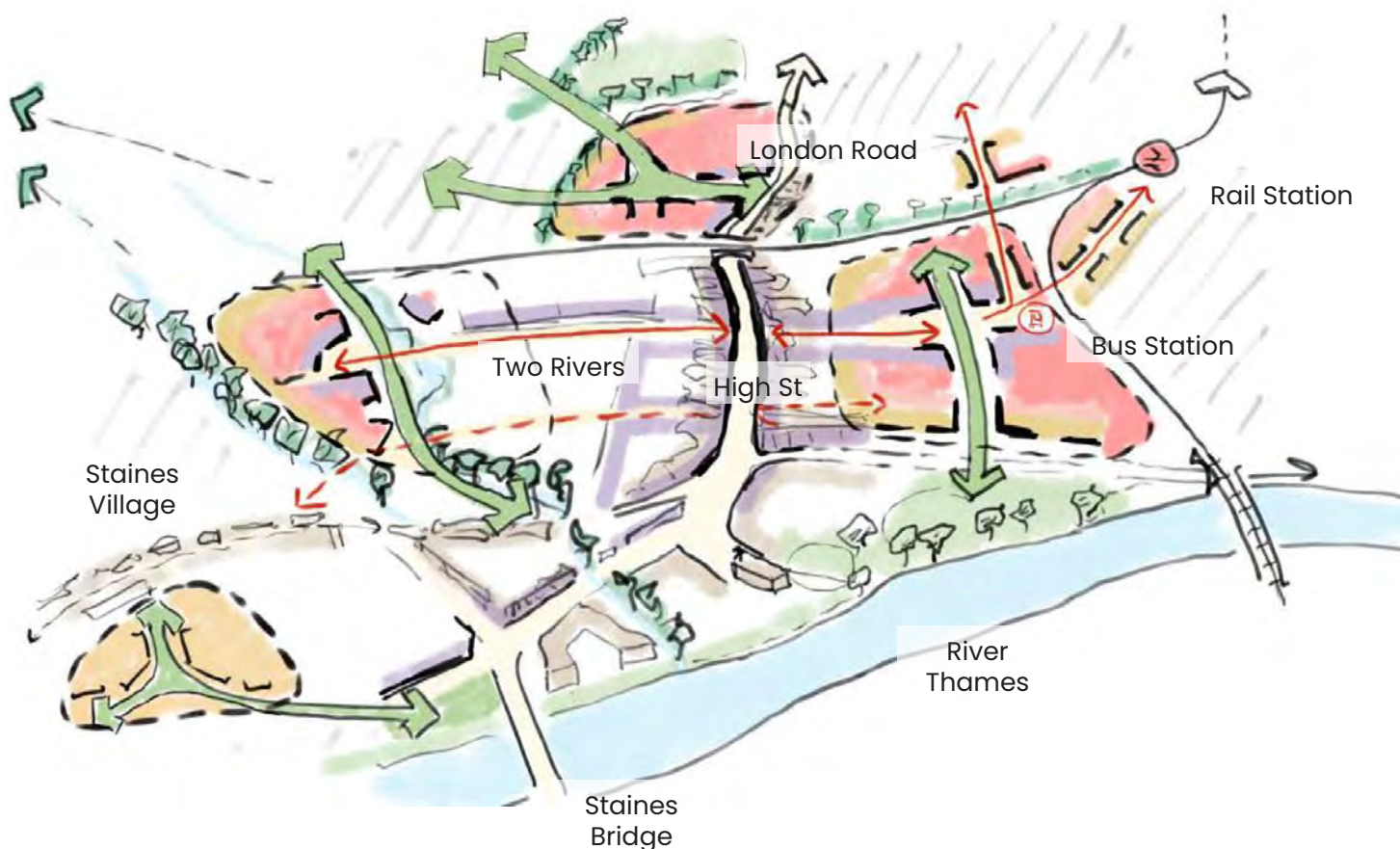
## A SPATIAL APPROACH FOR STAINES-UPON-THAMES TOWN CENTRE

It is anticipated that the town centre of Staines-upon-Thames will see substantial new development in the coming years. The spatial approach sets out an overview of how this might be accomplished in line with the vision set out in Chapter 3. Although it is illustrative, it guides the detailed design requirements set out later in the Design Code.

The spatial approach's key aims are:

- To reflect the ambition of the community to preserve what makes the town special and familiar to them by identifying key streets and areas for incremental, small-scale change
- To define connected new town centre neighbourhoods that can accommodate new open spaces, new high-quality homes and new mixed-use facilities
- To enhance the character and future sustainability of the town centre

The spatial approach is a composite of a number of layers and design thinking that work together to guide development in the future in a coherent way. These are explored further on the following page.



Protecting the historic core



Better spaces and connections



New green and blue spaces



New homes and streets for people

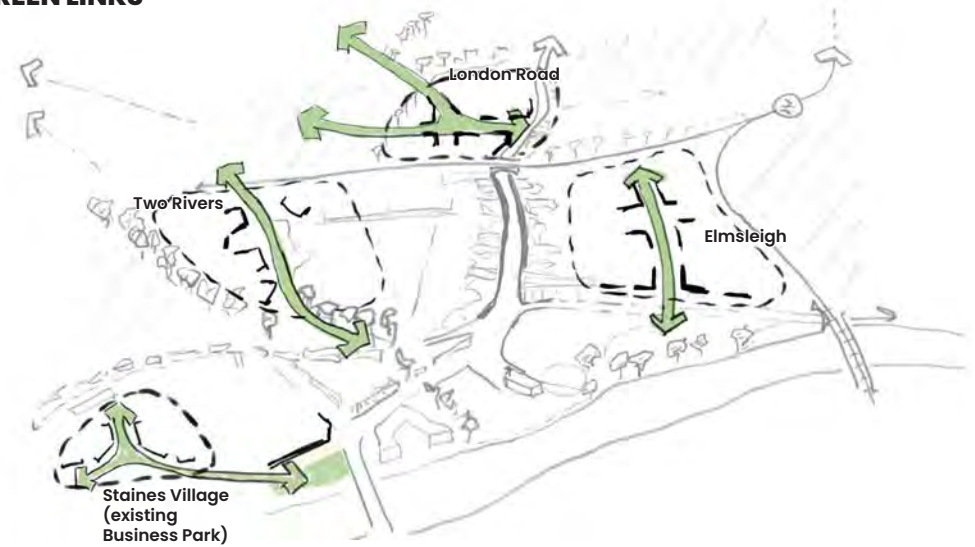


## PLACE IDENTITY



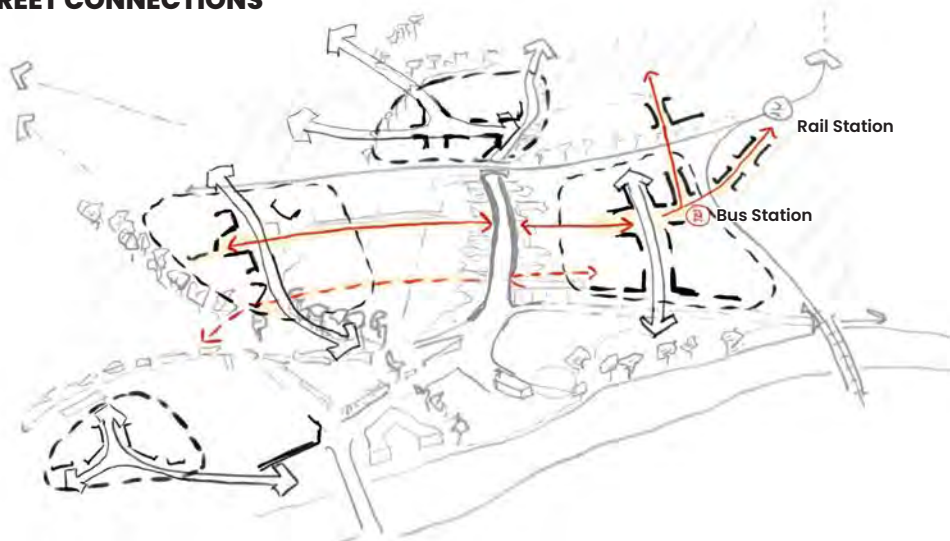
The places that are valued by the community and define the identity of Staines-upon-Thames form the core of the spatial approach.

## GREEN LINKS



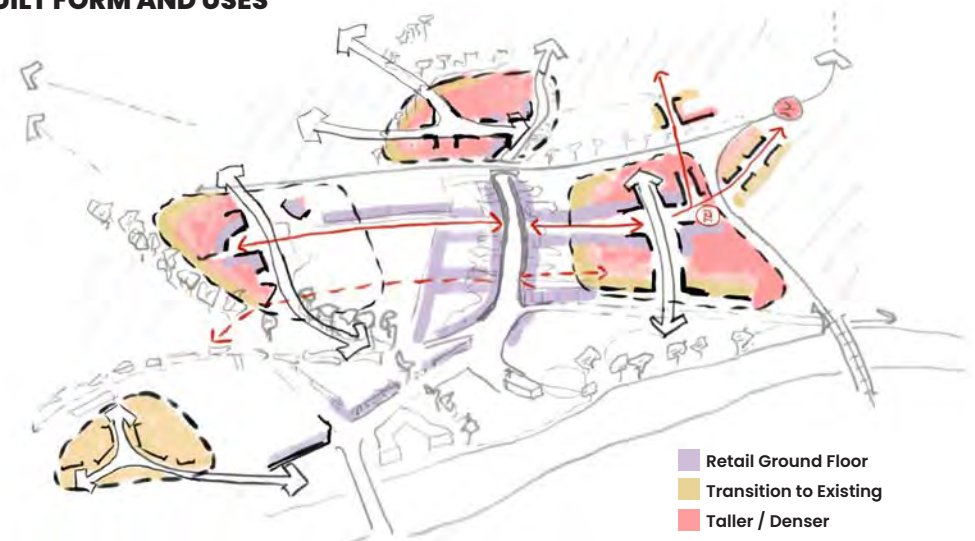
Areas of anticipated growth form new neighbourhoods, linked to their surrounding green open spaces and rivers through new green links.

## STREET CONNECTIONS



New neighbourhoods are linked and integrated to the High Street and surroundings through new walking and cycling street connections.

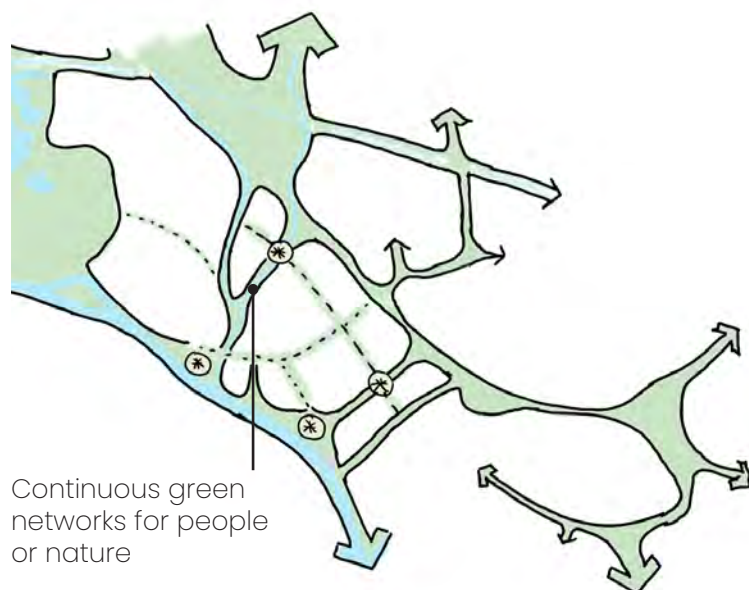
## BUILT FORM AND USES



New built form is related to surrounding neighbourhoods by ensuring transitions in height. The existing retail core is strengthened and extended.

## OVERALL TOWN CENTRE CODING STRATEGIES

Informed by the spatial approach, a range of strategies guide the detailed coding requirements across the town centre. The minimum requirements for individual schemes to implement these strategies are set out in the detailed Area Type Coding.



### Green & Blue Networks

The town centre is surrounded by green and blue assets but is poorly connected to them. New development will join up and enhance the existing networks of green and blue infrastructure, for both people and nature to use.

This strategy will be implemented by creating and enhancing open spaces, new green streets, street transformations to include more planting, and enhancements to ecological networks.

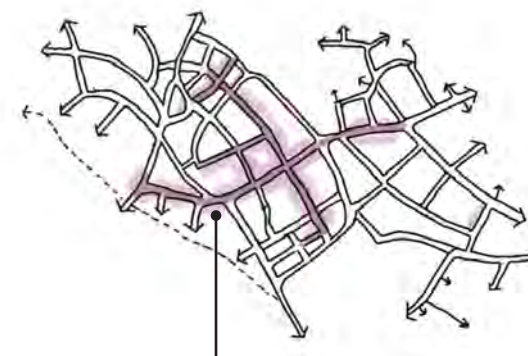
New connections



### Movement

The town centre is broken up by railway lines, major dual carriageway roads and large buildings that prevent movement. New development will enhance the existing street grid so that people can find their way and move around easily, and by more sustainable modes.

This strategy will be implemented by creating new street connections, enhancing the quality of existing streets and joining up the dots of existing active travel provision.

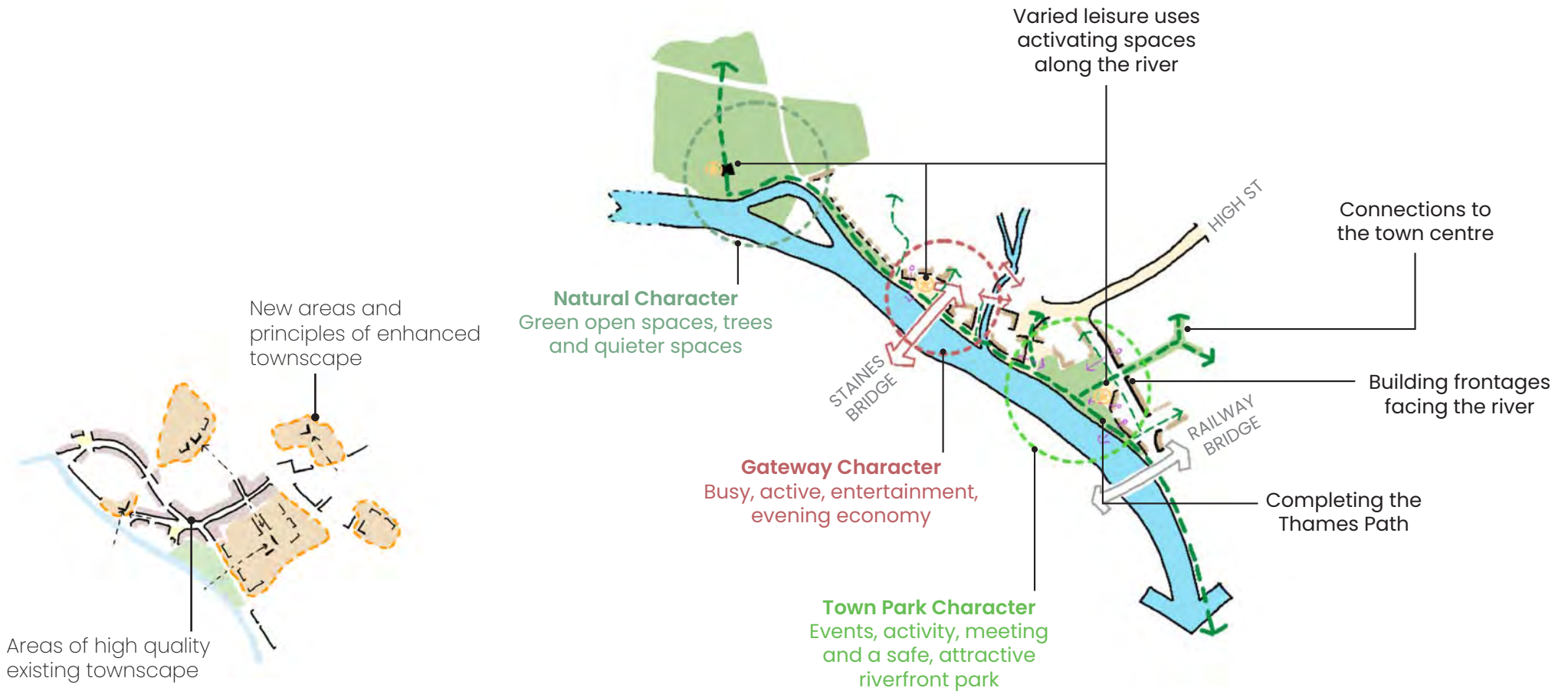


Retail and commercial core streets

### Uses & Facilities

The town centre has a strong existing High Street and retail offer, and new neighbourhoods will connect to and enhance them. New neighbourhoods will connect to and enhance the existing core of the town centre.

This strategy will be implemented by ensuring new streets and places have active commercial ground floors in the right places.



### Townscape

The new town centre neighbourhoods strongly correspond to areas with poor existing townscape, where new development could significantly enhance how the town centre is experienced overall.

This strategy will be implemented by the requirements for the arrangement of new open spaces, streets, heights, **marker** and **landmark buildings**, and the implementation of town centre neighbourhood townscape principles (Chapter 4).

### River Frontage

The River Thames that flows past Staines-upon-Thames is a vital part of the identity of the town. The overall strategy will be to establish or reinforce existing zones of activity, with attractive connections to the town centre, and development that fronts onto key spaces with complementary uses.

This strategy will be implemented by requirements for building heights, frontages, locations of open spaces, public realm priorities and key connectivity.

Relevant precedent examples for character of spaces and buildings are set out under 'Historic Core' and 'Memorial Gardens' Area Types.



## Staines Village: Conserving a Valued and Attractive Place



### DESIGN AIMS

New development will protect the existing attractive character of this area, with green space and small-scale urban grain.

### Existing Context & Place Identity

Staines Village is a quiet, attractive area centred around St. Mary's Church and Church Street, becoming progressively greener as Church Street approaches the River Thames at its western end. There are many small, domestic, historic houses which are terraced and of red or buff brick or render with slate or tiled roofs. The character is residential and small-scale.

Much of the built form has a tight grain to it, which is set against the green open space around the Church. Towards the eastern end of Church St building heights rise, with a notable landmark at Courage Tower.

### AOC-ST1 DESIGN REQUIREMENTS

Staines Village is covered by the Staines Village Conservation Area, and new development must take into account the existing character and context of this area. Development in this area type **must** comply with the following additional design requirements.

Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

The Conservation Area covers the whole of this Area Type. Development in Staines Village should therefore preserve or enhance the character of the Conservation Area.

The characteristics of the Conservation Area **must** inform the approach to:

- Built Form **Massing**
- Building Line
- Built Form Grain
- Open Space Character
- Street Design and Public Realm
- Materials and Landscape
- Detailing and Architectural Approach

All designs must observe a rigorous **design process** that sets out why and how the above parameters have been arrived at from an appraisal of the existing Conservation Area.

**The Area Type coding plan sets out key spatial considerations, particularly for the Staines Business Park allocated site.**



**Find out more** about the Conservation Area in the Staines Village Conservation Area Appraisal (2023).



View west along Church Street showing tight urban grain, curve of street and continuous building line



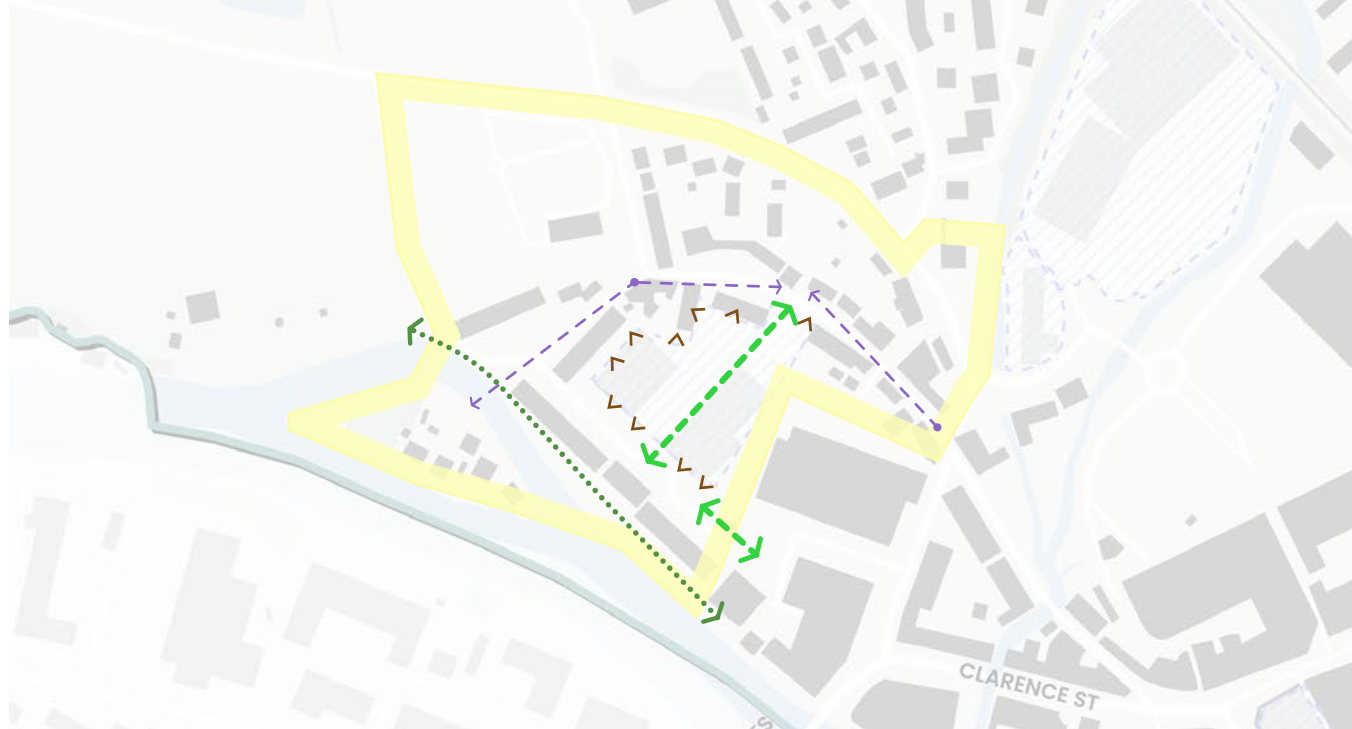
Church Street curves and opens slightly, to allow for softening from front gardens and a varying width of space.



St Mary's Church anchors the western end of the Village with a generous churchyard and hinted views towards the River Thames.

## AREA TYPE CODING PLAN

This plan sets out where design requirements apply within this Area Type.



Allocated site  
in Local Plan

### THE STREET & GROUND FLOOR

←● Key View to Retain

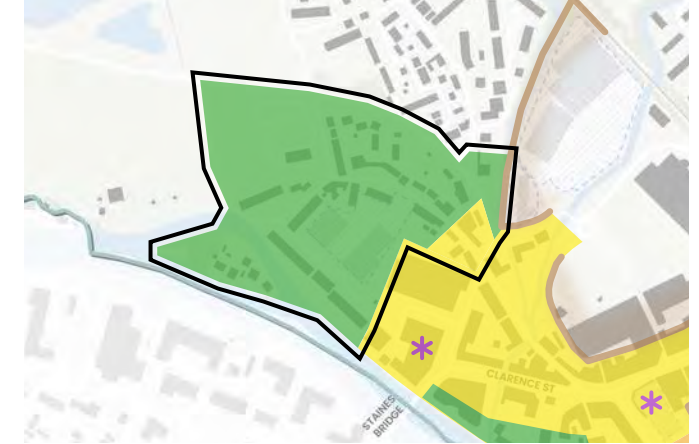
..... Thames Path

--- New active travel street  
connection

### SCALE & MASSING

▼▼ Sensitive Edge

## BUILDING HEIGHTS PLAN



Heights typically up to:

■ 3-4 storeys (approx 12m)

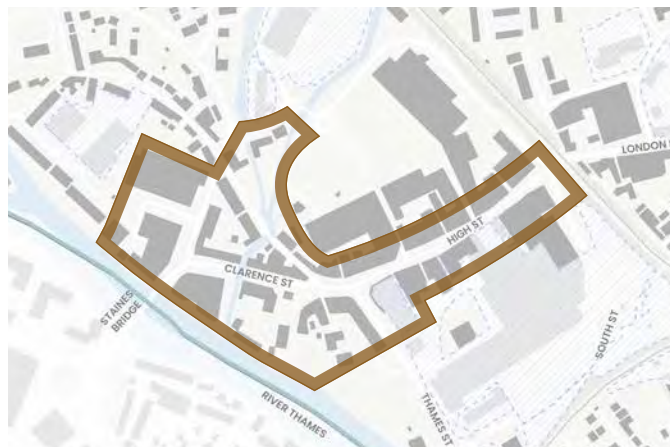
■ 5-6 storeys (approx 18m)

Heights are to be measured from pavement level to the eaves.

Typical storey heights for different uses are:

- Residential: 3m
- Commercial / Office: 4m
- Ground Floor Retail / Commercial: 4.5m

## Historic Core: Retaining the Character of the Town's Heart



### DESIGN AIMS

New development will strongly reflect the context, respecting its surroundings and retaining, not changing, the existing character.

### Existing Context & Place Identity

The heart of Staines-upon-Thames is a vibrant and successful High Street. This grew up on the historic Roman road crossing Staines bridge leading towards London. It is primarily a retailing street, with high activity levels, on-street uses such as the market and local events and is at the heart of the identity of the town. The Conservation Area covers the western half of this Area Type.

The built form throughout this area is primarily 3-4 storeys (approx 12m), with a fine urban grain and attractive townscape. There are some on-street trees and only one major open space in the Market Square.

The public realm is bisected by a major road, and the overall built form historically turns its back on the river.

### AOC-ST2 DESIGN REQUIREMENTS

General requirements for the **High Street Area Type** (Chapter 4) apply. Part of the area is covered by the Staines Village Conservation Area, and new development must take into account the existing character and context. Development in this area type **must** comply with the following additional design requirements.

Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

#### AOC-ST2a Building Heights

- Heights of between 3-6 storeys (approx 10-18m), to comply with the heights plan on the following page
- Protect the scale of and characteristic aspect ratios of existing streets and spaces with development not dominating the street scene or materially altering its street section (shown in Sections 1, 2, 3).

#### AOC-ST2b Building Line

- Building line is continuous, with buildings set at the front of the plot

#### AOC-ST2c Building Grain

- Building widths of between 6-15m
- Building frontage grain of between 6-10m, with wider buildings visually subdivided

#### AOC-ST2d Vertical Mix of Uses

- Ground floor retail and flexible commercial uses included in designs where this frontage type is specified

#### AOC-ST2e Public Realm

- Create a river front open space as part of development adjacent to Staines Bridge, requirements set out under 'Key Open Space Requirements' on following page..
- Enhanced planting and trees along river frontage to provide shade and 'soft edge' to town centre
- Improvements to pedestrian crossing point between High Street and Market Square

#### AOC-ST2f Facades, Detail & Richness

- Retention of existing façades, where they are of historic or local importance, or are of distinctive and attractive architecture, and where this is technically feasible
- Roofs to be pitched, with a variety of forms acceptable (see Chapter 4) and informed by contextual study
- Windows on frontage to match surrounding rhythm and characteristics
- Materials and architectural detailing to respond to prevailing form
- Views along Clarence Street terminated with **marker buildings**

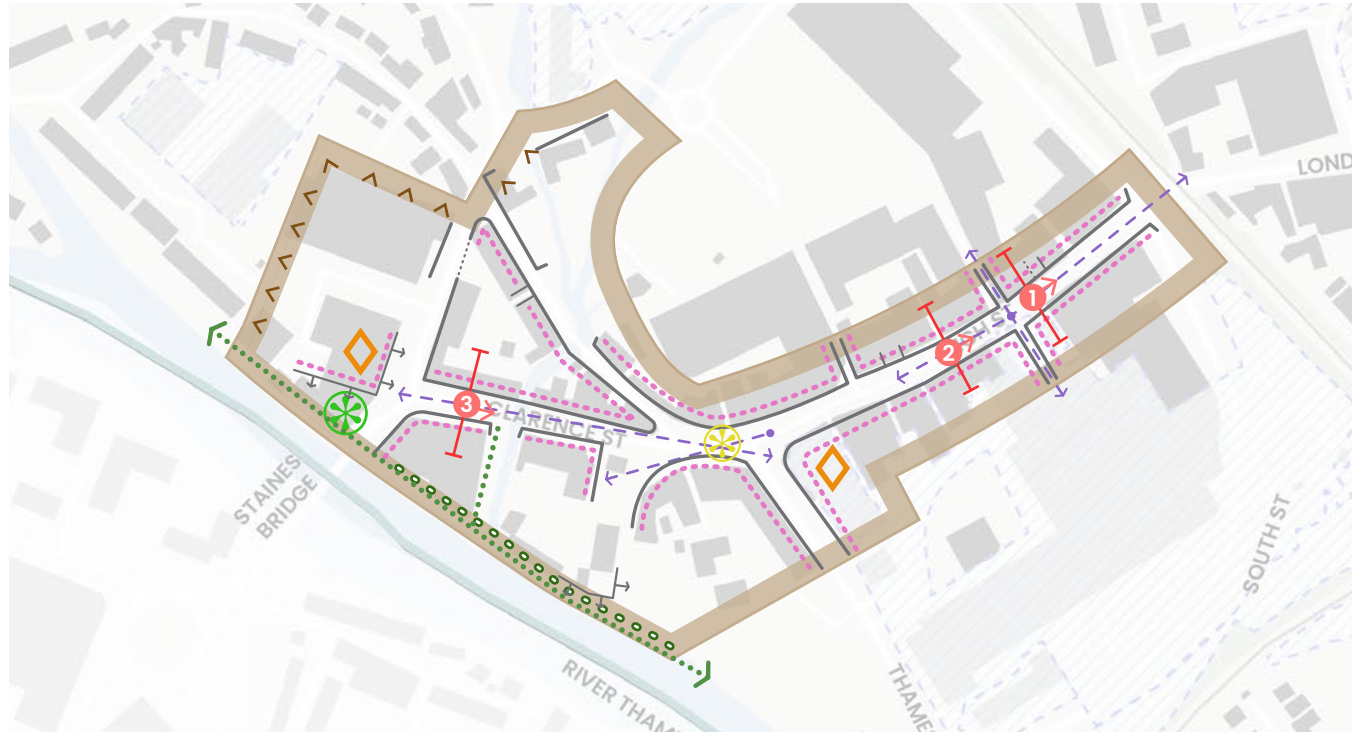


**Find out more** about the Conservation Area in the Staines Village Conservation Area Appraisal (2023).



## AREA TYPE CODING PLAN






This plan sets out where design requirements apply within this Area Type.



 Section location

 Allocated site in Local Plan





### BUILT FORM

-  Building Line
-  New Active Frontage
-  Key View to Retain
-  Marker Building
-  Sensitive Edge

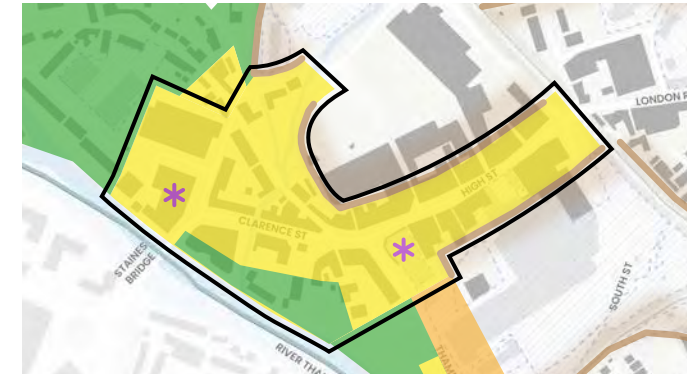
### VERTICAL MIX OF USES

-  Retail / Flexible Commercial Ground Floor



### PUBLIC REALM


-  New green open space
-  River frontage planting
-  Public realm enhancements
-  Thames Path and connecting paths

## BUILDING HEIGHTS PLAN



Heights typically up to:

-  3-4 storeys (approx 12m)
-  5-6 storeys (approx 18m)

 Locations where additional building height may be accepted, subject to:

- Review by an independent design review panel that includes community representation
- Additional height being set back from the street and sensitive edges
- Additional height complementing the wider townscape

Heights are to be measured from pavement level to the eaves.

Typical storey heights for different uses are:

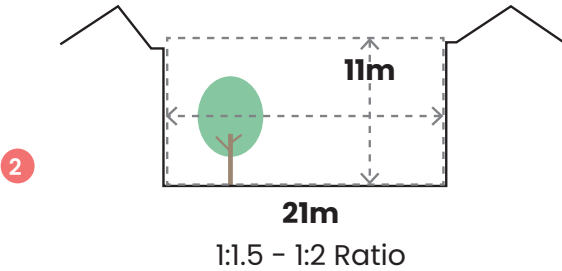
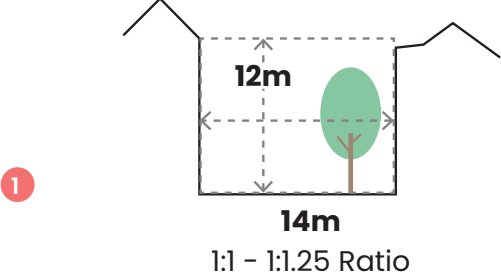
- Residential: 3m
- Commercial / Office: 4m
- Ground Floor Retail / Commercial: 4.5m

EXISTING STREET SECTIONS

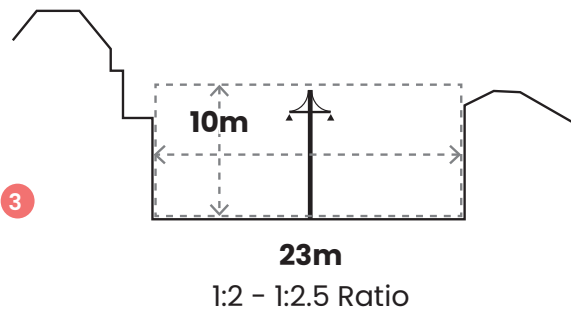
The scale of key streets and spaces will be conserved and protected, with development not dominating the street scene or materially altering its street section

For section locations see Coding Plan.

High Street



Clarence Street



MATERIALITY AND DETAILING

The High Street, Market Square and Clarence Street have a wide range of architectural styles, materials and features, tied together by distinctive dimensions of height, width, and building form.



Gable end roof forms



Corner brickwork detailing and brick variation



Market Square - colonnades



Double-height window articulation



Decorative ironwork



Cream and yellow brick



Shades of red brick



White render



## AOC-ST2g KEY OPEN SPACE REQUIREMENTS

Proposals for a new open space **must** include:

1. Thames Path running through space
2. Additional tree planting along frontage
3. Accessible connection between Clarence St and River Thames frontage
4. Opportunity for commercial reuse of bridge arches, with associated spill-out public realm
5. **Active frontages** at lower level facing river, with associated spill-out public realm
6. **Active frontage** at street level facing Bridge Street, with associated spill-out public realm
7. Open space with a mix of planted, treed and hard landscape

## EXAMPLES AND PRECEDENTS

Development in this area **could** implement the following design features, character and opportunities.



Providing spill-out space in the public realm through materials, planting, surface finishes and retractable canopies.



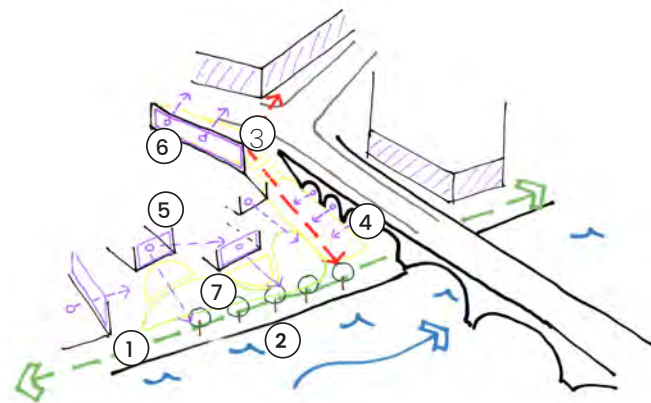
Creative use of existing heritage assets, frontages and spaces to bring back life and activity, such as the arches under Staines Bridge.



Creating new incidental spaces such as courtyards within development off main streets.



Activate the river frontage with planting, accessible landscape and lighting



Illustrative approach to applying the key design requirements



## Memorial Gardens: Improving and Respecting the River Thames Frontage



### DESIGN AIMS

New development in this area of the Thames frontage will face the river, relate to and help to animate the green open space, and create new connections to the rest of the town centre. Any development will be of exceptional architectural quality and enhance the existing townscape.

### Existing Context & Place Identity

The River Thames, and the bridge across it, is the primary reason for the existence of Staines-upon-Thames, and is a much valued asset.

Historically, the town 'turned its back' on the river, primarily seeing it as a location for industry and other marginal uses, and it has only been in the latter stages of the 20th century that its value as a leisure and recreation asset has been recognised, with the creation of Memorial Gardens and the Thames Path national long-distance footpath running through the town.

### AOC-ST3 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

#### AOC-ST3a The Street & Ground Floor

- Building line to be continuous along Thames Street. Setback from plot edge of at least 2m to provide additional public realm for use as spill-out space or planting
- Extend the Thames Path along the full extents of river frontage from Memorial Gardens to the railway bridge
- Planting and trees accommodated along Thames St in areas of extended public realm.
- Ground floor retail and flexible commercial uses where this frontage type is specified.
- Frontages to activate Memorial Gardens

#### AOC-ST3b Scale & Massing

- Heights to comply with the maximum heights plan and key principles in Sections 1 and 2 on the following pages
- Building widths of 10-25m
- Appropriate development typologies include Villas and linear blocks

#### AOC-ST3c Open Spaces

- Buildings to face Memorial Gardens
- Extend Memorial Gardens onto the existing car park, design requirements set out under 'Key Open Space Requirements' on following page.
- Extend the green open space network from Memorial Gardens towards the northwest

#### AOC-ST3d Detail & Richness

- Roofs to contribute to townscape with pitched form, and with variation when viewed from a distance.
- Building frontage grain of 10-15m, with wider buildings visually subdivided
- **Marker buildings** at key locations to provide legibility and townscape interest. See coding plan.
- 3-4 storey landmark mixed-use building opportunity adjacent to Memorial Gardens, anchoring the extended space

## AREA TYPE CODING PLAN

This plan sets out where design requirements apply within this Area Type.



Section location

Allocated site in Local Plan

### THE STREET & GROUND FLOOR

Building Line

New Active Frontage

Retail / Flexible Commercial Ground Floor

Key Overlooking Location

Existing path or active travel street to connect to

New active travel street connection

### OPEN SPACES

New green open space

Extension to public realm

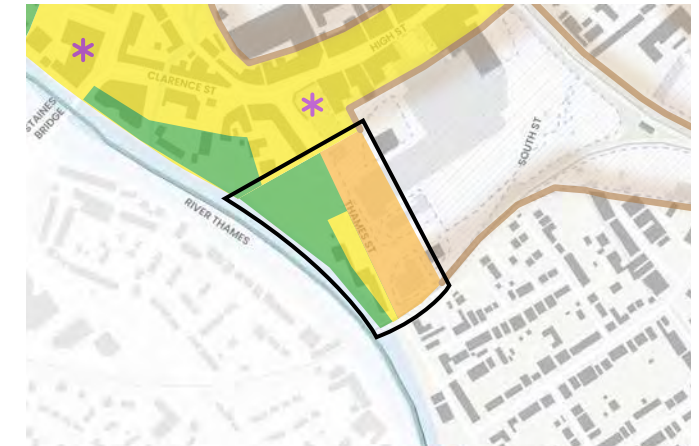
Street Planting & Greening

### DETAIL & RICHNESS

Marker Building

Landmark Building

## BUILDING HEIGHTS PLAN



Heights typically up to:

3-4 storeys (approx 12m)

5-6 storeys (approx 18m)

8 storeys (approx 24m)

See Sections 1 and 2 on following page for explanation and key principles on heights.

Heights are to be measured from pavement level to the eaves.

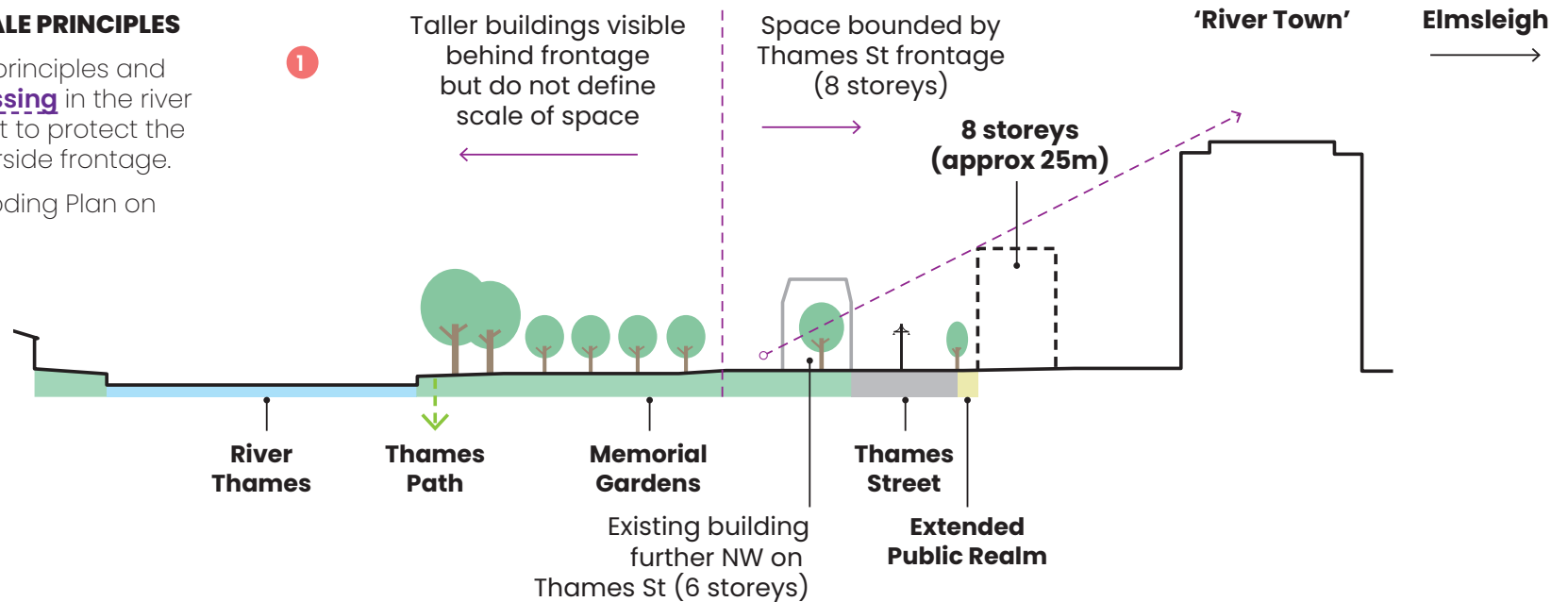
Typical storey heights for different uses are:

- Residential: 3m
- Commercial / Office: 4m
- Ground Floor Retail / Commercial: 4.5m

## (AOC-ST3b) HEIGHTS AND SCALE PRINCIPLES

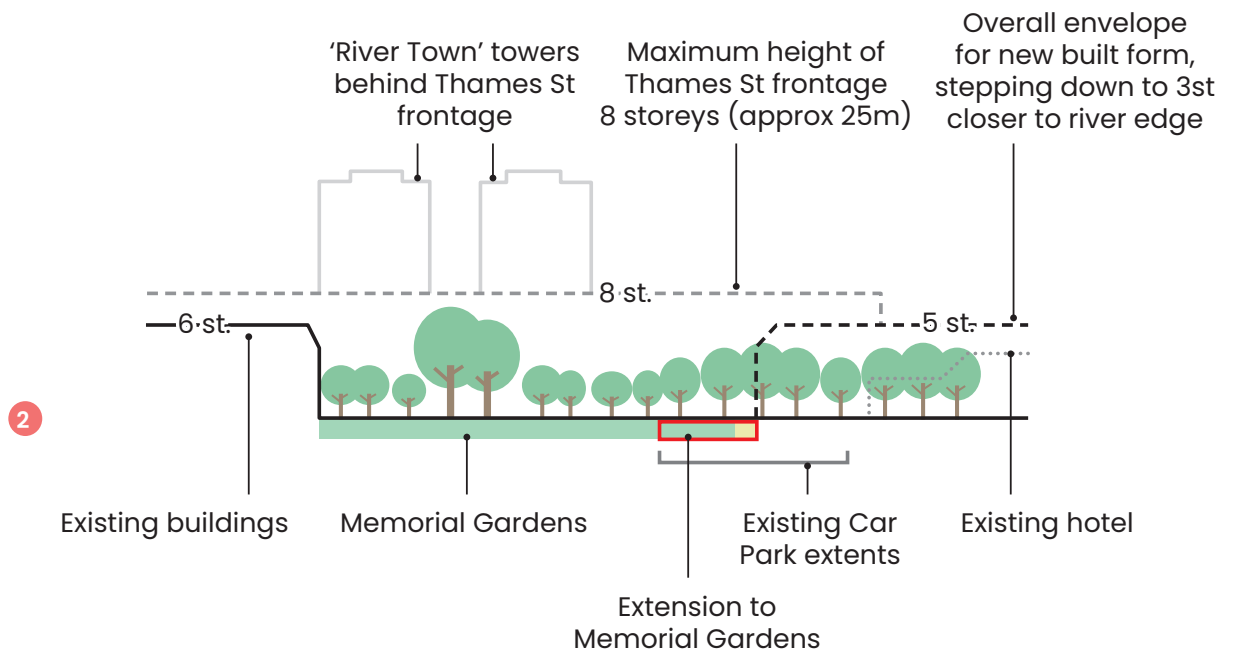
This diagram illustrates overall principles and rationale for the scale and **massing** in the river frontage area. These are set out to protect the scale and character of the riverside frontage.

For locations of sections see Coding Plan on previous page.



Key principles for **massing** are that development **must**:

- Have a frontage along Thames Street should at a maximum of 8 storeys (approx 24m) to retain an appropriate scale to Memorial Gardens, and to reduce the impact of taller buildings set back behind Thames Street
- Have maximum heights on the river side of Thames Street should be similar to those already in existence (up to 6 storeys, approx 18m, immediately adjacent to Thames Street)
- Have maximum heights on the river side of Thames Street should step down towards the river frontage, at a maximum of 3 storeys (approx 10m) if immediately adjacent to the river
- Have building mass that is broken up with variety in heights, roofscape and articulation of façades





## AOC-ST3e KEY OPEN SPACE REQUIREMENTS

Proposals for a new open space **must** include:

1. Thames Path running through space, extended along river front with overlooking from built form
2. Accessible connections and improved crossing across Thames St
3. West or Southwest facing **active frontage** facing river and green open space with associated spill-out public realm
4. **Active frontage** on Thames St
5. Extension of existing green open space
6. Additional tree planting

## EXAMPLES AND PRECEDENTS

Development in this area **could** implement the following design features, character and opportunities.



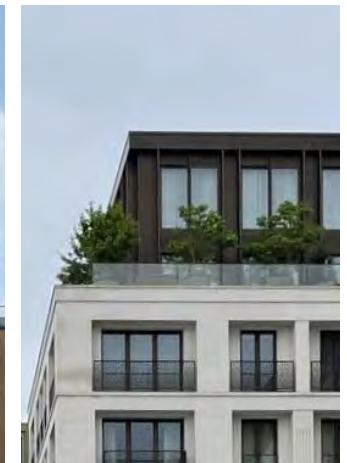
Use of hard landscape public realm to support active ground floor uses on the edge of Memorial Gardens, with trees for shade and to provide a transition to greener open space in Memorial Gardens.



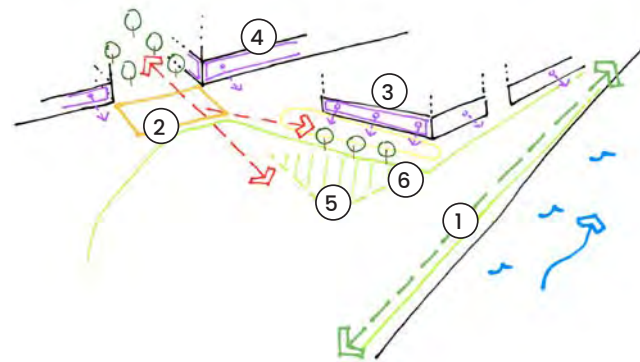
Fine-grained, varied frontages that address the river, open up views and connections, and provide overlooking to Memorial Gardens without overwhelming the scale of the space.



Activating open space with play and landscape features to encourage the use of Memorial Gardens as a destination, with play, seating and other activities available.



Use of gable-end and set back roofs to provide interest and variety to building tops, and usable private outdoor space facing the river.



Illustrative approach to applying the key design requirements

## Station Path: Improving Connections and Integrating Development Sensitive



### DESIGN AIMS

New development in this area will realise the potential of the sustainable location close to the railway station. It will benefit the surroundings by enhancing the Station Path, providing safe and attractive links between the path and Kingston Road, and reducing areas of severance and discontinuity. It will relate respectfully in scale and massing to the residential area to the north.

### Existing Context & Place Identity

The Station Path is an important gateway to the town and has an attractive green character, but is bordered by car parks and underused spaces that can make it feel unsafe.

Community and commercial uses, along with apartments, are located on Kingston Road. A mix of Victorian and Edwardian homes are interspersed with more modern, less well-integrated development and surface parking.

### AOC-ST4 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

#### AOC-ST4a The Street & Ground Floor

- Match and repair existing building lines.
- Provide a setback along Station Path for seating and planting.
- Provide passive surveillance of Station Path, particularly the railway underpass and surrounding area
- Pedestrian and cycle links between Kingston Road and the Station Path
- Street network to respect and connect to existing street grid and characteristic blocks of 55-75m width

#### AOC-ST4b Scale & Massing

- Heights to transition to surrounding built form as set out on the transitional edges plan and key principles in Section 1 on following pages
- Building widths or frontage grain of between 7-15m to reflect existing built form
- Appropriate development typologies include terraces, mews, linear blocks, villas and occasional towers on podiums.

#### AOC-ST4c Open Spaces

- Create a square in front of the Oast House which provides an appropriately-scaled space for this building. Requirements are set out under 'Key Open Space Requirements' on following page.

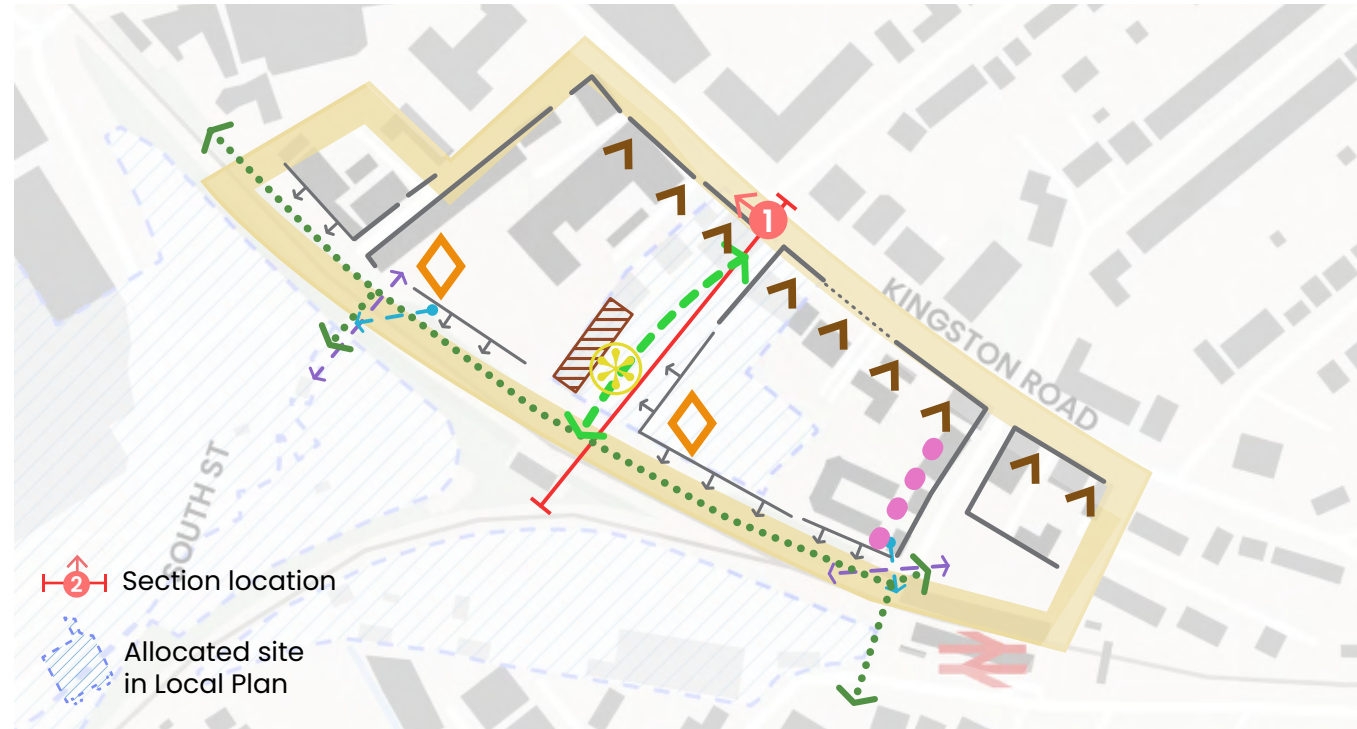
#### AOC-STd Detail & Richness

- Roofs up to 5 storeys (approx 15m) to be pitched with gable ends and dormers acceptable.
- Roofs of taller buildings to provide visual interest with distinctive form, and with variation when viewed from a distance.
- **Marker buildings** at key locations to provide legibility and townscape interest. See coding plan.



## AREA TYPE CODING PLAN

This plan sets out where design requirements apply within this Area Type.



### THE STREET & GROUND FLOOR

- Building Line
- ↕ New Active Frontage
- ↔ Key View to Retain
- Retail / Flexible Commercial Ground Floor
- ⋯ Repaired Building Line

### Key Overlooking Location

- ⋯ Existing path or active travel street to connect to
- New active travel street connection

### SCALE & MASSING

- ▼ Sensitive Edge

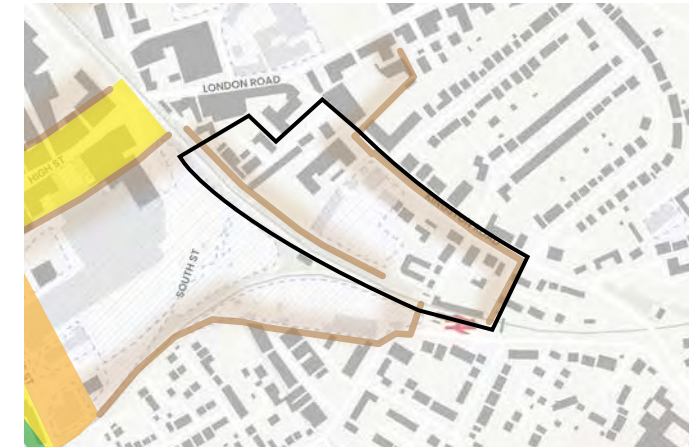
### OPEN SPACES

- ☼ Public realm enhancements

### DETAIL & RICHNESS

- ◇ Marker Building
- ▨ Heritage Asset to define edge of new public realm

## TRANSITIONAL EDGES PLAN



Transitional Edge, where heights step down to blend with prevailing built form and preserve street scale.

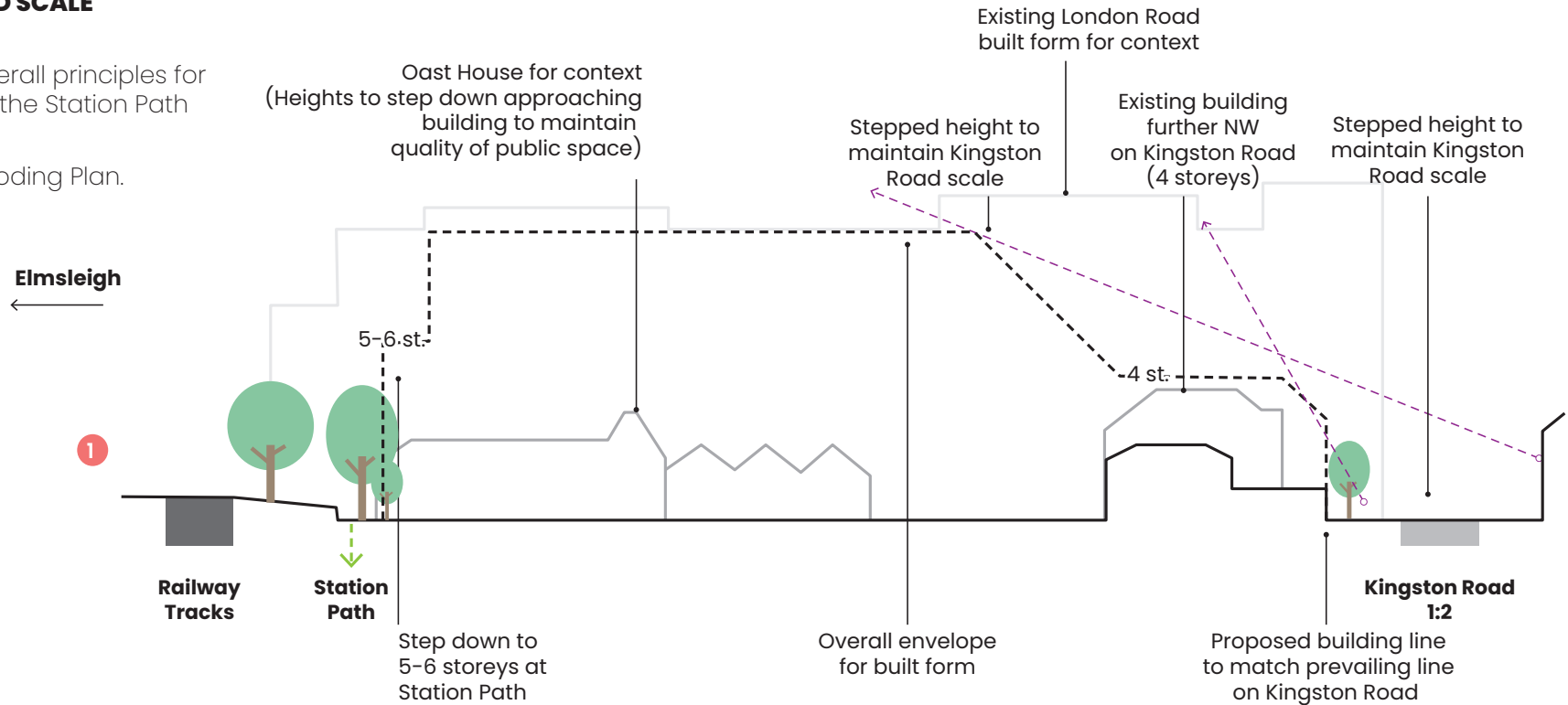
See Section 1 on following page for explanation and key principles on transitional edges.



## (AOC-ST4b) HEIGHTS AND SCALE PRINCIPLES

This diagram illustrates overall principles for the scale and **massing** in the Station Path area.

For section location see Coding Plan.



Key principles for **massing** are that development **must**:

- Step down to 3-4 storeys (approx 12m) to meet Kingston Road towards the east, preserving its scale and views from the street and buildings to the north
- Step down to 5-6 storeys (approx 18m) to meet Kingston Road towards the west, closer to London Road
- Step down to 5-6 storeys (approx 18m) create a human scale adjacent to Station path
- Step down to meet the public space in front of the Oast House as set out in the Key Open Space Requirements on the following page

## AOC-ST4e KEY OPEN SPACE REQUIREMENTS

Proposals for a new open space **must** include:

1. Improved overlooking of Station Path
2. Walking and cycling connection between Kingston Road and Station Path
3. Public space created in front of Oast House with new surrounding built form scaled and set back at taller heights, as shown in section inset.
4. 'Spill-out' public realm immediately in front of Oast House
5. Tree planting within public realm to provide shade and character to space
6. Overlooking and **active frontages** to all spaces from new built form

## EXAMPLES AND PRECEDENTS

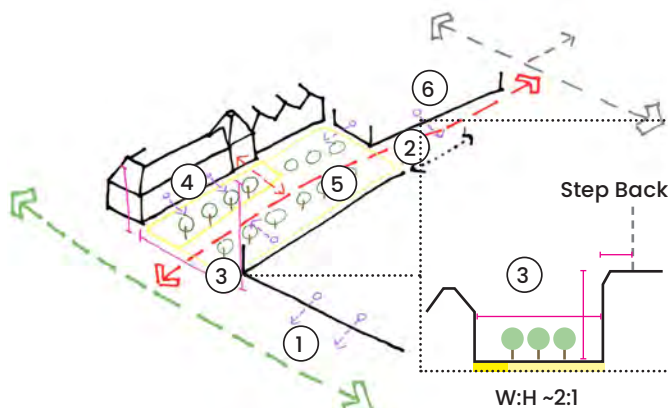
Development in this area **could** implement the following design features, character and opportunities.



Streets that prioritise people and limit vehicle speeds, and reflect the existing structure and key dimensions of blocks and streets in the surrounding context.



Use of artwork in the public realm to provide legibility and a distinctive character to this area, particularly on new pedestrian links past the Oast House.



Illustrative approach to applying the key design requirements



Use of mews streets to ensure high densities within blocks whilst maintaining a mix of house types in developments.

## London Road: A New Gateway Neighbourhood for the Town Centre



### DESIGN AIMS

New development in this area will create new high-quality green spaces and public realm for residents and the public that provide a setting for higher-density buildings, and maximise connectivity through to the suburbs and green spaces to the north.

### Existing Context & Place Identity

On the north-eastern side of the railway tracks, under the Iron Bridge, the London Road area is an extension of the High Street. It has some secondary and local retail and service uses, as well as being a focus for a number of larger high-density residential schemes, with associated ground floor retail uses.

London Road is an important gateway location for the town and is a major new neighbourhood for Staines-upon-Thames.

### AOC-ST5 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

#### AOC-ST5a The Street & Ground Floor

- Building line to be set back from plot edge along London Road to provide additional public realm, planting and spill-out space.
- Retail and commercial ground floor uses to be located along London Road frontage

#### AOC-ST5b Scale & Massing

- Heights to transition to surrounding built form as set out on the transitional edges plan and key principles in Section 1 on following pages
- General presumption of high residential densities and **Floor Area Ratio** of 3.0 or above.
- Appropriate development typologies include occasional podiums and towers, villas and linear blocks.

#### AOC-ST5c Open Spaces

- Create a linear green open space from London Road/Kingston Road towards the north. Requirements are set out under 'Key Open Space Requirements' on following page.

#### AOC-ST5d Detail & Richness

- Roofs of taller buildings should provide visual interest with distinctive form, and with variation when viewed from a distance.
- **Marker buildings** at key locations to provide legibility from London Road towards other developments further north. See coding plan.



## AREA TYPE CODING PLAN

This plan sets out where design requirements apply within this Area Type.



Section location

Allocated site  
in Local Plan

### THE STREET & GROUND FLOOR

- Building Line
- ↕ New Active Frontage
- ←● Key View to Retain
- Retail / Flexible Commercial Ground Floor

- ←● Key Overlooking Location
- Existing path or active travel street to connect to
- New active travel street connection

- SCALE & MASSING
- ▼▼ Sensitive Edge

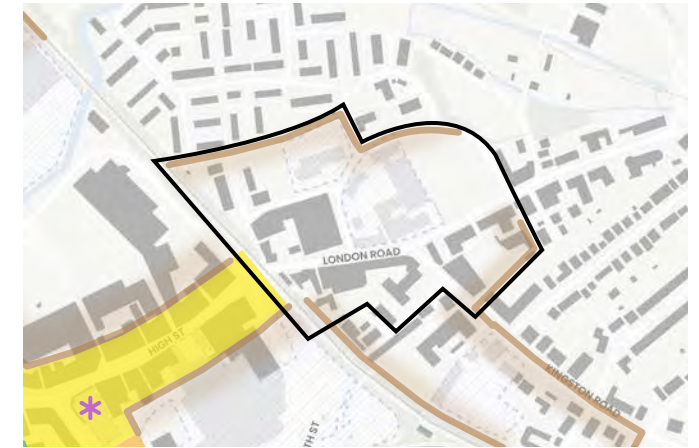
### OPEN SPACES

- ⊗ New green open space
- ⊗ Public realm enhancements
- Street Planting & Greening

### DETAIL & RICHNESS

- ◇ Marker Building

## TRANSITIONAL EDGES PLAN



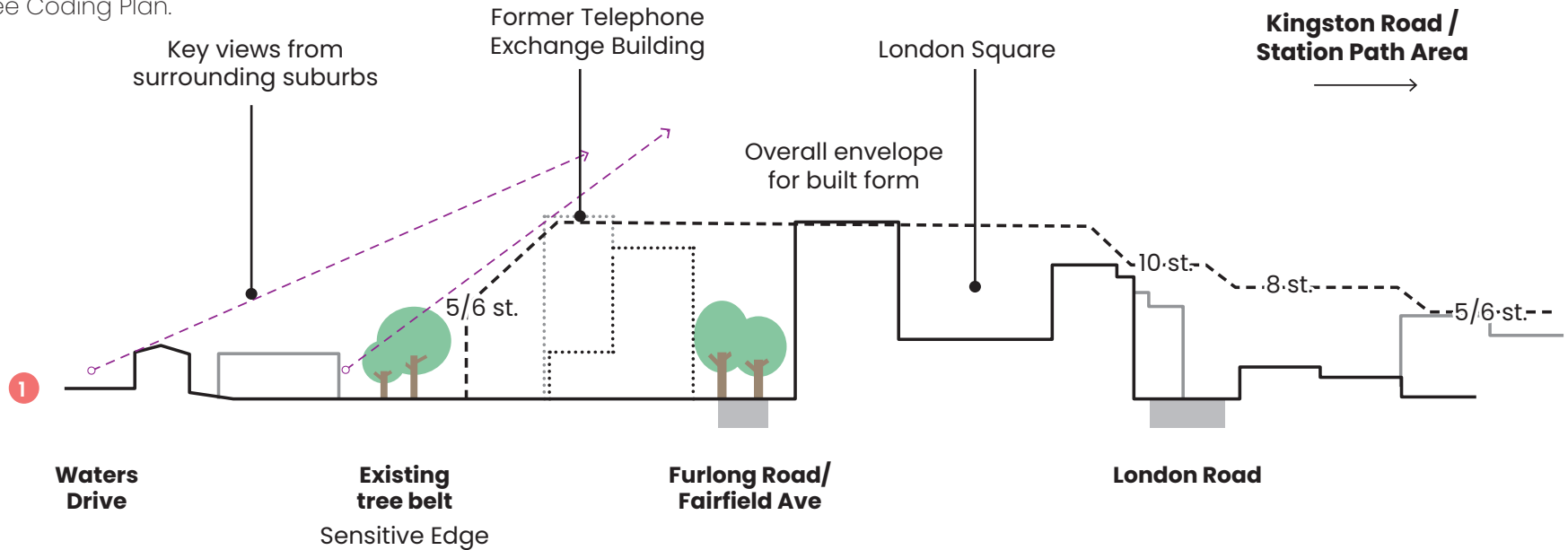
Transitional Edge, where heights step down to blend with prevailing built form and preserve street scale.

See Section 1 on following page for explanation and key principles on transitional edges.

## (AOC-ST5b) HEIGHTS AND SCALE PRINCIPLES

This diagram illustrates overall principles for the scale and **massing** in the London Road area.

For section location see Coding Plan.



Key principles for **massing** are that development **must**:

- Step down towards the northern edge bordering Waters Drive to approximately match the heights of the existing tree belt (5/6 storeys, approx 18m)
- Heights to peak in the centre of the neighbourhood at 12 storeys (approx 36-40m)
- Heights on London Road to be no higher than the currently prevailing heights of recent developments (approx 10 storeys / 32m)
- Be generally lower in height on the south-western side of London Road to transition to this lower-rise existing residential area

## AOC-ST5e KEY OPEN SPACE REQUIREMENTS

Proposals for a new open space **must** include:

1. Connections to wider open spaces and neighbourhoods to the north
2. Retention of existing trees
3. Overlooked public realm at key node
4. Connection from London Road to former Telephone Exchange site and public realm
5. Overlooked linear green open space
6. Improved pedestrian and cycling connections at London Road / Kingston Road junction
7. Retail frontage and associated 'spill-out' public realm on London Road
8. **Marker building** to terminate views along linear green open space

## EXAMPLES AND PRECEDENTS

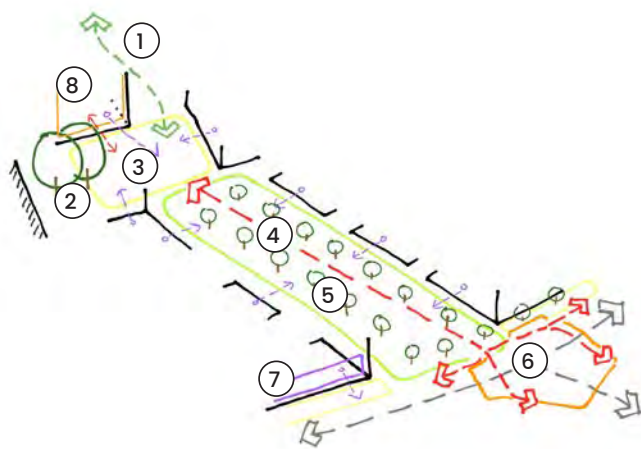
Development in this area **could** implement the following design features, character and opportunities.



Well-overlooked green open spaces with trees, seating and a choice of walking routes.



Active commercial ground floors that have a strong relationship with a pedestrian-friendly public realm.



Illustrative approach to applying the key design requirements



Urban character of public realm with trees, cycle parking and hard landscape at key nodes, such as along London Road or at the public realm square opportunity set out on the coding plan.



## Two Rivers: A New Neighbourhood with access to Nature



### DESIGN AIMS

New development in the Two Rivers area will create a new, integrated primarily residential neighbourhood strongly related to the watercourses running through it. Opportunities to reflect the grain or character of the historic industrial uses of the site are supported.

### Existing Context & Place Identity

To the north-west of the core of the historic town centre sits the Two Rivers Retail Park, which occupies land previously used for the linoleum industry. The area is dominated by a large surface car park and associated highway infrastructure and edged by modern retail and leisure units. The retail park integrates well with the core High Street and strengthens the overall town centre offer.

### AOC-ST6 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

#### AOC-ST6a The Street & Ground Floor

- New street layouts designed on 'superblock' principles to prioritise active travel, with vehicle movement limited to parking and service access to buildings
- New streets to connect to and extend the street grid of the town centre from the southeast
- Enhance the safety of the existing pedestrian bridge towards the west by locating of built form with **active frontages** facing it
- Connect new streets to existing footpaths along River Wraysbury corridor
- Retention and extension of retail, leisure and other commercial uses along key NW-SE axis. See Coding Plan.

#### AOC-ST6b Scale & Massing

- Heights to transition to surrounding built form as set out on the transitional edges plan and key principles in Section 1 on following pages
- **Massing** led by creating a comfortable street scale with the tightest width:height ratio of around 1:1. Heights that would break this street aspect ratio are permitted through the use of a **'shoulder'** where heights step back from the street edge.

- General presumption of high residential densities and **Floor Area Ratio** of 3.0 or above.
- Terraces and linear blocks to be used close to sensitive edges.
- All development typologies may be appropriate in other locations

#### AOC-ST6c Open Spaces

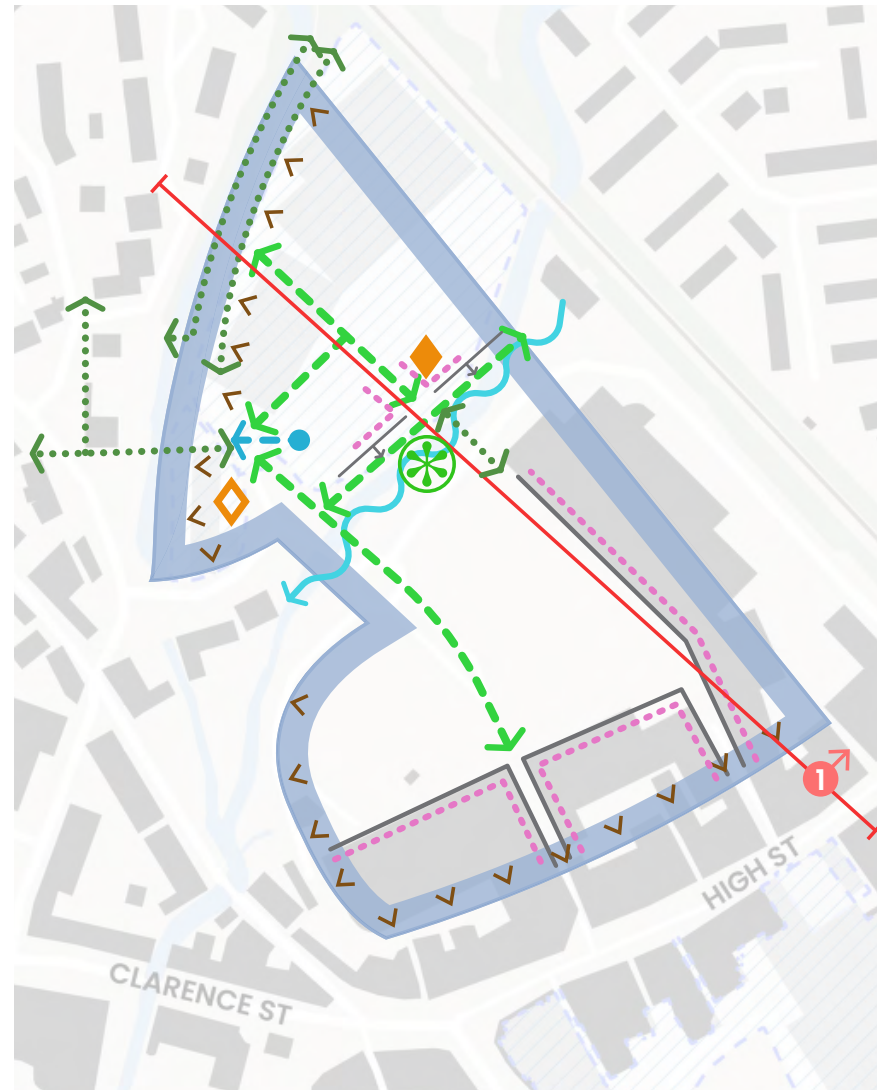
- Create a new linear park and public spaces along a restored River Colne. Requirements are set out under 'Key Open Space Requirements' on following page.
- Enhancement of the habitats and watercourses of the Rivers Wraysbury and Colne, including potential for river restoration approaches along the Colne.














#### AOC-ST6d Detail & Richness

- Roofs of taller buildings should provide visual interest with distinctive form, and with variation when viewed from a distance.

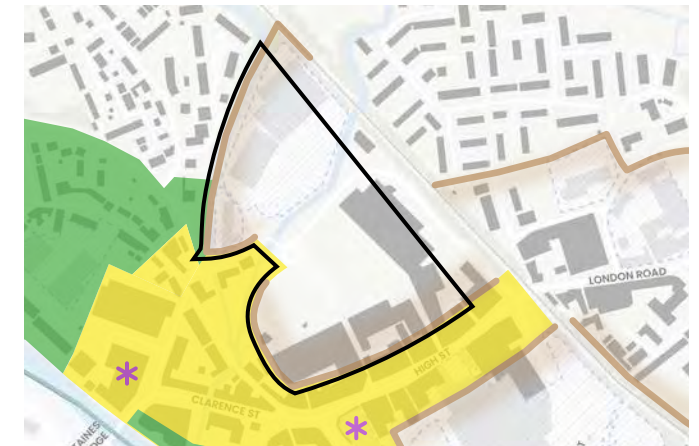
## AREA TYPE CODING PLAN


This plan sets out where design requirements apply within this Area Type.



-  Section location
-  Allocated site in Local Plan
- THE STREET & GROUND FLOOR**
  -  Building Line
  -  New Active Frontage
  -  Retail / Flexible Commercial Ground Floor
  -  Key Overlooking Location
  -  Existing path or active travel street to connect to
  -  New active travel street connection
- SCALE & MASSING**
  -  Sensitive Edge
- OPEN SPACES**
  -  New green open space
  -  River restoration opportunity
- DETAIL & RICHNESS**
  -  Marker Building
  -  Landmark Building

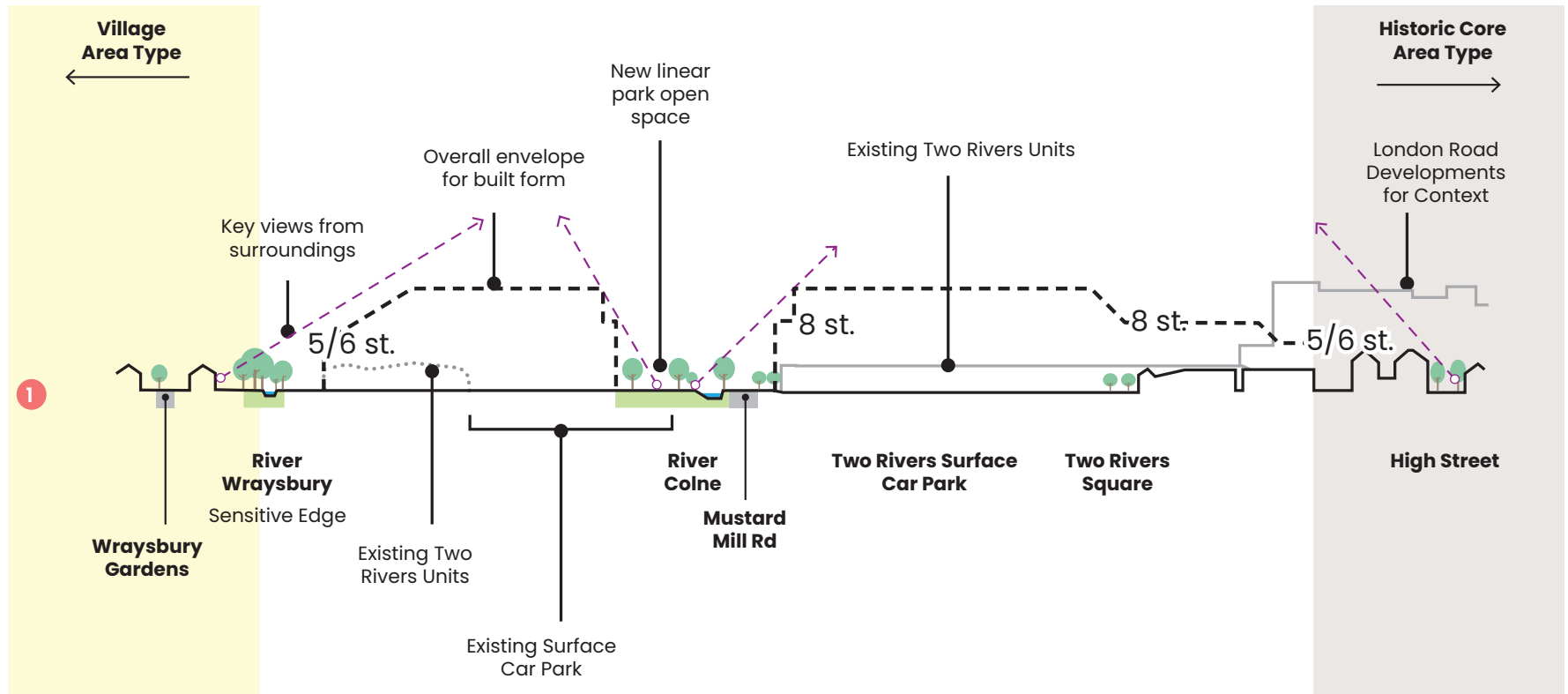
## TRANSITIONAL EDGES PLAN



-  Transitional Edge, where heights step down to blend with prevailing built form and preserve street scale.
- See Section 1 on following page for explanation and key principles on transitional edges.

**(AOC-ST6b) HEIGHTS AND SCALE PRINCIPLES**

This diagram illustrates overall principles for the scale and **massing** in the Two Rivers area. For section location see Coding Plan.



Key principles for **massing** are that development **must:**

- Step down towards the neighbouring residential area to the northwest, with heights at the edge approximately as high as the existing tree belt on the River Wraysbury (~15-18m, 5-6 storeys)
- Have heights of up to 8 storeys (approx 24m) immediately adjacent to the new linear park, with taller heights set back with a **'shoulder'**



## AOC-ST6e KEY OPEN SPACE REQUIREMENTS

Proposals for a new open space **must** include:

1. Connections across existing bridges to town centre streets into open space and towards the north and west
2. Crossing point to provide walking and cycling access to open space
3. Overlooked, activated public realm at node
4. **Marker building** to terminate view from town centre
5. Retail **active frontage** around key node and overlooking of open space from built form
6. Strengthening of existing trees and planting to create varying landscape character
7. Accessible green open space around river
8. Walking and cycling links through open space

## EXAMPLES AND PRECEDENTS

Development in this area **could** implement the following design features, character and opportunities.



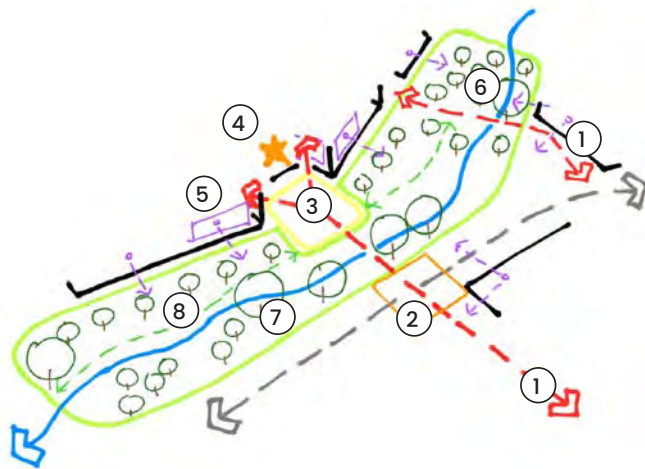
Views maintained through development to provide a visual connection to surrounding areas and natural spaces.



Using the design and materiality of bridges as important connection points and features.



Use varied and attractive landscape and public realm features to make use of the River Colne as the heart of a new open space, reactivating this area for both nature and people.



Illustrative approach to applying the key design requirements

## Elmsleigh: A Bustling Town Centre Neighbourhood for all



### DESIGN AIMS

New development in the Elmsleigh area will create a new, integrated mixed-use town centre neighbourhood, progressively changing the existing monolithic, inward-facing built form to one of streets, open spaces and individual buildings, with much better connections to the river frontage and surrounding neighbourhoods and facilities.

### Existing Context & Place Identity

The area to the south-east of the High Street is a covered 1970s shopping centre with a large multi-storey car park and service entrances on main streets. It is a part of the town's retail offer.

This part of the town centre is also an important gateway for public transport users, arriving by rail or bus, and for those walking or cycling, as the majority of homes in Staines-upon-Thames located within walking or cycling distance of the town centre are located to the south-east.

### AOC-ST7 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

#### AOC-ST7a The Street & Ground Floor

- New street layouts designed on 'superblock' principles to prioritise active travel, with vehicle movement limited to parking and service access to buildings. South Street to become a multi-modal street (chapter 4).
- Retain existing NW-SE High St to bus station connection as a pedestrian priority street
- Retention and extension of existing retail, leisure and other commercial uses along key NW-SE axis, connecting to High Street and bus station. See Coding Plan.
- Mix of retail and commercial unit sizes to provide opportunities for smaller and independent businesses
- Create new NE-SW pedestrian priority connection towards river frontage
- **Active frontage** onto South Street and bus station

#### AOC-ST7b Scale & Massing

- Heights to transition to surrounding built form as set out on the transitional edges plan and key principles in Section 1 on following pages

- **Massing** led by creating a comfortable street scale with the tightest width:height ratio of around 1:1. Heights that would break this street aspect ratio are permitted through the use of a '**shoulder**' where heights step back from the street edge.
- General presumption of high residential densities and high **Floor Area Ratio** of 3.0 and above.
- Appropriate development typologies include podiums and towers, villas and linear blocks closer to edges.

#### AOC-ST7c Open Spaces

- New green open space at heart of new neighbourhood, where streets cross. Requirements are set out under 'Key Open Space Requirements' on following page.

#### AOC-ST7d Homes & Practicalities

- Retain existing public car parking provision within consolidated deck structures, with attractive façades or sleeved by other development

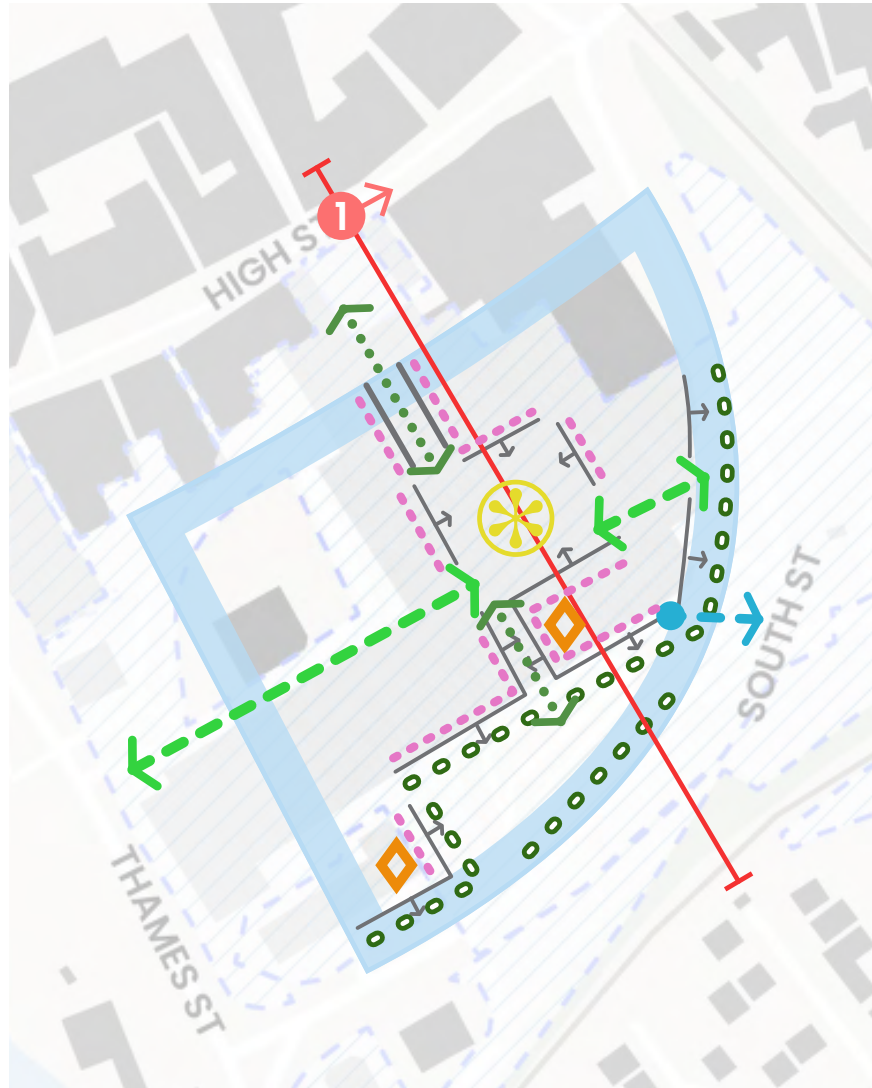
#### AOC-ST7e Detail & Richness

- Roofs of taller buildings should provide visual interest with distinctive form, and with variation when viewed from a distance.



## AREA TYPE CODING PLAN

This plan sets out where design requirements apply within this Area Type.



1 Section location

2 Allocated site in Local Plan

### THE STREET & GROUND FLOOR

— Building Line

→ New Active Frontage

●●● Retail / Flexible Commercial Ground Floor

←● Key Overlooking Location

..... Existing path or active travel street to connect to

--- New active travel street connection

### OPEN SPACES

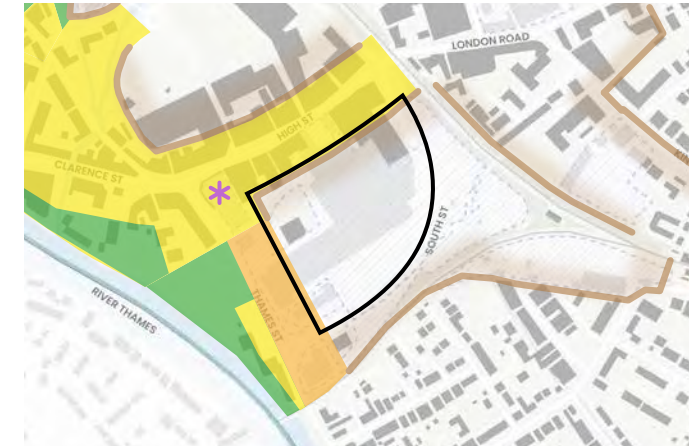
☼ New open space / public realm

●●● Street Planting & Greening

### DETAIL & RICHNESS

◇ Marker Building

## TRANSITIONAL EDGES PLAN



Transitional Edge, where heights step down to blend with prevailing built form and preserve street scale.

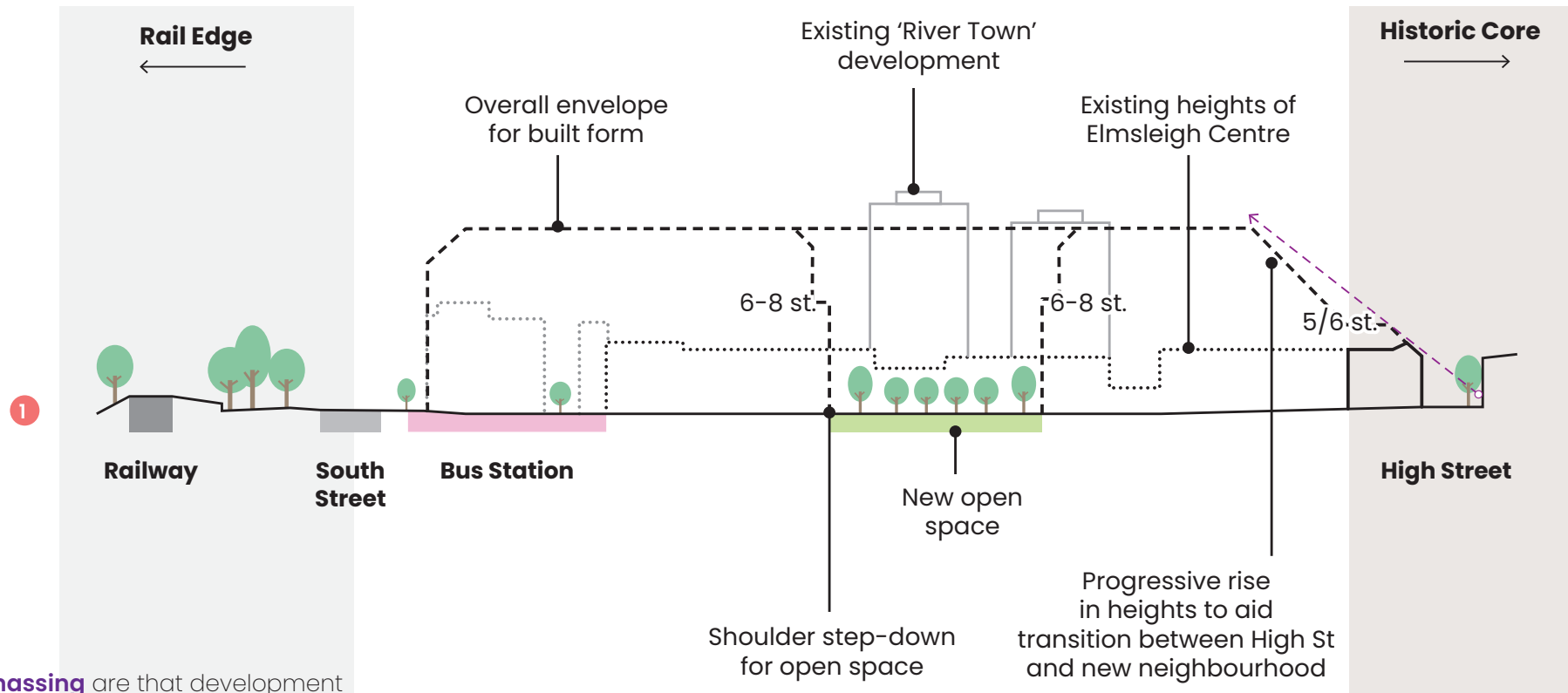
See Section 1 on following page for explanation and key principles on transitional edges.



## (AOC-ST7b) HEIGHTS AND SCALE PRINCIPLES

This diagram illustrates overall principles for the scale and **massing** in the Elmsleigh area.

For section location see Coding Plan.



Key principles for **massing** are that development **must**:

- Step down towards the permitted heights in the High Street (5-6 storeys, approx 18m)
- Ensure that views from street level in the High Street are not interrupted by new built form in the Elmsleigh Area
- Have a podium or base facade of up to 8 storeys (approx 24m) surrounding any new public open spaces, with taller buildings set back above a **'shoulder'**

## AOC-ST7f KEY OPEN SPACE REQUIREMENTS

Proposals for a new open space **must** include:

1. Key connections to High Street, Bus Station and Memorial Gardens
2. Potential secondary connection to the northeast
3. Green open space or new public realm with trees and planting
4. Retail **active frontages** facing space with associated 'spill-out' public realm
5. Built form to create active edge along southwestern frontage
6. Tree-lined, overlooked connection towards the river and Memorial Gardens

## EXAMPLES AND PRECEDENTS

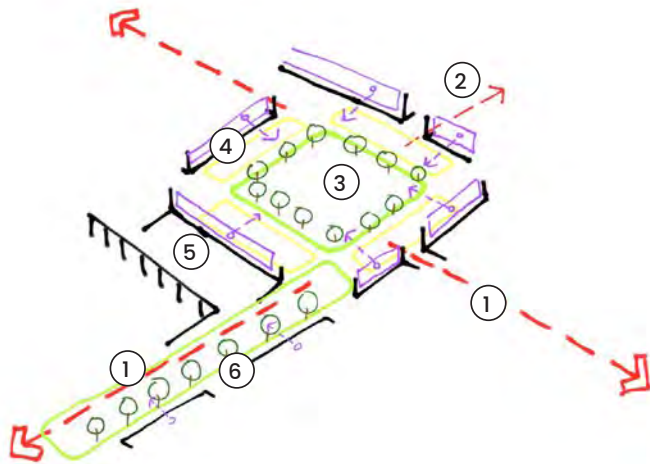
Development in this area **could** implement the following design features, character and opportunities.



Variety in materials, architecture and roof types to provide variation while maintaining fine grain, human-scale streets.



Creation of an open space with a distinct urban character, variety of spaces and uses within.



Illustrative approach to applying the key design requirements



Use of patterns, materials and detailing in the public realm to create attractive pedestrian-priority streets that lead people through the neighbourhood.

## Railway Edges: Improving the Quality of Streets and Spaces



### DESIGN AIMS

New development in these locations will ensure that noise levels and quality of spaces next to the railway line are acceptable for residents. They should create good quality street environments and ensure that any existing or new connection points across the railways are safe, overlooked and become part of the wider street network.

### Existing Context & Place Identity

Two busy railway lines divide Staines-upon-Thames town centre from its surrounding suburbs. On the river/town centre side of these lines are a series of car parks and other areas where new development is proposed. There is little existing built form but on the other side of the railway lines are primarily existing suburbs.

### AOC-ST8 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

#### AOC-ST8a The Street & Ground Floor

- Create a strong street edge with a podium or continuous base building to limit impact of railway on wider town centre neighbourhoods. See following pages for principles.
- Safeguard potential new link across Egham railway tracks towards railway station. See Coding Plan.
- Incorporate new street trees and planting along main roads, particular South St to aid transformation to multi-modal street.

#### AOC-ST8b Scale & Massing

- Heights to transition to surrounding built form as set out on the transitional edges plan
- **Massing** led by creating a comfortable street scale with the tightest width:height ratio of around 1:1. Heights that would break this street aspect ratio are permitted through the use of a '**shoulder**' where heights step back from the street edge.
- General presumption of high residential densities and high **Floor Area Ratio** of 3.0 and above, particular close to major roads such as South St.

#### AOC-ST8c Open Spaces

- Create a gateway space at edge of Elmsleigh neighbourhood. Requirements are set out under 'Key Open Space Requirements' on following page.

#### AOC-ST8d Homes & Practicalities

- Retain existing public car parking provision within consolidated deck structures, with attractive façades or sleeved by other development

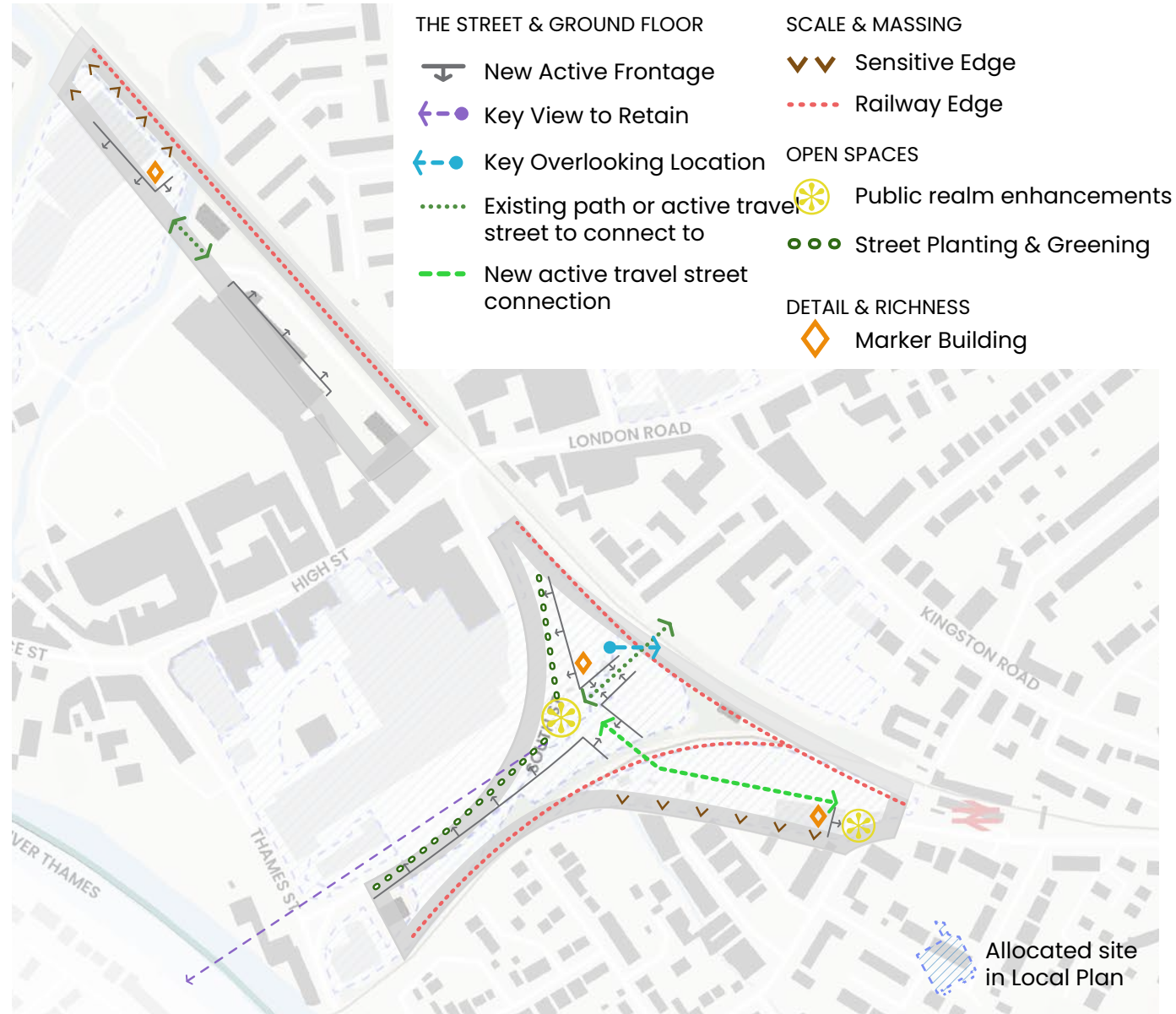
#### AOC-ST8e Detail & Richness

- Roofs of taller buildings should provide visual interest with distinctive form, and with variation when viewed from a distance.



## AREA TYPE CODING PLAN

This plan sets out where design requirements apply within this Area Type.



## TRANSITIONAL EDGES PLAN



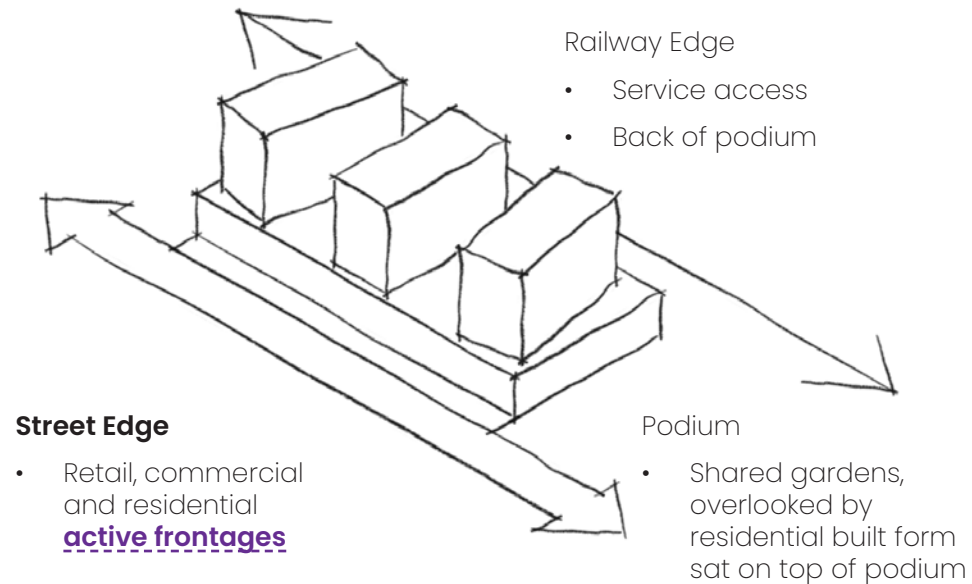
Transitional Edge, where heights step down to blend with prevailing built form and preserve street scale.

## AOC-ST8f RESPONDING TO A RAILWAY LINE

This diagram sets out overall principles and an illustrative example for arranging built form along a railway edge.

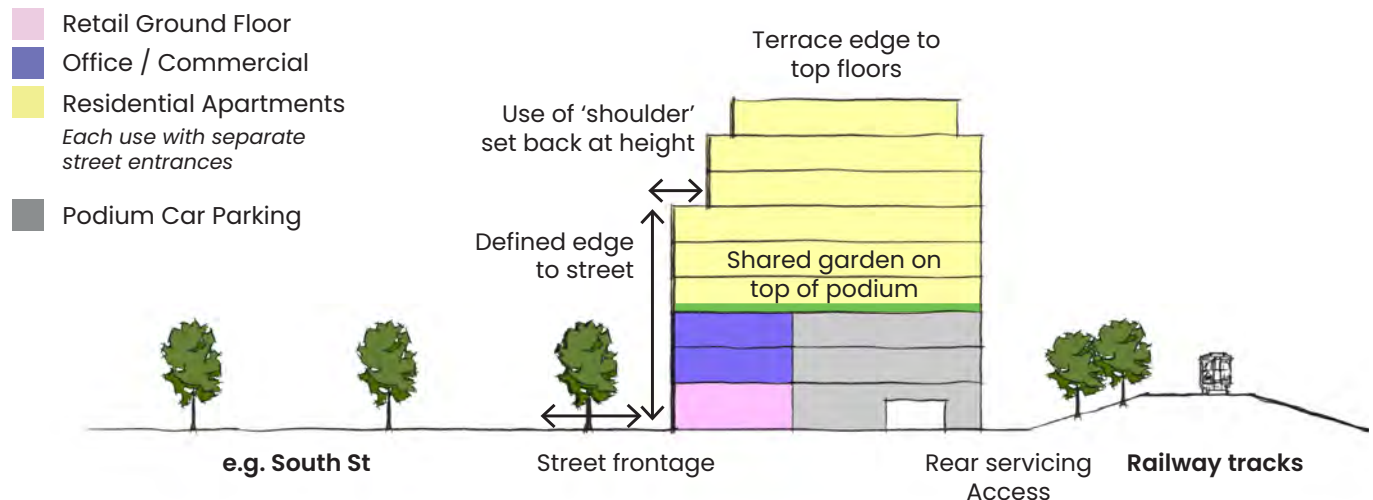
Key principles for massing are that development **must**:

- Have a continuous podium, plinth or base building along the street edge that provides good enclosure and active frontage to the street
- Arrange taller massing above this in a way that provides residents with a variety of views to both town centre and across surrounding areas and the railway lines



## CREATING A STREET EDGE

This diagram sets out overall principles for how the use of a base podium with active frontage can create an attractive street environment, vertical integration of different uses, and how massing should relate to the street and the railway lines.



## EXAMPLES AND PRECEDENTS

Development in this area **could** implement the following design features, character and opportunities.

- 
- A modern, multi-story residential building with a dark, textured facade and large glass windows. The building features several balconies with glass railings and is situated on a grassy area with a stone wall in the foreground.

A modern, multi-story apartment building with a light-colored facade and dark-framed balconies, situated on a city street under a blue sky with clouds. The building features a mix of brick and concrete materials, with large windows and balconies. A few people and a dog are visible on the sidewalk in front of the building.

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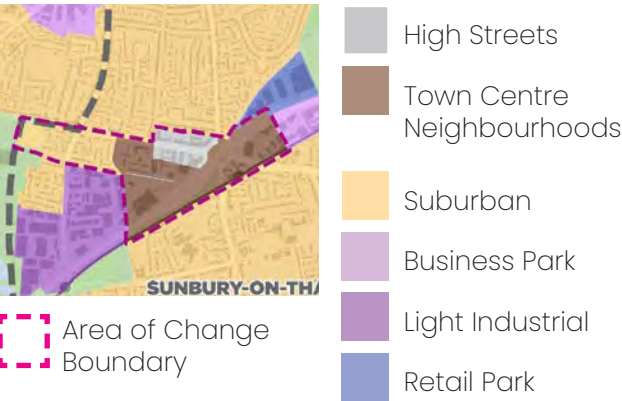


# Sunbury Cross

## OVERVIEW

This section sets out further Design Requirements and guidance for development in Sunbury Cross town centre.

## EXTENT AND CONTEXT



## DEVELOPMENT CONTEXT

Sunbury Cross could see significant change in coming years. The [Local Plan](#) allocates around 1,000 new homes to be built, mostly at higher densities and resulting [floor area ratios](#) than the prevailing built form, and on sites that are poorly connected within a challenging location.

Some new homes will be on sites that are currently undeveloped and others will be redevelopment sites. The Design Code sets out the requirements for all these areas, and also sets out a vision of how to transform the area over time.




The varied existing context of low and high-rise development in Sunbury Cross

## DESIGN AIMS

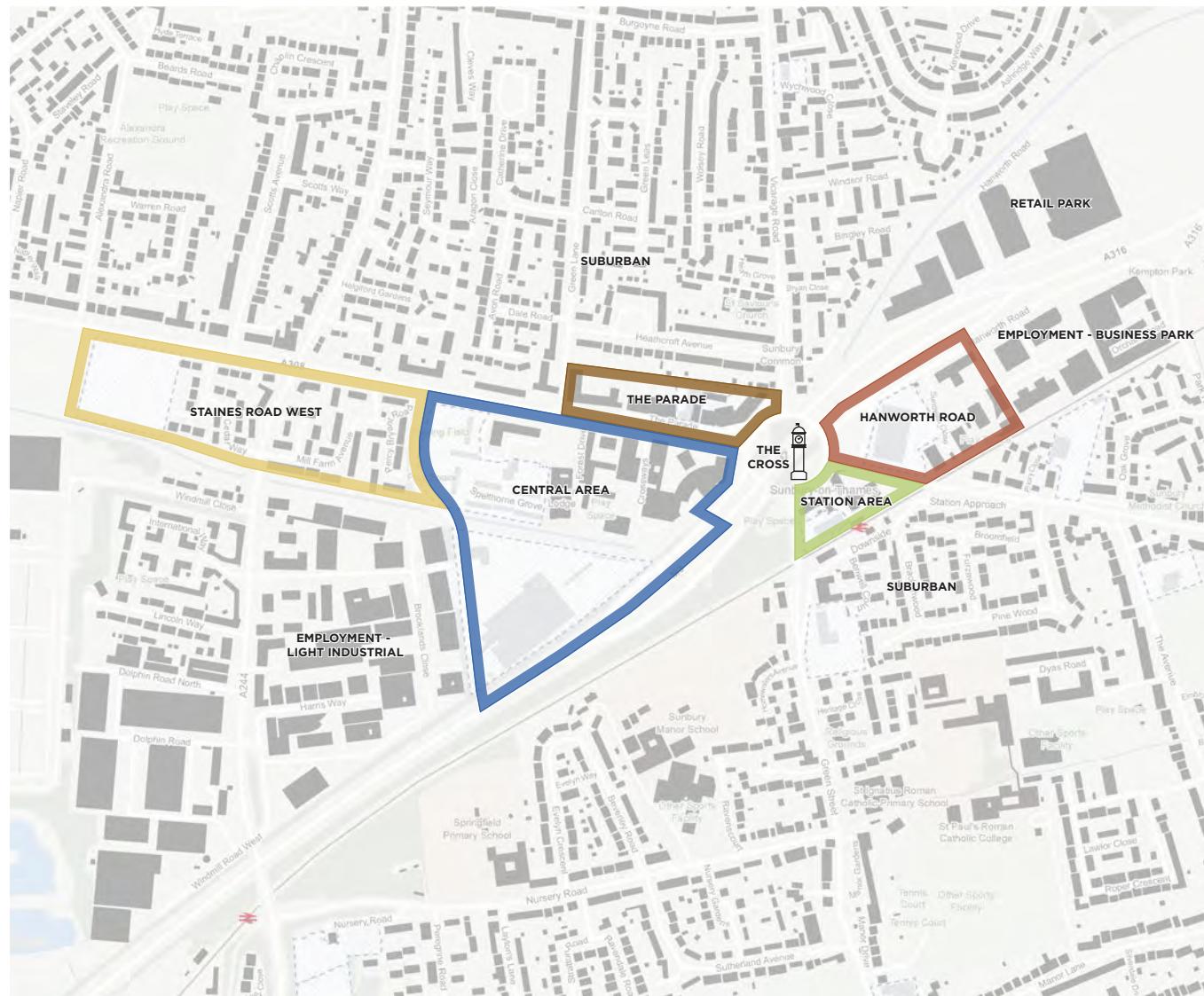
New development in Sunbury Cross town centre **will**:

- Create connections between areas of new development and the core of Sunbury Cross
- Improve the road safety and personal security of the public realm and streets
- Reduce severance created by infrastructure
- Link the railway station to the main shopping area
- Create a more human-scale place and street environment
- Create much-needed green open spaces for residents and the community

 **Find out more** background information about the borough in Appendix A 'Understanding Spelthorne Today'.

 Personal and road safety, noise, air quality and quality of life are priorities for the community in Sunbury Cross.

## Area Types



Within the Area of Change, more detailed requirements are set out by finer-grain Area Types. Each Area Type in the town centre is considered by whether it will largely retain its existing character and contribution to overall place identity, or whether it is likely to change substantially in character.

### Incremental Change

Retaining existing character and place identity  
Design requirements strongly reflect context.



#### THE PARADE

General requirements for the High Street Area Type (Chapter 4) apply.



#### STAINES ROAD WEST

General requirements for Suburban Area Type (Chapter 4) apply.



#### STATION AREA

General requirements for the Town Centre Neighbourhoods Area Type (Chapter 4) apply.



#### HANWORTH ROAD



#### CENTRAL AREA

### Transformative Change

Defining a new character and place identity  
Design requirements set key parameters only.



#### THE CROSS

The Cross itself, now the M3 junction, lies mostly outside of the scope of the Design Code. Principles for change are set out in the Spatial Vision on the following pages.



## A FUTURE VISION FOR SUNBURY CROSS

Sunbury Cross is a place negatively impacted by a complex mix of major roads, railways and water supply infrastructure. It was transformed in the late 1960s and early 1970s by the M3 motorway and grade-separated junction, which continues to detract from its quality as a place, and causes severance, noise, safety and air quality issues.

Sunbury Cross can be more than a motorway junction with hard edges, railway and road severance, aqueduct edges, wedges of land, a retail mall and section of frontage shops and disconnected residential neighbourhoods. The spatial vision sets out the possibilities that could be pursued through wider work with multiple planning, highway and transport authorities as well as individual schemes.

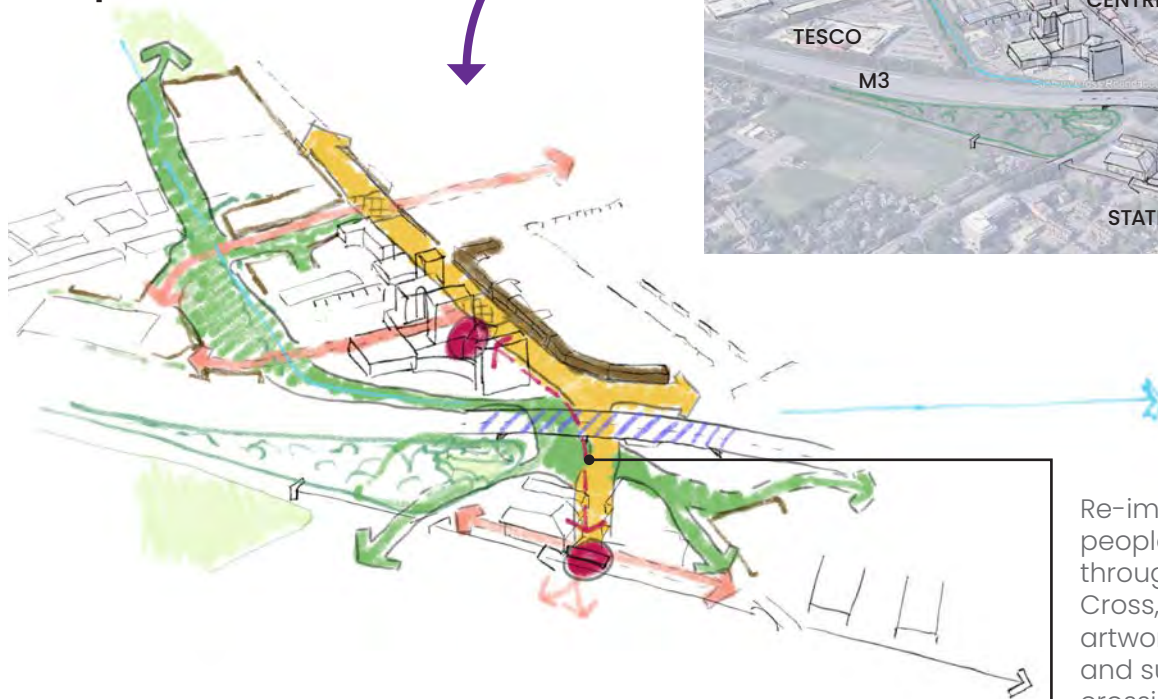
The Design Code explores how new development can help to make this happen through incremental change. Major change at Sunbury Cross requires further work and co-ordination amongst all parties to move towards a transformative masterplan.

The vision's key aims are:

- Connect existing and new neighbourhoods with each other and creating a sense of place
- Reuniting East and West Staines Road
- Connect the centre with the railway station
- Create new green spaces and links

Many of the key changes at Sunbury Cross will be beyond the scope of individual applications, requiring wider co-ordination on changes to infrastructure. Designs **should** respond to the potential for change in the future and make appropriate provision.

### Concept



### Key Themes and Projects

#### Short Term

- Linking the centre and the station

#### Medium Term

- Transforming Staines Road West
- New links between neighbourhoods
- Human-scale built form
- New green spaces and networks

#### Long Term

- M3 junction reconfiguration





**Short Term**

Linking the centre and the station



Top: Phoenix Flowers, M8, Glasgow (7N Architects)

Bottom: Toronto Gardiner Freeway Park

Providing an attractive, safe and clear surface route through existing space under the M3 would improve the ease of walking and cycling movement for all between Sunbury Common / the centre and Lower Sunbury / the station. This can be accomplished through the use of artwork, improved lighting, activated useful spaces and passive surveillance from surrounding built form.

**Medium Term**

Transforming Staines Road West



Exchange St 'Grey-to-Green' transformation, Sheffield

Staines Road West is currently a major dual carriageway and a barrier for moving between shops and neighbourhoods. New green infrastructure, public realm changes and a transformation to a 'boulevard'-style environment would create a healthier, more attractive street environment.

**Medium Term**

New green spaces and networks



Mayfield Park, Manchester

The Staines Aqueduct is a major piece of infrastructure severing links between neighbourhoods. Although access to the aqueduct may need to remain restricted, it could form the basis of a future green network linking neighbourhoods together and contributing to a greener, healthier Sunbury Cross.

**Long Term**

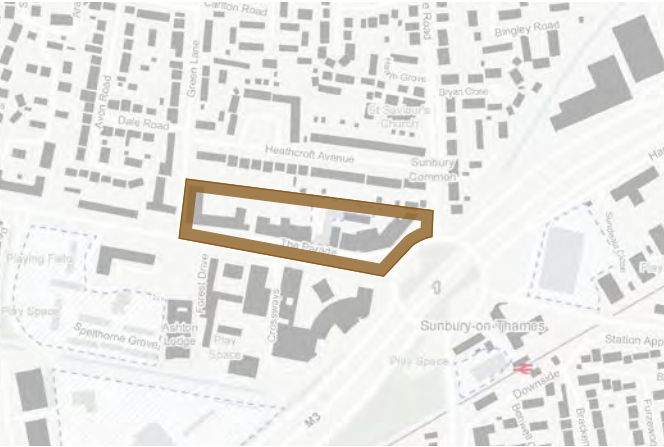
M3 junction reconfiguration



Masshouse Circus transformation to Masshouse Plaza, Birmingham

The M3 junction and flyover dominate Sunbury Cross today. Long-term, this should change, following precedent from other cities across the world who have successfully reconsidered the need for urban motorways

**The Parade:** The Historic Link to the Past in Sunbury Cross



**DESIGN AIMS**

New development in this area **will** respond to the strong existing building line and low-rise built form, with some intensification along Staines Road West.

**Existing Context & Place Identity**

The Parade, a row of shops and commercial buildings, is one of the few areas of Sunbury Cross that retains its more traditional built form. To the north it borders a largely post-war suburban area, and it thus forms an important transitional area from the Centre to the south.

**AOC-SC1 DESIGN REQUIREMENTS**

General requirements for the **High Streets Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

**AOC-SC1a Building Heights**

- Heights of up to 4 storeys (approx 12m), plus 1 dormer/mansard storey integrated in roof, in compliance with maximum heights plan

**AOC-SC1b Building Line**

- Match existing building line along Staines Road West

**AOC-SC1c Building Grain**

- Typical building frontage grain of 6-10m

**AOC-SC1d Vertical Mix of Uses**

- Ground floor commercial space
- Apartments located above ground floor uses

**AOC-SC1e Public Realm**

- Incorporate new street trees and planting along Staines Road West
- Future-proof designs for potential removal of frontage service access road
- Rear parking courts overlooked by built form
- Minimise access points to rear parking courts through main frontage
- Entrances to dwellings from the street

**AOC-SC1f Facades, Detail & Richness**

- Pitched roofs facing the street or gable-end, to reflect adjacent buildings
- Windows on frontage to match surrounding rhythm and characteristics
- Materials and architectural detailing to respond to prevailing form

**BUILDING HEIGHTS PLAN**



Heights typically up to:

5 storeys (approx 15m)

Heights are to be measured from pavement level to the eaves.

Typical storey heights for different uses are:

- Residential: 3m
- Commercial / Office: 4m
- Ground Floor Retail / Commercial: 4.5m

## Staines Road West: Connecting new Development to the Surroundings



### DESIGN AIMS

New development in this area will front onto Staines Road West and address the Sports Club fields to the west. Intensification of densities and built form over the prevailing area is supported, provided there is a transition in built form to existing areas within and adjacent to the area.

### Existing Context & Place Identity

This area type runs between Staines Road West and the Staines Aqueduct. It contains two low-rise cul-de-sac style developments divided by Windmill Road, and the eastern boundary is an **active frontage** onto Escot Road.

### AOC-SC2 DESIGN REQUIREMENTS

General requirements for the **Suburban Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

#### AOC-SC2a Edges

- **Active frontage** facing onto Staines Road West following 'Dual Carriageways, Urban Road' edge type (Chapter 4)
- **Active frontage** overlooking sports field to the west following 'Open Spaces' edge type (Chapter 4)
- **Active frontage** facing aqueduct to south, following 'Watercourses' edge type (Chapter 4)
- Built form on allocated site backing onto existing residential gardens to the west following 'Residential, Backing Onto' edge type (Chapter 4)

#### AOC-SC2b Streets

- New streets to comply with street types set out under 'New Residential Neighbourhoods' (Chapter 4)
- Continuous grid of new streets within allocated site, no use of cul-de-sacs
- Connections from new development to existing streets and cul-de-sacs

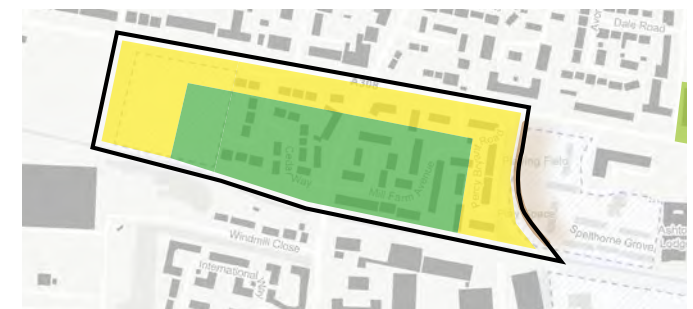
#### AOC-SC2c Open Spaces

- At least one new open space within allocated site, within homes
- Potential for new open space adjacent to aqueduct at southern edge of allocated site

#### AOC-SC2d Built Form

- Heights to comply with the maximum heights plan
- Observe building line on Staines Road West
- Pitched roofs on all buildings

### BUILDING HEIGHTS PLAN



Heights typically up to:

- 3-4 storeys (approx 12m)
- 6 storeys (approx 18m)



Station Area: Improved links to public transport



**DESIGN AIMS**

New development in this area will enable better accessibility of the station for pedestrians and cyclists, provide frontage to surrounding roads, and create a more consistent built form. Significant intensification is possible here in the context of the railway station as an important public transport link.

Existing Context & Place Identity

The area around Sunbury Station is a small pocket of development cut off from its surroundings by the M3, railway line and the feeder roads to Sunbury Cross roundabout. Land uses are largely commercial with no consistent architectural style, building line or grain.

AOC-SC3 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

AOC-SC3a The Street & Ground Floor

- Provide walking and cycling access to the station from Green Street and Staines Road East.
- No on-street and frontage parking on Station Road to make it a safer, more welcoming space for walking and cycling.
- Limit the number of vehicle accesses across footways from Station Road.
- Provide more pedestrian public realm along Station Road
- A consistent building line on Station Road with no 'leftover' spaces
- Incorporate new street trees and planting along Station Road
- Active frontages and passive surveillance facing Green Street and Staines Road East

AOC-SC3b Scale & Massing



- Heights to transition to surrounding built form as set out on the transitional edges plan
- Grain of Station Road frontage 6-10m wide

AOC-SC3c Detail & Richness

- Roofs of taller buildings should provide visual interest with distinctive form, and with variation when viewed from a distance.

TRANSITIONAL EDGES PLAN



-  Transitional Edge, where heights step down to blend with prevailing built form and preserve street scale.
-  Gateway Edge, where heights step up to meet The Cross or provide an edge to the M3

## Hanworth Road: Changing uses and the quality of spaces around



### DESIGN AIMS

New development in this area will improve connections to the rest of Sunbury, and create more attractive and inspiring places for people to live. High-density development fronting onto the roundabout will make efficient use of land in a well-connected location.

### Existing Context & Place Identity

The area to the north of Staines Road East is bounded by the railway line and the M3. Early 2000s apartment buildings and other coarse-grain development is changing the use of a former commercial and industrial area, but the area is severed from the surroundings by major infrastructure, and is dominated by surface car parking.

### AOC-SC4 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements.

#### AOC-SC4a The Street & Ground Floor

- **Active frontage** to face Staines Road East
- **Active frontage** to face Hanworth Road
- Planting and street trees on Staines Road East and Hanworth Road
- Direct pedestrian access from new development to Sunbury Cross roundabout

#### AOC-SC4b Scale & Massing

- Heights to transition to surrounding built form as set out on the transitional edges plan, with heights greatest near the roundabout
- Building typologies of linear block, villas and podiums with towers

#### AOC-SC4c Open Spaces

- Open space to be screened from the M3 and Sunbury Cross roundabout by interposed built form

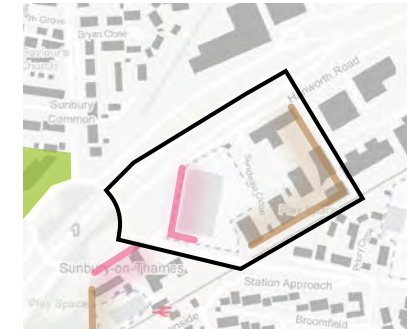
#### AOC-SC4d Homes & Practicalities

- Podium or shared rear courtyard car parking, accessed from Hanworth Road

#### AOC-SC4e Detail & Richness

- Roofs of taller buildings should provide visual interest with distinctive form, and with variation when viewed from a distance.

### TRANSITIONAL EDGES PLAN



**Transitional Edge**, where heights step down to blend with prevailing built form and preserve street scale.

**Gateway Edge**, where heights step up to meet The Cross or provide an edge to the M3

Central Area: New and Renewed Connected Neighbourhoods



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DESIGN AIMS

New development in this area will create new connections, improve the quality surveillance and road safety of the public realm. It will create a more human-scale place with fewer opportunities for crime or unused leftover space. New homes will have access to safe green open spaces on their doorsteps.

Existing Context & Place Identity

The Central Area of Sunbury Cross contains a wide mix of uses and built form, with a number of taller towers and a poor, car-dominated and broken-up street environment. To the northeast, the shopping centre is designed for access by car and turns its back on surrounding streets. To the south, a large supermarket sits within surface parking and with poor, pedestrian accessibility that feels unsafe. In the northwest, a number of residential developments are bounded by infrastructure and lack connectivity and quality open space.

AOC-SC5 DESIGN REQUIREMENTS

General requirements for the **Town Centre Neighbourhoods Area Type** (Chapter 4) apply. Development in this area type **must** comply with the following additional design requirements. Where design requirements have a spatial requirement (e.g. location of key frontages) these are set out on the Area Type coding plan on the following page.

AOC-SC5a The Street & Ground Floor

- Street trees on Staines Road West
- New development to provide **active frontages** and passive surveillance along all streets, particularly at locations highlighted on plan
- Repair building line along Staines Road West (see diagram below)

AOC-SC5b Scale & Massing

- Heights to transition to surrounding built form as set out on the transitional edges plan, with heights greatest near the roundabout and M3
- Building typologies of linear block, villas and podiums with towers

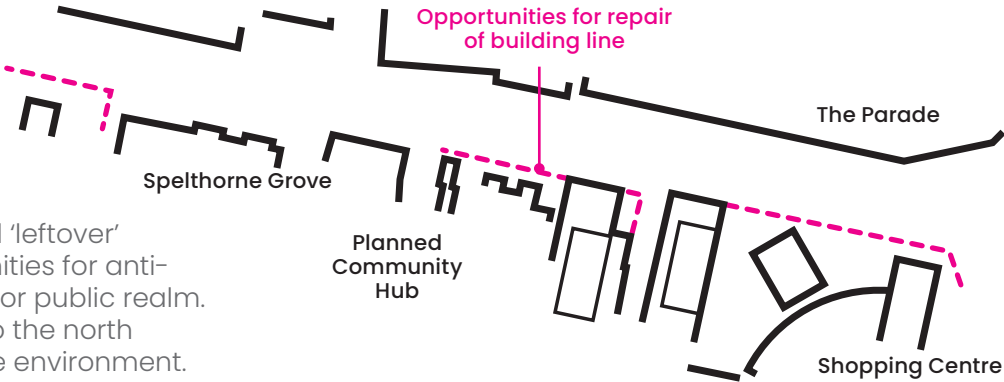
AOC-SC5c Open Spaces

- Relocate existing open space in Spelthorne Grove to be surrounded and overlooked by built form, providing public pedestrian link through to supermarket
- Small overlooked green open space to provide connection from supermarket to shopping centre

AOC-SC5d Detail & Richness

- Use **marker buildings** to terminate views as noted on plan and provide legibility
- Roofs of taller buildings should provide visual interest with distinctive form, and with variation when viewed from a distance.

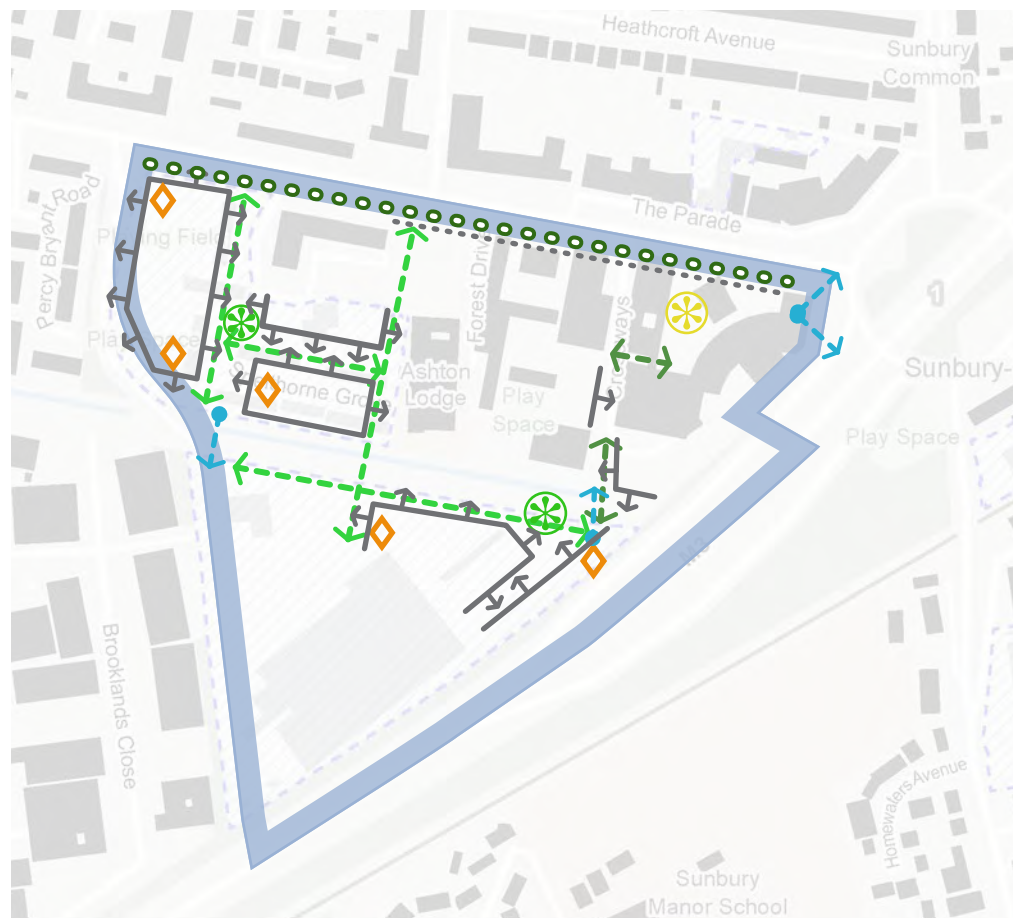
Comparison of existing building lines on Staines Road West. Broken up building line with tall buildings to the south overlooking creates spaces that lack passive surveillance or are unused 'leftover' spaces, creating opportunities for anti-social behaviour and a poor public realm. Continuous building line to the north creates a 'High Street' type environment.





## AREA TYPE CODING PLAN

This plan sets out where design requirements apply within this Area Type.



Allocated site in Local Plan

### THE STREET & GROUND FLOOR

- New Active Frontage
- Repaired Building Line
- Key Overlooking Location
- Existing path or active travel street to connect to
- New active travel street connection

### OPEN SPACES

- New green open space
- Public realm enhancements
- Street Planting & Greening

### DETAIL & RICHNESS

- Marker Building

## TRANSITIONAL EDGES PLAN



Transitional Edge, where heights step down to blend with prevailing built form and preserve street scale.



Gateway Edge, where heights step up to meet The Cross or provide an edge to the M3







# Preparing your Application



# What you need to do now

When submitting your planning application, you will need to demonstrate that you have complied with the necessary requirements set out in the Code. You will also need to demonstrate that you have followed an appropriate and comprehensive [design process](#).

To demonstrate compliance, you will need to submit:

- A completed copy of the relevant Design Code checklist for the Area Type your application falls within. These can be found at the end of this chapter.
- For major applications (i.e. 10 dwellings or more, or where the floorspace is 1000 sq. metres or more, or the site is 1ha or more) a Design and Access Statement which sets out the design process undertaken

## DESIGN CODE CHECKLISTS

A Design Code Checklist must be completed for all applications, to self-assess compliance with the Code and to clearly signpost where information about compliance is held within the submitted application documents.

You should submit the relevant Area Type checklist with your application (e.g. Town Centre Neighbourhood), based on where your application is. In Inner Suburban or Suburban Area Types, you should submit the relevant checklist for your development type (e.g. Residential Extension or New Residential Neighbourhoods).

All Checklists can be found at the end of this Chapter, and are available for separate download on the Spelthorne Borough Council website.

## DESIGN AND ACCESS STATEMENT

Design and Access Statements (DAS) are required for major applications, i.e. 10 dwellings or more, or where the floorspace is 1000 sq. metres or more, or the site is 1ha or more.

Through the Design and Access Statement applicants need to explain and justify the choices made which will achieve high quality outcomes by providing a contextual survey and analysis, showing how this has informed the design proposals.

The Design and Access Statement will vary in its detail and coverage depending on the type and scale of application. It could include the following, aligned to the Design Process set out in Chapter 2:

### Step 1: Understanding The Site and Context

Plans and diagrams showing an understanding of the surrounding character, features, movement, planned change and other baseline information set out in Chapter 2. This should inform plans of site constraints and opportunities for the proposal and wider area.

### Step 2: The Vision

An overview of what the proposal intends to achieve, with a narrative and key visualisations.

### Step 3: Developing and Testing Options

An explanation of the design development taken as part of the proposal's design, including input and changes derived from community and stakeholder engagement.

### Step 4: Site Parameters

For larger proposals, the key parameters and strategies for movement, green and blue infrastructure, sustainability, built form and land use that underly the detail proposals, and have been informed by the site analysis.

### Step 5: Resolving the Details

Plans, elevations, visualisations, sections and other annotated technical diagrams that show clearly what is being proposed, to an appropriate level of detail.

# How to get further help

Spelthorne Borough Council will support developers and applicants to deliver high-quality design and place-making, through a comprehensive design process that aims to deliver the outcomes set out in the Design Code.

The Council offers a number of services to help support the [design process](#) set out in Chapter 2.

## PRE-APPLICATION SUPPORT

This is the starting point for all proposals. Come to us at an early stage to discuss your proposal, obtain feedback on your emerging scheme and how best to improve the design. We will be able to ensure that wider council teams are able to feed into early discussions to ensure integrated thinking and better design outcomes.

Although there is a cost to applicants, you will benefit from a better-quality application and a clearer route to the determination of your planning application. The iterative nature of design means that, particularly for larger applications, a number of pre-application reviews are advised as you progress your design proposals.

## PLANNING PERFORMANCE AGREEMENTS (PPAS)

These are voluntary agreements between the applicant and the Spelthorne Council that set out the actions, resources and timescales for handling a particular planning application. They can be used to support good design through a continuous and iterative process of officer engagement across the council, and may include dedicated design workshops.

They can be used for any type of planning application but usually for large-scale, complex proposals. They can be used at any stage from early brief development through to conditions and reserved matters. Fees for PPAs depend on the size and complexity of the proposal.

## DESIGN REVIEW

Design Review is an independent and impartial evaluation of proposals best undertaken at pre-application stage. It is a collaborative process, where constructive feedback can be given to improve the design quality of proposals.

Local Plan policy PS2 encourages Design Review to be undertaken for proposals that have significant impacts in relation to design or public interest.

## FURTHER GUIDANCE

A wide range of further guidance has been published by other organisations addressing in more detail particular aspects of design. They can provide further assistance to applicants.

- Surrey Healthy Streets Design Code
- Manual for Streets
- Trees and Design Action Group – guidance on street trees and green infrastructure
- Secured by Design
- Natural England Green Infrastructure Planning and Design Guide
- Active Design (Sport England)
- Building with Nature
- Building for a Healthy Life
- LTN 1/20 – Cycle Infrastructure Design
- Spelthorne Climate Change SPD
- LETI Climate Emergency Design Guide
- CoMoUK – guidance on Mobility Hub design
- Surrey Sustainable Drainage System Design Guidance
- Ciria UK – The SuDS Manual

# Glossary

**Above Ordinance Datum (AOD)**

Vertical datum used by the Ordinance Survey as the basis for deriving altitude. Building heights and parameter plan height limits are expressed in terms of AOD.

**Access**

This term has two broad meanings: The route(s) to a site and the route layout within a site, related to different modes of movement (foot, cycle, vehicular), and the inclusive approach to design, which aims to create a built environment which is accessible to everyone, regardless of age or ability.

**Active frontage**

A building frontage to the public realm which is characterised by entrances and windows (residential, commercial or retail), allowing interaction between the public realm and the use facing the street, as well as passive surveillance of the public realm.

More information on active frontages in town centre neighbourhoods is found under TC-S1.

**Area type**

Parts of the local area that share common features and characteristics. For example, a suburban area type might bring together a number of different streets with common densities, heights, building line, under the umbrella term “Inner Suburbs”. Common rules and parameters can then be applied to the “Inner Suburbs” area type in the design code. Example area types are provided in the National Model Design Code, but area types should be defined locally.

The borough’s area types are defined at the start of Chapter 4. Staines-upon-Thames and Sunbury Cross Areas of Change have more detailed Area Types defined in Chapter 5.

**Boundary treatment**

The physical interface that delineates the public realm from a private building, crossing which enters a defensible zone before reaching the building entrance. Treatments can include planting, low fences or walls.

**Borough-wide**

Relating to the borough of Spelthorne.

**Block**

A building or set of continuous buildings within a plot.

**Building line**

The linear definition of a building’s frontage facing the street. Usually shared by different building typologies and sizes to organise the definition between the public street and private internal space of the building and urban block.

**Building height**

The height of a building measured AOD. For the purposes of determining the prevailing height in the area, the number of storeys can be also used.

**Character**

The combination of features of a building or a place that give it a distinctive identity compared with other buildings or areas.

**Contemporary development**

Contemporary development is the architecture of the 21st century. It is characterised by efficient layouts that use a combination of low rise, mid-rise and tall buildings in perimeter blocks to optimise capacity. These tend to be set within gridded street networks that are highly permeable and legible.

**Conserve**

Enhancing and protecting the existing character.

**Context**

The surrounding environment of a proposed development, including existing buildings, landscape and consented schemes.



**Defensible space**

The area occupying space between a building entrance and the boundary treatment. Typically associated with residential buildings, they provide a sense of spatial separation and visual privacy between the public street and private home at ground floor.

**Density**

In the case of residential development, a measurement of either the number of habitable rooms per hectare or the number of dwellings per hectare.

**Design code**

A set of illustrated design requirements that provide specific, detailed parameters for the physical development of a site or area. The graphic and written components of the code should build upon a design vision, such as a masterplan or other design and development framework for a site or area.

**Design-led approach**

Using urban design and architectural processes to prepare proposals that represent the optimum design response to a site, responding to the surrounding context of massing, connections, open spaces and other factors. This process should be evidenced through exploring a range of options.

**Design process**

The process of developing a proposal for a site. The design process is expected to follow good urban design principles set out in the National Design Guide and the National Model Design Code.

A full explanation of what is expected is contained in Chapter 2.

**Dual aspect**

A habitable unit with windows on two walls facing different directions.

**Elevation**

A vertical projection of one side of a building, showing a single façade.

**Enclosure**

The extent to which streets and open spaces are visually defined by buildings, walls and trees.

**Façade**

The external faces of a building, characterised by a choice of materials, windows, doors, entrances, and openings.

**Fenestration**

The arrangement of entrances, windows, balconies, and other openings on a building facade.

**Formal / informal**

A formal layout of streets and building groups is characterised by symmetrical or geometric plans and elevations. The features of an informal design include layout and elevations which are asymmetrical, winding and which relate to natural site characteristics.

**Floor Area Ratio (FAR)**

A metric used to calculate the density of developments regardless of building type and use. FAR is expressed as the ratio of a building's total floor area to the size of the plot upon which it is built.

**Frontage**

The front face of a building articulated with entrances and windows. Well defined frontage enables overlooking from the building out into the street or space, creating a positive relationship between the two.

**Gateway**

The marking of a point of entry to an area of character or to a specific development through a change built form, landscape materials or a key view to signify and reinforce the transition.

**Grain**

The general shape and direction of building footprints. Fine grain refers to the higher intensity of smaller plots or streets. Coarse grain refers to larger scale plots with fewer roads.

**Green infrastructure**

A network of multi-functional green space, urban and rural, which is capable of delivering a wide range of environmental and quality of life benefits for local communities.

**Gridded Street Network**

A style of street network defined by a repetition of streets and urban blocks intersecting at right angles, comprising an overall grid structure. Regular grid patterns allow for ease of accessibility and legibility.

**Hierarchy**

A logical sequence of spaces, streets or building forms, increasing or decreasing in size or density throughout a development.

**Impermeable**

An unconnected street or pedestrian network with a low frequency of routes, inhibiting easy passage of movement. Often associated with coarse urban grain patterns or illegible layouts such as cul-de-sacs or free form block estates.

**Innovative development**

A departure from both the traditional and modern approaches. Innovation could be technological or design-related.

**Landmark building**

Landmark buildings are prominent buildings that are easily recognisable and have significant cultural or historical value. Landmark buildings do not have to be tall, and they should be used sparingly in development.

More information on landmark buildings in town centre neighbourhoods is found under TC-D2.

**Layout**

The layout of a block relates to the arrangement of buildings, open spaces and streets and the relationship between these components in creating an efficient, positive and legible environment.

**Legible**

The combination of buildings, streets, trees, and open spaces that use visual cues to create an intuitive and easily navigable environment.

**Linear block**

A building consisting of stacked apartments and maisonettes organised in a linear urban form. Can be stand alone and running parallel with a street to form a contemporary terrace, or form part of a block that forms the perimeter between the public street and private internal space.

More information on linear blocks in town centre neighbourhoods is found under TC-M2.

**Listed Building**

A building that is included on the List of Buildings of Special Architectural or Historic Interest administered by Historic England on behalf of the Secretary of State for Digital, Culture, Media and Sport. Listed buildings are graded I, II\* or II with grade, I being the highest. Buildings within the curtilage of a listed building constructed before 1948 are also protected. The significance of a listed building may be external and/or internal.

**Local Plan**

The plan for the future development of the local area, drawn up by the local planning authority in consultation with the community. In the context of the Spelthorne Design Code, this refers to the Spelthorne Local Plan 2022-2037.

**Low-rise buildings**

Low-rise buildings are classified as buildings up to and including 3 storeys e.g. up to 9 metres.

**Marker building**

Marker buildings are memorable buildings that stand out from the surrounding built form. They can help people to navigate and make the townscape more distinctive and interesting.

More information on marker buildings in town centre neighbourhoods is found under TC-D2.

**Massing**

The three-dimensional volume and structure of a building's urban form. Massing is expressed through the size, shape and scale of its different components. Commonly understood as the expression of a building without any finer architectural elements and details. Massing can influence the ways in which a building is perceived, particularly in regards to reducing the impact of visual bulk.

**Mews**

Traditionally a stables at the rear of a building along a back (service street). The term now typically describes quiet streets of smaller homes inserted within a larger block, with an intimate character and semi-private atmosphere.

More information on mews in town centre neighbourhoods is found under TC-M2.

**Mid-rise buildings**

Mid-rise buildings are classified as buildings between 4 and 6 storeys e.g. between 12 and 18 metres.

**Mixed-use / mixed-use development**

Provision of a mix of complementary uses, such as residential, community and/or leisure uses, on a site or within a particular area.

**National Model Design Code**

The National Model Design Code provides detailed guidance on the production of design codes, guides and policies to promote successful design.

**Overlooking**

A term used to describe the effect when a development or building affords an outlook over adjoining land or property, often causing loss of privacy.

**Over shadowing**

The effect of a development or building on the amount of sunlight presently enjoyed by a neighbouring property, resulting in a shadow being cast over that neighbouring property.

**Parade**

A continuous row of shops or commercial units, typically in the town centre. They sometimes have residential accommodation above.

**Passive surveillance**

Design that increases the occupation and/or visibility of a space to deter crime.

**Perimeter block**

A perimeter block is an urban form that concentrates the development of a city block along its outermost – or public – edges to strongly define a boundary between public and private or semi-private space. This form is highly efficient by making best use of available land and avoiding surplus spaces that lack clear role of function. The blocks themselves are impermeable but are set within a highly permeable street network.

**Permeable**

A connected street or pedestrian network with a high frequency of routes that allow easy passage of movement, often associated with fine urban grain patterns.

**Place**

A space in the built environment that has some meaning for people due to the activities and uses which characterise the space, or the quality of the space itself.

**Plot**

An area of developable land less public open space, primary road infrastructure, and non-developable areas.



**Plot coverage**

The proportion of a site that is occupied by a building's footprint. The plot ratio of a development is calculated by dividing the building's footprint by the total area of a site.

**Prevailing height**

The average or typical building height within an area. Please see Building height above.

**Primary Street**

The principle route or main access. Dominant to the secondary street network joining it. Often wider and carrying more significant traffic volumes or a route for public transport.

**Public realm**

The public realm is any part of a site, area, village, town or city that everyone can use and enjoy, including streets, squares and parks. The public realm is very important for pedestrian movement, as it connects various places and buildings.

**Rhythm**

The repeated pattern of an element such as a building, street or architectural detail.

**Roofline**

The profile of the top edge of a building.

**Roofscape**

The appearance of buildings as seen along the skyline, as well as the uses and occupancies as seen from tall buildings.

**Roof Form**

The type of roof based on its three-dimensional size and shape, often belonging to and characteristic of different typologies. Roof forms can include fat, gabled, hipped, mansard, butterfly, saw-tooth and more.

**Scale**

Most commonly understood as building height, though scale is relative to another (usually neighbouring) building's height. It can also relate to the size of a building's different elements e.g. massing, fenestration, rather than purely its absolute building height.

**Secured by Design**

The national police scheme which aims to minimise crime and opportunities to commit crime through better design of buildings and places.

**Secondary Street**

Subordinate to the primary street. Often more local routes, within residential areas.

**Setting**

The physical (built and landscape), community and economic surroundings in which the development takes place.

**Set back**

A step-like recess in massing of upper storeys, used where proposed building heights exceed the shoulder height of street. This strategy can preserve the established street width ratio and allow daylight to reach lower storeys.

**Shoulder**

The part of a taller building where a set back occurs. It should be treated as a distinct part of an elevation, to crown the lower part of a building visible from the street. See also set back above.

More information on the use of shoulders can be found under TC-M3.

**Single aspect**

A habitable unit with windows on one walls facing a single direction.

**Storey / number of storeys**

Number of storeys is described as the number of floors in the building that have all internal perimeter walls of full floor height. If there is additional accommodation in the roofspace that is created within a pitched or similar style roof, where all perimeter walls are not of full floor height, this would not count as a full storey.

A building containing X number of full storeys with additional accommodation in the roofspace would be called 'X storeys with rooms in the roofspace'. If there are multiple (Y) floors within the roofspace this would be described as 'X storeys with rooms in the roofspace contained in Y floors'.

**Street hierarchy**

A system of classifying different routes within a movement network. This is principally based on the type and volume of movements a route supports, as well as its characteristics in terms of neighbouring building scale, use and enclosure. The character of a route can change along its length e.g. High Street along an arterial route.

**Suburban**

An area on the edge of a large town or city, typically residential in character. Suburbs became common in the UK during the 19th and 20th centuries when the development of rail and road transport made commuting viable.

**Taller building**

Building that exceeds prevailing height of the surrounding area (please see chapter 6.3).

**Traditional development**

Directly reflects the local vernacular and historic architectural styles, materials and features.

**Townscape**

The urban equivalent of landscape: the overall effect of the combination of buildings, changes of level, green spaces, boundary walls, colours and textures, street surfaces, street furniture, uses, scale, enclosure, views etc.

More information on townscape in town centre neighbourhoods is found under TC-D1.

**Typology**

The classification of buildings into typical and easily recognisable types, based on shared characteristics such as scale, massing, layout, architectural style and period. This organisational device can also apply to urban blocks e.g. Perimeter Block, Free Form Block.

**Urban Greening Factor (UGF)**

A tool used to evaluate the quality and quantity of natural features proposed as part of a development, such as planting, waterbodies and green roofs, collectively referred to as urban greening.

More information on calculating the Urban Greening Factor is found in Chapter 6.

**Villa blocks**

A building consisting of stacked apartments. A villa block is characterised by a central core and efficient circulation arrangement, typically with three to five dwellings per floor, per core. This enables habitable rooms to have views in multiple directions.

More information on villa blocks in town centre neighbourhoods is found under TC-M2.

**Wayfinding**

The process of navigating through and around the development, using spatial and visual clues and/or markers.

# High Streets

## Design Code Checklist

Development proposed within High Street Area Types **must** complete this checklist to self-assess compliance with the Spelthorne Design Code. Submit the completed checklist with your planning application.

REF	PG.	REQUIREMENT	DOES YOUR PROPOSAL COMPLY?				JUSTIFICATION If <b>Partially</b> or <b>No</b> , provide a reference to where in your planning application you have provided an evidenced justification If <b>N/A</b> please state why.
HS-1	44	Building Heights	Fully	Partially	No	N/A	
HS-2	44	Building Lines	Fully	Partially	No	N/A	
HS-3	45	Building Grain	Fully	Partially	No	N/A	
HS-4	45	Vertical Mix of Uses	Fully	Partially	No	N/A	
HS-5	46	High Street Public Realm	Fully	Partially	No	N/A	
HS-6	47	Shop Fronts	Fully	Partially	No	N/A	
HS-7	47	Facades	Fully	Partially	No	N/A	



# Town Centre Neighbourhoods

## Design Code Checklist

Development proposed within Town Centre Neighbourhoods **must** complete this checklist to self-assess compliance with the Spelthorne Design Code. Submit the completed checklist with your planning application.

REF	PG.	REQUIREMENT	DOES YOUR PROPOSAL COMPLY?				JUSTIFICATION If <b>Partially</b> or <b>No</b> , provide a reference to where in your planning application you have provided an evidenced justification If <b>N/A</b> please state why.
THE STREET AND GROUND FLOOR							
TC-S1	50	Active Frontages	Fully	Partially	No	N/A	
TC-S2	51	Spill-Out Space	Fully	Partially	No	N/A	
TC-S3	52	Street Networks and Design	Fully	Partially	No	N/A	
TC-S3a		Pedestrian Priority Streets	Fully	Partially	No	N/A	
TC-S3b		Multi-Modal Streets	Fully	Partially	No	N/A	
TC-S4	52	Street Trees and Planting	Fully	Partially	No	N/A	
SCALE AND MASSING							
TC-M1	54	Neighbourhood Massing Approach	Fully	Partially	No	N/A	
TC-M2	56	Development Typologies	Fully	Partially	No	N/A	
TC-M2a		Terraces, Back-to-Backs, Mews	Fully	Partially	No	N/A	

TC-M2b		Linear Blocks	Fully	Partially	No	N/A	
TC-M2c		Villa Blocks	Fully	Partially	No	N/A	
TC-M2d		Podiums and Towers	Fully	Partially	No	N/A	
TC-M3	62	Tall Building Design	Fully	Partially	No	N/A	
TC-M3a		Breaking Up Massing	Fully	Partially	No	N/A	
TC-M3b		Scale of the Street	Fully	Partially	No	N/A	
TC-M3c		Microclimate	Fully	Partially	No	N/A	
OPEN SPACES							
TC-O1	64	Neighbourhood Open Space Approach	Fully	Partially	No	N/A	
TC-O2	65	Safety and Security	Fully	Partially	No	N/A	
TC-O3	66	Public Open Spaces	Fully	Partially	No	N/A	
TC-O3a		Squares and Parks	Fully	Partially	No	N/A	
TC-O3b		Courtyards, Pocket Parks	Fully	Partially	No	N/A	
TC-O3c		Linear and Transit Spaces	Fully	Partially	No	N/A	
TC-O4	68	Shared / Communal Open Spaces	Fully	Partially	No	N/A	
TC-O4a		Ground-Level Gardens	Fully	Partially	No	N/A	
TC-O4b		Podium Gardens	Fully	Partially	No	N/A	

TC-O4c		Roof Gardens and Terraces	Fully	Partially	No	N/A	
TC-O5	70	Landscape Character	Fully	Partially	No	N/A	
TC-O5a		Hard Landscape	Fully	Partially	No	N/A	
TC-O5b		Soft Landscape	Fully	Partially	No	N/A	
TC-O5c		Street Furniture	Fully	Partially	No	N/A	
TC-O5d		Street Trees	Fully	Partially	No	N/A	
TC-O5e		Surface Water Drainage Features	Fully	Partially	No	N/A	
HOMES AND PRACTICALITIES							
TC-H1	72	Space Standards	Fully	Partially	No	N/A	
TC-H2	73	Mix of Homes	Fully	Partially	No	N/A	
TC-H3	73	Dwelling Aspect	Fully	Partially	No	N/A	
TC-H4	74	Residential Entrances and Circulation	Fully	Partially	No	N/A	
TC-H4a		Shared Entrances	Fully	Partially	No	N/A	
TC-H4b		Private Entrances	Fully	Partially	No	N/A	
TC-H5	75	Private Amenity Spaces	Fully	Partially	No	N/A	
TC-H5a		Balconies	Fully	Partially	No	N/A	
TC-H5b		Private Garden Space	Fully	Partially	No	N/A	



TC-H6	76	Vehicle and Cycle Parking	Fully	Partially	No	N/A	
TC-H6a		Visitor Cycle Parking	Fully	Partially	No	N/A	
TC-H6b		Residents' Cycle Parknig	Fully	Partially	No	N/A	
TC-H6c		Underground Parking	Fully	Partially	No	N/A	
TC-H6d		Podium Parking	Fully	Partially	No	N/A	
TC-H6e		Integrated Parking	Fully	Partially	No	N/A	
TC-H6f		Surface or On-Street Parking	Fully	Partially	No	N/A	
DETAIL AND RICHNESS							
TC-D1	78	Townscape	Fully	Partially	No	N/A	
TC-D2	79	Distinctive Buildings	Fully	Partially	No	N/A	
TC-D2a		Marker Buildings	Fully	Partially	No	N/A	
TC-D2b		Landmark Buildings	Fully	Partially	No	N/A	
TC-D3	80	Design of Elevations	Fully	Partially	No	N/A	
TC-D3a		Bases, Middles and Tops	Fully	Partially	No	N/A	
TC-D3b		Proportions	Fully	Partially	No	N/A	
TC-D3c		Building Tops and Roofs	Fully	Partially	No	N/A	
TC-D3d		Balconies	Fully	Partially	No	N/A	

TC-D3e		Corners	Fully	Partially	No	N/A	
TC-D3f		Windows and Fenestration	Fully	Partially	No	N/A	
CLIMATE CHANGE AND SUSTAINABILITY							
TC-C1	84	Mitigation: Reducing Energy Use	Fully	Partially	No	N/A	
TC-C2	85	Mitigation: Reducing Embodied Carbon	Yes	Partially	No	N/A	
TC-C3	86	Adaptation: Preparing for a Changing Climate	Yes	Partially	No	N/A	

Areas of Change

All Town Centre Neighbourhoods are within defined Areas of Change. Proposals must comply with the spatial coding requirements for the detailed Area Type that they fall within.

What Area of Change Area Type is your application within? (tick multiple if across boundaries)

STAINES-UPON-THAMES TOWN CENTRE

AOC-ST3 Memorial Gardens p128

AOC-ST4 Station Path p132

AOC-ST5 London Road p136

AOC-ST6 Two Rivers p140

AOC-ST7 Elmsleigh p144

AOC-ST8 Railway Edges p148

Historic Core is part of the High Streets area type. Please complete the High Streets checklist.

Staines Village is part of the Villages area type. Please set out the detailed Design Process in your Design & Access Statement.

SUNBURY CROSS

AOC-SC3 Station Area p158

AOC-SC4 Hanworth Road p159

AOC-SC5 Central Area p160

The Parade is part of the High Streets area type. Please complete the High Streets checklist.

Staines Road West is part of the High Streets area type. Please complete the relevant Suburban checklist.

DOES YOUR PROPOSAL COMPLY?			
Does your proposal comply with the specific Area Type Design Requirements set out under the below categories, including spatial requirements on Area Type Coding Plan?			
The Street & Ground Floor			
Fully	Partially	No	N/A
Scale & Massing			
Fully	Partially	No	N/A
Open Spaces			
Fully	Partially	No	N/A
Homes & Practicalities			
Fully	Partially	No	N/A
Detail & Richness			
Fully	Partially	No	N/A
If No (or partially no), please provide a reference to where in your planning application you have provided an evidenced justification			
If N/A please state why.			



# Inner Suburban

## Design Code Checklist: New Homes Or Apartments on Existing Streets

Development of new homes or apartments on existing streets proposed within Inner Suburban Area Types **must** complete this checklist to self-assess compliance with the Spelthorne Design Code. Submit the completed checklist with your planning application.

							JUSTIFICATION
REF	PG.	REQUIREMENT	DOES YOUR PROPOSAL COMPLY?				If <b>Partially</b> or <b>No</b> , provide a reference to where in your planning application you have provided an evidenced justification  If <b>N/A</b> please state why.
DEVELOPMENT TYPE: NEW HOMES OR APARTMENTS ON EXISTING STREETS							
IS-A1	88	Layout Principles	Fully	Partially	No	N/A	
IS-A2	89	Built Form Parameters	Fully	Partially	No	N/A	
IS-A3	89	Roof Form	Fully	Partially	No	N/A	
IS-A4	89	Front Boundary Treatment	Fully	Partially	No	N/A	
IS-A5	90	Daylight, Privacy and Overlooking	Fully	Partially	No	N/A	
IS-A6	90	Access, Cycle and Vehicle Parking	Fully	Partially	No	N/A	
IS-A7	90	Apartment Development	Fully	Partially	No	N/A	
IS-A8	91	Detail, Richness and Materiality	Fully	Partially	No	N/A	

# Inner Suburban

## Design Code Checklist: Residential Extensions

Development of residential extensions proposed within Inner Suburban Area Types **must** complete this checklist to self-assess compliance with the Spelthorne Design Code. Submit the completed checklist with your planning application.

							JUSTIFICATION If <b>Partially</b> or <b>No</b> , provide a reference to where in your planning application you have provided an evidenced justification If <b>N/A</b> please state why.
REF	PG.	REQUIREMENT	DOES YOUR PROPOSAL COMPLY?				
DEVELOPMENT TYPE: RESIDENTIAL EXTENSIONS							
IS-X1	92	Context & Character	Fully	Partially	No	N/A	
IS-X2	92	Privacy & Outlook	Fully	Partially	No	N/A	
IS-X3	93	Daylight	Fully	Partially	No	N/A	
IS-X4	93	Side Extensions	Fully	Partially	No	N/A	
IS-X5	93	Dormers	Fully	Partially	No	N/A	

# Suburban

## Design Code Checklist: New Residential Neighbourhoods

Development of new residential neighbourhoods proposed within Suburban Area Types **must** complete this checklist to self-assess compliance with the Spelthorne Design Code. Submit the completed checklist with your planning application.

REF	PG.	REQUIREMENT	DOES YOUR PROPOSAL COMPLY?				JUSTIFICATION If <b>Partially</b> or <b>No</b> , provide a reference to where in your planning application you have provided an evidenced justification If <b>N/A</b> please state why.
DEVELOPMENT TYPE: NEW RESIDENTIAL NEIGHBOURHOODS							
S-U1	96	Ensuring Distinctiveness	Fully	Partially	No	N/A	
S-U2	96	Edges	Fully	Partially	No	N/A	
S-U2a		Trees, Woodland and Hedgerows	Fully	Partially	No	N/A	
S-U2b		Open Spaces	Fully	Partially	No	N/A	
S-U2c		Watercourses and Water Bodies	Fully	Partially	No	N/A	
S-U2d		Streets and Roads	Fully	Partially	No	N/A	
S-U2e		Dual Carriageways	Fully	Partially	No	N/A	
S-U2f		Railways	Fully	Partially	No	N/A	
S-U2g		Residential	Fully	Partially	No	N/A	
S-U2h		Local Facilities	Fully	Partially	No	N/A	
S-U2i		Industry and Commercial Uses	Fully	Partially	No	N/A	



<b>S-U3</b>	100	<b>Movement: Streets</b>	Fully	Partially	No	N/A	
<b>S-U3a</b>		Street Layout Approach	Fully	Partially	No	N/A	
<b>S-U3b</b>		Main Streets	Fully	Partially	No	N/A	
<b>S-U3c</b>		Secondary Streets	Fully	Partially	No	N/A	
<b>S-U3d</b>		Local or Residential Streets	Fully	Partially	No	N/A	
<b>S-U3e</b>		Mews or Edge Lanes	Fully	Partially	No	N/A	
<b>S-U4</b>	102	<b>Movement: Car Parking</b>	Fully	Partially	No	N/A	
<b>S-U4a</b>		On-Plot Parking	Fully	Partially	No	N/A	
<b>S-U4b</b>		On-Street and Shared Parking	Fully	Partially	No	N/A	
<b>S-U5</b>	104	<b>Open Spaces</b>	Fully	Partially	No	N/A	
<b>S-U5a</b>		Open Spaces Amongst Homes	Fully	Partially	No	N/A	
<b>S-U5b</b>		Open Spaces on Edge of Built-up Area	Fully	Partially	No	N/A	
<b>S-U6</b>	106	<b>Landscape Character</b>	Fully	Partially	No	N/A	
<b>S-U6a</b>		Hard Landscape	Fully	Partially	No	N/A	
<b>S-U6b</b>		Soft Landscape	Fully	Partially	No	N/A	
<b>S-U6c</b>		Street Trees	Fully	Partially	No	N/A	
<b>S-U6d</b>		Surface Water Drainage Features	Fully	Partially	No	N/A	

# Suburban

## Design Code Checklist: New Homes Or Apartments on Existing Streets

Development of new homes or apartments on existing streets proposed within Suburban Area Types **must** complete this checklist to self-assess compliance with the Spelthorne Design Code. Submit the completed checklist with your planning application.

							JUSTIFICATION If <b>Partially</b> or <b>No</b> , provide a reference to where in your planning application you have provided an evidenced justification If <b>N/A</b> please state why.
REF	PG.	REQUIREMENT	DOES YOUR PROPOSAL COMPLY?				
DEVELOPMENT TYPE: NEW HOMES OR APARTMENTS ON EXISTING STREETS							
S-A1	108	Layout Principles	Fully	Partially	No	N/A	
S-A2	109	Built Form Parameters	Fully	Partially	No	N/A	
S-A3	109	Roof Form	Fully	Partially	No	N/A	
S-A4	109	Front Boundary Treatment	Fully	Partially	No	N/A	
S-A5	110	Daylight, Privacy and Overlooking	Fully	Partially	No	N/A	
S-A6	110	Access, Cycle and Vehicle Parking	Fully	Partially	No	N/A	
S-A7	110	Apartment Development	Fully	Partially	No	N/A	
S-A8	111	Detail, Richness and Materiality	Fully	Partially	No	N/A	

# Suburban

## Design Code Checklist: Residential Extensions

Development of residential extensions proposed within Suburban Area Types **must** complete this checklist to self-assess compliance with the Spelthorne Design Code. Submit the completed checklist with your planning application.

REF	PG.	REQUIREMENT	DOES YOUR PROPOSAL COMPLY?				JUSTIFICATION If <b>Partially</b> or <b>No</b> , provide a reference to where in your planning application you have provided an evidenced justification If <b>N/A</b> please state why.
DEVELOPMENT TYPE: RESIDENTIAL EXTENSIONS							
S-X1	112	Context & Character	Fully	Partially	No	N/A	
S-X2	112	Privacy & Outlook	Fully	Partially	No	N/A	
S-X3	113	Daylight	Fully	Partially	No	N/A	
S-X4	113	Side Extensions	Fully	Partially	No	N/A	
S-X5	113	Dormers	Fully	Partially	No	N/A	





Prepared for Spelthorne Borough Council by



Fathom Architects



## Appendix 2- Statutory Consultation Strategy

### Overview

Proposed statutory consultation dates: 23<sup>rd</sup> June 2025- 3rd August 2025  
(23.06.25- 03.08.25)

Duration: 6 weeks

Consultation platform: Commonplace

Document locations: Available to view online on the Design Code Commonplace and the Council website will also signpost that they will be available for the public to view in public libraries and at the civic centre during office hours.

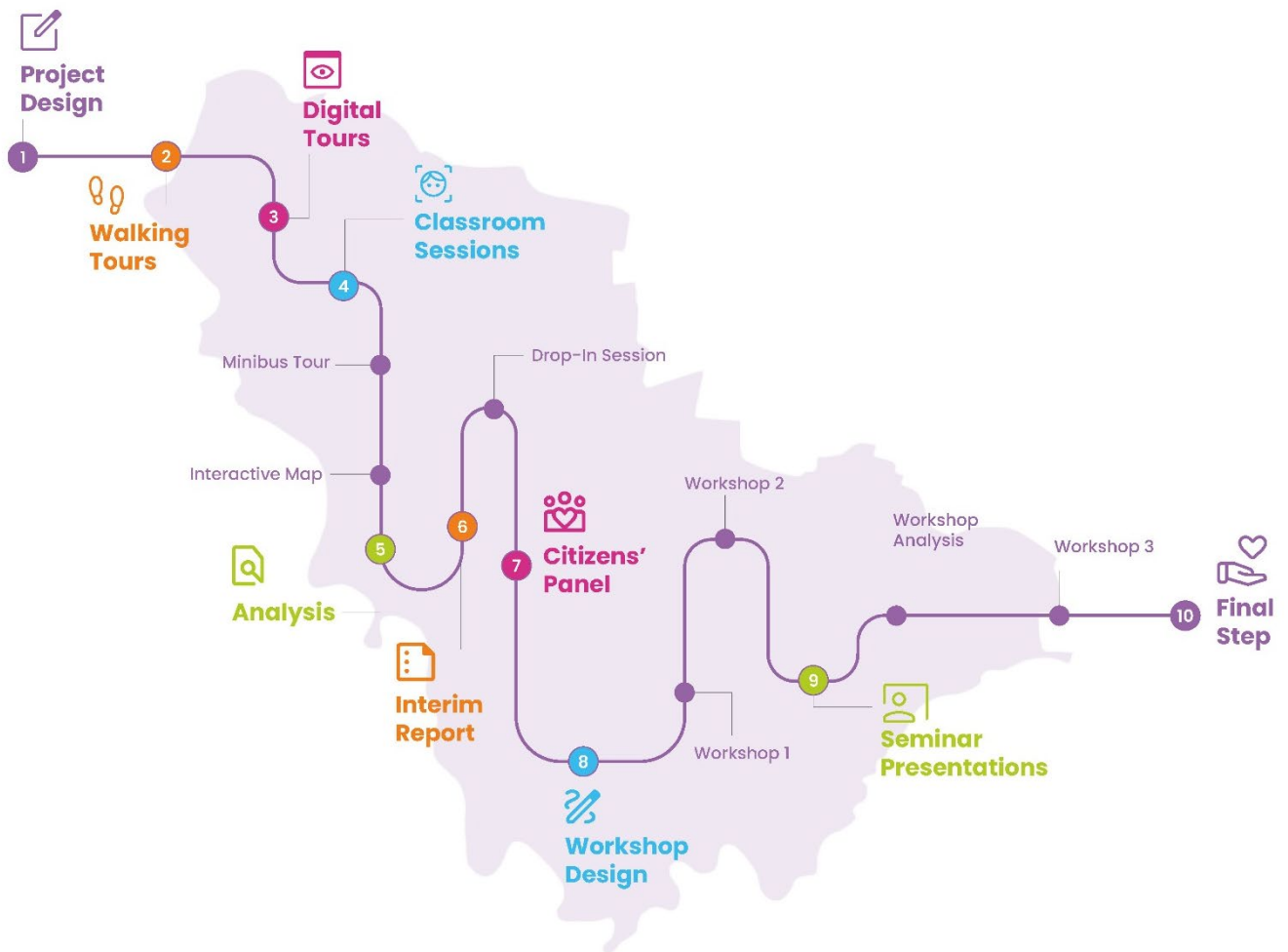
### Promotion of the statutory consultation will take the following forms:

- **Promotion on SBC website**
  - Homepage
  - Design Code page
  - News items/ e-news
  - Press release
    - Also to be sent on to the BBC
- **Promotion on Commonplace**
  - Regular news stories at least once a week before and during consultation
- **Promotion on SBC social media streams**
  - Social Media Posts (Instagram, Facebook, X, LinkedIn etc.)
    - Regular posts at least once a week
    - Countdown posts published to highlight consultation closing soon
    - Pinned posts to top of social media pages
  - Social Media Stories (Instagram, Facebook)
    - Regular stories along with posts at least once a week
  - Social Media Advert
    - Ad to run for duration of whole consultation
    - Targeted at whole borough area
    - More concise strapline and indication to go to Commonplace
    - Link straight to consultation survey
  - YouTube video
    - A 'how-to' video so people can visually understand how they can respond
- **Promotions at key location around Borough**
  - Posters
    - Borough-wide key locations includes noticeboards across the Borough, council offices, libraries, community centres and others
  - Digital Screens
    - In Elmsleigh and Staines High Street



- Summer Bulletin
  - To go out w/c 30.06.25 during proposed consultation dates and delivered across the Borough
- **Emails to Councillors, local Residents Associations/community groups and neighbouring authorities**
  - Emails to Residents' Associations
    - Regular emails before and during consultation with a call to action and salient points covered
  - Emails to Councillors
    - Regular emails before and during with a call to action and salient points covered
  - Emails to schools and youth groups
  - Emails to other community groups (housing associations, Citizens' Panel etc.)
  - Emails to statutory consultees
- **Promotion to staff**
  - Spelnet news
  - Presentation at all staff meeting
- **Further promotions to Members**
  - Presentation at All Member meeting
  - Member Briefing Note
- **Other promotions**
  - Email footers on Council emails
  - Spelthorne Business Forum news article

## Appendix 3- Draft\_The Engagement Proccess (Stage 1 to Stage 2)



*Infographic – The Spelthorne Engagement Process*

## Context

Design codes are a critical mechanism for guiding urban development and ensuring the visual and functional integrity of places where people live, work and play. The preparation of design codes requires the involvement of local people to ensure that the resultant design code is what is referred to as “provably popular”.

The concept of "provably popular" design codes is embedded in several key national policies and guidance documents and the project team in Spelthorne were mindful of these documents when designing the engagement process. The main sections that address this requirement are:

- **National Model Design Code (NMDC):** The NMDC emphasises the importance of community engagement in the creation of design codes. It outlines that design codes should be developed in collaboration with local communities to ensure they reflect local preferences and needs. The NMDC states that design codes must be "provably popular," meaning they should have demonstrable support from the community. This is to be achieved through extensive public consultation and engagement processes, ensuring that the design codes are not only technically sound but also resonate with the local population.
- **National Planning Policy Framework (NPPF):** The NPPF highlights the significance of involving local communities in the planning process. It encourages local authorities to produce design codes that are informed by public opinion and have broad community support. The NPPF underscores the need for design codes to be "provably popular" by demonstrating that they have been shaped by the views and preferences of local residents. This approach aims to create a sense of ownership and acceptance among the community, leading to more successful and sustainable developments.
- **Department for Levelling Up, Housing and Communities (DLUHC):** The DLUHC has commissioned research to define "provably popular" design and provide guidance on how local authorities can measure and demonstrate popular design in their areas. This research is part of the government's broader efforts to improve design quality across England and ensure that new developments are well-received by the community. The findings from this research will inform future policy and guidance, helping local authorities create design codes that are both effective and widely supported.

By incorporating these principles into national policy and guidance, the UK government aims to ensure that design codes are not only technically robust but also aligned with the preferences and aspirations of local communities. This approach fosters greater community involvement, enhances the quality of new developments, and promotes a more inclusive and democratic planning process. The project team has endorsed this approach and designed a locally responsive version for Spelthorne.

To achieve a “provably popular” outcome for Spelthorne, it has been essential to integrate the views and inputs of local people into the process of code preparation. Local community involvement has been vital in capturing the unique preferences, needs, and aspirations of residents of the borough, which in turn ensures that the resultant Spelthorne Design Code resonate with the majority of the population. By engaging local stakeholders through walking tours, conversations, and participatory workshops, the project team has gathered valuable insights and feedback, enabling the creation of a code that will be widely accepted and



supported. This inclusive approach not only enhances the legitimacy and acceptance of the Spelthorne Design Code but also advances a sense of ownership and pride among residents, contributing to the overall success and sustainability of future development projects.



## **1. Project Design**

The success of any project lies in thorough planning and execution. For the preparation of the Spelthorne Design Code, the project team embarked on an extensive mapping of a series of interlinked and coordinated tasks. This systematic approach was essential to gather the best possible evidence to support the design code's development. This preparation phase took place in May and early June 2024.

Our strategic task mapping began with a thorough analysis of the project's objectives, aligning them with the community's unique characteristics and needs. We ensured that each task was designed to contribute valuable insights and data, ultimately supporting a cohesive and comprehensive design code.

Once the project design was finalised, the project team sought approval from the client (Spelthorne Borough Council) to ensure all stakeholders were on the same page. This approval marked the transition from planning to the logistics, planning, and delivery phase. The project team took charge of coordinating the various tasks, ensuring that each activity was executed efficiently and effectively. This included scheduling community engagement sessions, arranging resources, and managing timelines.

Throughout the project's duration, our team maintained close communication with the Council, providing regular updates and addressing any emerging challenges. This collaborative and structured approach laid a solid foundation for creating Design Codes that genuinely reflect the aspirations and needs of the local community.



## 2. Walking Tours

Walking Tours were a cornerstone of the community engagement strategy, designed to gather firsthand insights from local residents about the areas they cherished and those they felt needed improvement. These self-guided tours were conducted during both daytime and evening hours, each lasting approximately two hours. Three walks were held across each of the agreed days at 10am, 2pm and 6pm. Offering different timeslots across each day was designed to attract different demographic groups e.g. retired, working age etc. In total, there were 18 in-person walks arranged from late June through until mid-July 2024.

Local residents played a crucial role in shaping these tours, as they led members of the project team to locations that held significant meaning to them, both positive and negative.

The essence of these tours lay in their informal and conversational nature. As we walked through the neighbourhoods, residents freely shared their thoughts, experiences, and concerns. This open dialogue allowed us to gain a deep understanding of the community's perspectives on various urban elements, such as public spaces, infrastructure, and aesthetics. The routes for these tours were not predetermined; instead, they were decided by the local residents on the day of the tour. Our team followed their lead, taking notes and listening attentively to their stories and feedback.

To ensure that residents could speak candidly, local politicians were politely barred from participating in these tours. This decision was made to create a safe and open environment for genuine conversations. To balance this exclusion, we introduced a minibus tour for local politicians (see later) where they could learn about the issues highlighted by residents during the Walking Tours. Through this approach, we ensured that the voices of local people were heard and respected, forming the basis for a design code that truly reflects the community's desires and needs.

The following quotes are taken from the project team's written notes and provide an insight into the key messages received during this research phase:

- *"Staines, Shepperton, Ashford, Stanwell, and Sunbury all have different personalities, and we have to keep that"*
- *"It's been fascinating to explore the different neighbourhoods in this way"*
- *"This is a welcome process, fantastic to be listened to like this"*
- *"Ashford has a village feel in certain areas, an arts and crafts approach to its design. We want to keep this theme today, if we can"*
- *"We want a design code that doesn't compromise the needs of residents or businesses"*
- *"We need to design for now and for the future"*
- *"This process has left us optimistic about the architecture that we have, and have been able to build in the past and this design code will help us create good places in future too"*
- *"I'm really proud of our area, I want to show it off more"*

- *"I love the trees because it means you can hear bird song" (youth group)*
- *"I love walking along the moor every day, this is the best part about this borough"*
- *"We can easily get to London and enjoy lots of green space at the same time"*
- *"Unfortunately we ignore our river"*
- *"I love how Debenhams building curves round, like the flow of the river"*
- *"Let's rejuvenate, not redo!"*
- *"Two worlds of river and land are not connecting"*
- *"The river front is the most beautiful part of what we have left"*
- *"It's so green here, let's keep it that way"*

Comments from those that took part in the Walking Tours



### 3. Digital Tours

In addition to the conventional Walking Tours, we recognised the need to engage with community members who might be unable to participate in person due to various constraints, such as limited mobility or scheduling conflicts. To address this, we introduced Digital Tours, leveraging modern technology to extend our reach and inclusivity. These Digital Tours were essentially virtual versions of the physical Walking Tours, conducted through the Zoom platform and using Google Maps.

Local residents were invited to sign up for these Digital Tours, which typically took place in the evenings and lasted approximately 90 minutes. These events all took place in July 2024. The format allowed participants to explore their neighbourhoods virtually, sharing their thoughts and opinions just as they would during an in-person tour. The digital format offered several advantages, including the ability to cover a much larger area within the given time frame, as we could quickly navigate across different locations on the map.

Participants in the Digital Tours provided valuable insights into the areas they liked and disliked, discussing various urban elements and their impact on the community. The flexibility of the digital format made it accessible to a broader audience, ensuring that even those with limited mobility or tight schedules could contribute their perspectives. By combining technology with community engagement, we were able to gather comprehensive input from a diverse range of residents, enriching the evidence base for the design code.

For both digital and in-person walks, 68 people signed up in advance via the Eventbrite system and a total of 58 people took part across all events,





#### 4. Classroom Sessions

In July 2024, the project team engaged with Ashford Youth Club, Sunbury Manor School and Matthew Arnold School. In total, around 80+ young people between the ages of 12 and 17 have been involved in the project, sharing with us their ideas and aspirations. The project team worked closely with students aged 12-16 in school classrooms, engaging them through thoughtfully designed workshops sessions, including bespoke worksheets. These worksheets consisted of a series of specific questions, which served as prompts to delve into their memories, recent experiences, and aspirations for the future. The goal was to create an environment where students could freely express themselves, reflecting on their personal journeys and envisioning their future goals. Additionally, to cater to different modes of expression, we invited students to draw their ideas, providing a visual representation of their thoughts. This approach allowed us to gather both written and drawn outputs, capturing a holistic view of their perspectives. Through these sessions, students were not only able to articulate their thoughts but also to see their ideas take form, fostering a sense of ownership and engagement in the process.

Key outcomes were:

- Leisure, food and drink activities make an area attractive, more so than a retail offer
- They often seek out quieter, more peaceful areas, away from busy high streets. This was especially true of teenage girls that like to find a “hangout” of their own
- As a demographic that cannot drive, access to safe, convenient walking and cycling routes gives them independence and autonomy over the way they live and move across the borough
- They often use different neighbourhoods in different ways e.g. visit Staines on a Saturday for food and browse the shops but go to Ashford on the bus during the week for five-a-side football and a haircut
- Drawings from young people included reference to having enough space to play and decoration, colour and design details on walls and shopfronts

#### *Minibus Tour*

To bridge the gap between local residents and policymakers, the project team organised a minibus tour for local politicians around the borough. This tour took place on Friday 19<sup>th</sup> July 2024 and included stops at locations that had been highlighted by local people during our Walking Tours and through our Digital Tours. The objective was to provide politicians with a firsthand experience of the areas and issues that were important to the community. Along the way, insights and ideas that students from the classroom sessions had contributed were shared. This direct interaction helped to convey the community's concerns and aspirations in a tangible manner, facilitating a more informed and empathetic dialogue between policymakers and residents. The minibus tour not only raised awareness but also fostered a collaborative spirit, encouraging politicians to take proactive steps towards addressing local issues.

### *Interactive Map*

To further engage the community, an interactive map was established using the Commonplace Platform. This map allowed local people to place a pin on a map to identify buildings, streets, or spaces that they liked or disliked. Green pins indicated places they liked, while red pins marked areas they disliked. Accompanying each pin, individuals could provide explanations for their preferences, offering valuable insights into the factors influencing their opinions. This interactive map served as a powerful tool for visualising community sentiments, highlighting areas of concern, and identifying potential opportunities for improvement. By enabling residents to share their thoughts in this way, the project team were able to gather a wealth of information that reflected the diverse perspectives within the community.

By the time the interactive map was closed to contributions in December 2024, 346 comments, and a further 278 reactions to these comments.



## **5. Analysis**

We now began a process of analysing and interpreting the results from all the events and activities conducted up until this point. This comprehensive analysis allowed a synthesis of the data collected from the Walking Tours, the Digital Tours, the classroom sessions, the minibus tour, and interactive map, transforming it into actionable insights. By identifying common themes, patterns, and unique viewpoints, we gained a deeper understanding of the community's needs, aspirations, and challenges. This analysis not only informed the recommendations for future development of the code but also provided a robust foundation for ongoing community engagement and development efforts.



## **6. Interim Report**

Based on the data gathered up until this report from the various techniques, and its analysis, an Interim Report was issued in early August 2024. This captured the main themes and emerging concepts. This report was a crucial milestone in the project, serving as a foundation for the development of the design code. The Interim Report's primary aim was to ensure that the public opinion that was likely to inform the design code was communicated to the project board at the Council. The report highlighted the common themes, and notable insights drawn from the diverse range of inputs collected during the initial phase of the project. The data included examples of the students' written and drawn outputs from the classroom sessions, and

feedback from the Walking Tours and the interactive map. Each of these sources provided a unique perspective on the community's needs, preferences, and aspirations. By collating and analysing this information, we were able to identify the main priorities and concerns of the residents. There were 5 x key areas of consensus across all age groups:

1. Details, decoration and attention to the eye-level experience makes a big difference with the public. Plain buildings are seen as boring or unattractive. We have found it to be less of a “modern vs. old” debate, more a “visual interest vs. plain” debate
2. Everyone loves nature, and wants access to green space and areas of tranquillity
3. Colour matters, and younger people in particular want to see more of this
4. The uniqueness of places is important, as represented in the buildings e.g. one house being different to its neighbours, but also between neighbourhoods e.g. Ashford is different to Staines. The design code should encourage and enhance this positive differentiation
5. The public are keen to see the reuse and refurbishment of empty properties. They are annoyed when they see a good building left empty and neglected

The Spelthorne Design Code was always going to be covering a series of “core topics” that will be common to all borough-wide design codes across the country. However, to ensure a responsiveness to local public opinion as expressed in Spelthorne, the following 5 x priority topics are deserving of specific attention to address local on-the-ground issues, as identified through the research:

#### Neighbourhood Vision

- To maintain and enhance the distinctiveness between different parts of the borough
- Code to provide a contextual overview of each area to set the scene

#### Green Space

- A popular public issue
- Code to encourage integration of green space in all developments
- To provide examples at all scales e.g. micro spaces, such as window boxes and roof gardens through to neighbourhood greens

#### Connections & Layout

- Ensure safe connections within the area and to nearby areas
- Design for age and gender differences
- Layouts that offer choice are preferred



### Reuse & Refurb

- More a policy issue than a code issue but vital for public support
- Code to provide good example of refurb and reuse and to encourage conversions before new builds

### Decoration & Detail

- Code to identify architectural elements that lend themselves best to such decorative treatment
- To avoid superficial treatment but to integrate into the whole

Each of these 5 x priority topics can be tracked back to the first phase engagement results. The Interim Report's findings have been instrumental in shaping the design code by ensuring it was grounded in real community sentiment. The outcomes of the design code needed to be provably popular, emphasising the importance of public buy-in and support for the project.

### *Drop-In Session*

A drop-in session was held in early November 2024, to allow residents of Staines-upon-Thames (and any other residents of the borough) an opportunity to comment on the future of the town. While the design code will be a borough-wide code, addressing issues across all settlements, this event was arranged because Staines is the principal town in the borough, and the one most likely to undergo the greatest change.

Held in the indoor Elmsleigh Shopping centre in Staines-upon-Thames, this location was chosen due to its high foot traffic and all-weather accessibility, ensuring maximum engagement from the community. The afternoon session was designed to be informal and open, allowing residents to drop by at their convenience, making it easy for a diverse range of individuals to participate. A series of informational displays and interactive stations were set up, including a large-scale map of the town, as well early ideas from the emerging code. These displays included historic maps and diagrams offering a visual representation of areas of character.

Residents were encouraged to provide feedback through various means, including written comments and one-on-one discussions with project team members. To facilitate meaningful conversations, several facilitators were present (both from the project team and the council) equipped with knowledge about the project and ready to answer questions, address concerns, and gather suggestions. This face-to-face interaction was invaluable in understanding the community's sentiments and priorities.

Overall, the Drop-In Session fostered a sense of community involvement and ownership, ensuring that the public's voice was central to the planning process.



## 7. Citizens' Panel

In October and November 2024, a Citizens' Panel was established, consisting of fifty residents who were representative of the community's diverse demographics, including age, gender, and geographic location within the borough. The selection process was inclusive, inviting all participants from previous events to apply, as well as issuing a public advertisement to reach a broader audience. This approach ensured that the panel was truly reflective of the community it aimed to represent. Once finalised, the panel was invited to participate in three interactive workshops designed to gather deeper insights and foster active engagement. The first two of these workshops were held in November 2024.

These workshops provided a platform for residents to get involved in the detail of design coding, share their experiences, and contribute to the ongoing development of the project. The diverse composition of the panel ensured that a wide range of perspectives were considered, enriching the discussions and outcomes. The commitment by the Council to establishing a panel underscored its promise to a participatory and democratic process. The insights gained from the Citizens' Panel were invaluable in informing the next stages of the project, ensuring that the community's voice remained central to the decision-making process. This initiative not only empowered residents but also strengthened the relationship between the community and the Council.



## 8. Workshop Design

Using the findings from the Interim Report, the project team designed two half-day workshops that aimed to address the fundamental issues identified during the early research phase. Each workshop was structured to include three tasks, carefully crafted to respond to the key themes and concerns highlighted by the community. The tasks were diverse in nature, ensuring that various aspects of the project were covered comprehensively. They included activities such as group discussions, and hands-on exercises, all designed to encourage active participation and collaboration among the panel members.

The interactive format of the workshops allowed participants to engage deeply with the topics, share their ideas, and co-create solutions. By directly responding to the issues raised in the initial research phase, the workshops ensured that the community's input was not only heard but actively incorporated into the project.

This iterative process of feedback and refinement was crucial in developing a design code that was both relevant and responsive to the community's needs. The workshops also provided an

opportunity to test and validate the emerging concepts, gathering real-time feedback from the panel members. This dynamic approach ensured that the project remained adaptive and aligned with the evolving priorities of the community. The outcomes of the workshops were documented and integrated into the final stages of the project, ensuring a cohesive and well-informed approach to the design code development.

#### *Workshop One – Saturday 16<sup>th</sup> November 2024*

1. The first task in this workshop was to develop a written vision statement for the entire borough. Participants were encouraged to think broadly about the future of their community, considering aspects such as quality of life, infrastructure, and cultural identity. This vision statement would serve as a guiding framework for future development and planning, in respect of the code.
2. The second task was an interactive activity inspired by the results from the interactive map. Participants were presented with a series of 50 photographs, divided into two categories: 25 'red' photographs representing areas or features that needed change, and 25 'green' photographs showcasing elements that should be conserved or enhanced. This visual exercise helped participants identify specific design features and prioritise actions.
3. Finally, the workshop focused on creating written vision statements for each of the five neighbourhoods that comprise the borough. These neighbourhood-specific statements allowed for a more localised approach, addressing unique characteristics and needs while contributing to the overall vision for the borough.

Throughout the workshop, participants engaged in group discussions, shared personal experiences, and collaborated on crafting the vision statements. This inclusive and participatory process ensured that diverse perspectives were considered.

#### *Workshop Two – Saturday 23<sup>rd</sup> November 2024*

The following Saturday afternoon, the panel reconvened for Workshop Two, which built upon the foundation laid in the first session. This workshop delved into three critical topics: gender-inclusive design, neighbourhood expansion, and high-quality tall building design.

4. The first task at the second event explored gender-inclusive design, aiming to create spaces that were welcoming and accessible to everyone, regardless of gender or age. Participants discussed strategies to ensure public spaces were safe, comfortable, and accommodating for all community members.
5. The next task focused on expanding existing neighbourhoods into currently undeveloped areas. This exercise required participants to consider factors such as edge conditions, the local context while considering sustainable and cohesive expansion plans.



6. The final task tackled the design of high-quality tall buildings, encouraging participants to think creatively about aesthetics, functionality, and integration with the surrounding environment.

Throughout this second workshop, participants engaged in lively discussions and collaborative problem-solving activities. The interactive nature of the workshop fostered a sense of camaraderie and shared purpose, as participants worked together to address complex urban planning challenges.

→ *“We have worked with photographs, we have worked with discussions, we’ve done written stuff. I think it’s good because it just sort of creates a focus for the way that different brains work as well actually. We are not just sitting and talking or sitting and watching, it’s been a good mix, I think.”*

Diane Ludlow, Citizens’ Panel Member

→ *“This, from what I hear, is very innovative. Only a handful of councils in the country are doing this kind of thing where they’re engaging with residents to understand what design should look like. I think the country would benefit from more discussion like this. Spelthorne is pioneering something really good here.”*

Alex Balkan, Citizens’ Panel Member



## 9. Seminar Presentations

During both half-day events, the professional team delivered several short slideshow presentations on key topics related to urban planning and design. These presentations covered a range of subjects, including street design, green spaces, sustainability, and inclusive design.

The purpose of these presentations was to provide participants with background information and context, helping them better understand the issues at hand and informing their contributions to the workshop tasks. The presentations were strategically scheduled throughout the afternoon to structure and break up the agenda, ensuring that participants remained engaged and focused.

Each presentation was followed by a brief Q&A session, allowing participants to ask questions, seek clarification, and share their perspectives. This interactive format encouraged active learning and dialogue, encouraging a deeper understanding of the topics discussed. The professional team's expertise and insights were instrumental in guiding the workshop discussions and ensuring that participants had the knowledge and tools needed to contribute effectively. By blending informative presentations with hands-on activities, the workshops created a dynamic and enriching experience for all involved.

### *Workshop Analysis*

Following the completion of both half-day workshops, the results were processed, analysed, and interpreted to extract valuable insights and inform the next steps of the project. This comprehensive analysis involved synthesising the data collected from various activities, including vision statements, photograph evaluations, and design exercises.

By identifying common themes, patterns, and unique perspectives, the analysis provided a holistic understanding of the community's priorities, concerns, and aspirations. The findings from the analysis were used to refine and validate the initial code concepts, ensuring that they aligned with the community's needs and preferences.

The analysis also highlighted areas of consensus and divergence, offering a nuanced view of the diverse viewpoints within the community. This in-depth understanding was crucial in shaping the final design code and ensuring that it was grounded in real community sentiment. The results of the analysis were shared with stakeholders, including local policymakers, educators, and community leaders, fostering transparency and collaboration.

This rigorous and participatory approach ensured that the project remained adaptive and responsive to the evolving priorities of the community.

### *Workshop Three – Saturday 1<sup>st</sup> March 2025*

The third workshop represented a crucial opportunity for the Citizens' Panel to test the draft design code to ensure it best responded to local opinion. As before, held on a Saturday afternoon, this workshop was planned to build on the previous two workshops, incorporating the insights and feedback gathered thus far. The main focus of this session was to critically examine the draft design code. Participants were provided with detailed copies of the draft code, along with explanatory notes and contextual information to guide their review.

The workshop was structured around a series of interactive tasks and discussions. Participants worked in small groups, each focusing on a specific aspect of the design code, such as public spaces, building aesthetics, sustainability, and accessibility. These groups were tasked with identifying strengths and weaknesses, proposing modifications, and prioritising key elements. This collaborative approach ensured that diverse perspectives were considered.

Throughout the session, facilitators and project team members circulated among the groups, providing support, answering questions, and recording feedback. This dynamic interaction helped to clarify misunderstandings, address concerns, and refine ideas. Additionally, there were plenary sessions where groups presented their findings and recommendations to the entire panel, encouraging cross-group dialogue and consensus-building.

The workshop concluded with a synthesis of the feedback, highlighting the key themes and actionable suggestions.



## **10. Spelthorne Design Code**

The results of the analysis directly informed the final Spelthorne Design Code, ensuring that it was rooted in the community's needs, preferences, and aspirations. The final code used the insights and recommendations derived from this engagement process creating a comprehensive and actionable document.

The final design code includes this community engagement appendix, detailing the participatory processes and methods used throughout the project. This appendix provided a transparent account of the community's involvement, highlighting the importance of inclusive and collaborative approaches in urban planning.

By capturing the collective vision and priorities of the community, the final report laid the groundwork for a sustainable and inclusive future for the borough.





## Citizens' Panel Workshop Results

### Task 1: Borough-Wide Vision

The Citizens' Panel explored multiple vision drafts for Spelthorne in 2035. The proposed statements emphasised themes of heritage, sustainability, and cohesive urban design, while addressing the borough's unique challenges. Feedback from the group shaped the following priorities:

#### Key Vision Highlights

- **Sustainable Urban Design:** Spelthorne aspires to blend modern infrastructure with heritage through sustainable, high-quality architecture that supports vibrant, interconnected communities.
- **Commitment to Green Spaces:** Maintaining and rejuvenating green spaces was a major theme. Participants highlighted the importance of integrating natural areas into urban environments for residents' well-being.
- **Social and Physical Connectivity:** Better public transport, accessible infrastructure, and enhanced pedestrian and cycling pathways were frequently mentioned to improve overall connectivity and accessibility.
- **Community-Centric Development:** Incorporating community hubs, affordable housing, and outdoor recreational spaces to meet the diverse needs of Spelthorne's residents.
- **Flood Resilience:** Mitigating flood risks through thoughtful planning was a clear priority.

#### Key Themes the Panel Wanted to Include

- **Heritage:** Celebrate and preserve the borough's historical identity while adapting it for modern use.
- **Green:** Protect green spaces, promote biodiversity, and ensure natural landscapes are accessible.
- **Connectivity:** Enhance transport links, particularly to major hubs like Heathrow, and improve pathways for pedestrians and cyclists.
- **Community:** Foster inclusivity by designing spaces that cater to all ages, cultures, and abilities.
- **Timeless Design:** Focus on cohesive, durable architectural styles that avoid dated aesthetics.

### Key Themes the Panel Opposed or Criticised

- **Mismatch:** Participants were critical of inconsistent architectural styles that clash rather than complement each other.
- **Concrete:** Avoid overly industrial or bland designs that lack character and greenery.
- **Overdevelopment:** The group expressed concern about overly dense developments, especially those that fail to include sufficient social and physical infrastructure.

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## **Task 2: Summary of Areas to Conserve and Change**

The Citizens' Panel identified specific areas, themes, and features that should be either preserved or improved to align with the vision for Spelthorne in 2035. Feedback focused on heritage, green spaces, community facilities, and the borough's riverside and urban character.

### Areas to Conserve

**Green Spaces:** Participants consistently emphasised the importance of preserving well-maintained green spaces for recreation and biodiversity.

- Staines Moor: Valued for its beauty and role as a Site of Special Scientific Interest (SSI)
- Riverside Parks: Iconic and essential for community wellbeing.
- Walled Garden in Sunbury: An award-winning example of successful land repurposing.

**Heritage and Landmarks:** Protect and enhance historic buildings and conservation areas, ensuring they remain visually attractive and accessible e.g. historic villages like Stanwell Moor and Sunbury's period houses.

**Riverside Character:** Maintain and improve the aesthetic and recreational use of riverside areas, including the River Thames and Colne. Encourage walking and cycling along the riverside while keeping architectural additions sympathetic to the environment.

**Community Spaces:** Retain local hangout spots and green areas used by families and young people. Plus calls for better maintenance to sustain their appeal.

### Key Words for Conservation

- Heritage
- Green Spaces
- Riverside
- Community Facilities

### Areas to Change

**Neglected and Poorly Maintained Spaces:** Participants flagged neglected buildings and infrastructure that detract from the borough's aesthetic e.g. derelict sites like car parks near Bridge Close (Staines) and abandoned buildings on Church Road (Ashford). Public spaces with limited upkeep, such as parts of the riverside and poorly maintained pathways.

Accessibility and Infrastructure: Improve pathways, lighting, and overall safety in key areas e.g. riverside pathways in Staines: Often unsafe or poorly lit.

- Linear Park in Sunbury: Needs better lighting and access.
- Shepperton High Street: Requires better crossings and improved pedestrian infrastructure.

**Modernising Outdated Architecture:** Participants criticised bland, mismatched, and concrete-heavy structures that fail to integrate with the borough's character e.g. brutalist developments, particularly in Sunbury and Roman Court in Staines was highlighted as a poorly executed example of greenery integration.

**Underutilised Spaces:** Suggestions to repurpose derelict or functional-only spaces into vibrant community hubs or recreational areas e.g. Shepperton Library: Proposed as a site for market stalls and public events and The Swan Sanctuary: Calls for improved public access.

**Green Space Integration:** Encourage better placement of greenery in urban developments to create inviting environments e.g. Riverside spaces: Emphasise biodiversity and community use.

#### Key Words for Change

- Neglect
- Lighting
- Accessibility
- Repurposing
- Mismatch

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### **Task 3: Neighbourhood Visions**

This task focused on the panel's vision for the future of each neighbourhood in Spelthorne, exploring key aspirations for development, connectivity, and community integration by 2035.

#### Staines

By 2035, Staines aims to be a vibrant, well-connected urban centre with improved riverside access, better design, and flood mitigation. Key priorities include balancing heritage, enhancing connectivity, and transforming the area into a modern, safe town with green spaces and a public riverfront.

*Key Words: Gentrification, Riverfront, Connectivity*

#### Sunbury

By 2035, Sunbury aims to maintain its low-rise character, with no developments exceeding eight floors and ensuring that higher-density projects meet design standards for safety. Key priorities include improving accessibility for all, particularly those with reduced mobility, and enhancing vehicle access. The community seeks a comfortable, well-designed environment with ample



common areas and parking spaces, reducing congestion and creating a welcoming, less pressured atmosphere. Improvements to pedestrian crossings and road access are also essential, with a preference for manageable, human-scale development.

*Key Words: Accessibility, Low-Rise, Parking, Mobility*

### Ashford

By 2035, Ashford envisions a safe, community-focused area with plenty of green spaces. Key features include a revitalised town square, local parks, cafes, restaurants, and minimal high-rise development. Priorities are supporting local shops, providing green spaces, and creating a hub for youth activities. Community feedback emphasises keeping Ashford family-friendly with a focus on youth and vibrant public spaces.

*Key Words: Community, Green Spaces, Youth Activities*

### Shepperton

By 2035, Shepperton aims to preserve its village charm while embracing thoughtful development. Key features include sympathetic developments (max 2 storeys), a new town square, better cycling infrastructure along the Thames, and a semi-pedestrianised High Street. Priorities focus on balancing growth with Shepperton's character, preserving the high street and green spaces, and improving access for cyclists and pedestrians. Community feedback highlights the desire to keep independent shops while supporting sustainable growth and transport.

*Key Words: High Street, Conservation, Cycling*

### Stanwell

No direct feedback was provided from Stanwell, but general comments highlight a need for stronger identity and cohesion. Key features include developing a clearer identity, cohesive planning, and better integration with the rest of Spelthorne. Priorities focus on addressing infrastructure gaps and creating more connected spaces to foster community. Community feedback expresses concerns about Stanwell being too sprawled, rundown, and disconnected from the rest of the borough.

*Key Words: Identity, Cohesion, Infrastructure*

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## Task 4: Safety and Accessibility & Mobility Across the Borough

**Most Pinch and Pain Points Identified:** Unsurprisingly, Staines had the highest number of issues, particularly around connectivity, lighting, and accessibility. The second place with the most issues, following Staines, was Stanwell.

### Commonalities Across Borough (in order of number of mentions):

1. **Poor Lighting:** The most frequently mentioned issue, affecting safety in residential areas, parks, pathways, and underpasses.
2. **Narrow/Uneven Pavements:** Widespread concerns about walkability and accessibility, particularly for vulnerable users like wheelchair or pram users.
3. **Anti-Social Behaviour (ASB):** Recurring issue in neglected or poorly maintained public spaces, including parks and car parks.
4. **Unsafe Crossings:** Significant safety risks highlighted, particularly near schools and high-traffic areas.
5. **Traffic and Parking Issues:** Speeding and poorly managed parking were raised frequently but slightly less than other issues.

### Commonalities per Area:

#### *Stanwell*

- **Pinch Points (Orange Stickers):** Narrow roads, uneven walkways (e.g., Clare Road, Oaks Road), poor lighting on streets like Hadrian Way and Cordella Road, and inadequate frequency of bus routes.
- **Pain Points (Red Stickers):** Anti-social behaviour (ASB) near derelict buildings and public spaces, drug dealing, intimidating youth gatherings, dark or unsafe streets (e.g., Riverside Road), and poor road conditions hindering parking or walking.

#### *Staines*

- **Pinch Points (Orange Stickers):** Lack of connectivity between areas like the bus station and Riverside, inadequate cycle paths, poor lighting (e.g., South Street, Coopers Lane), and obstructed pavements affecting accessibility for wheelchairs or buggies.
- **Pain Points (Red Stickers):** Unsafe car parks and walkways (e.g., Bridge Close, Staines Park), ASB (e.g., Thames Street), poorly lit areas, narrow and uneven pavements (e.g., Kingston Road, A308), and a lack of public toilet signage.

#### *Sunbury*

- **Pinch Points (Orange Stickers):** Poorly maintained roads (e.g., Green Street), lack of wheelchair-friendly park gates, and limited pedestrian and cycling infrastructure.
- **Pain Points (Red Stickers):** High-speed traffic on Thames Street and narrow pavements, drug-related activities in public car parks, poor lighting, dangerous pedestrian crossings, and conflicts between vehicles and pedestrians.

### *Ashford*

- **Pinch Points (Orange Stickers):** Poor road conditions (e.g., Knapp Road), insufficient lighting, and limited social hubs or community activities.
- **Pain Points (Red Stickers):** Dangerous crossings near schools (e.g., School Road), ASB near fast-food outlets, narrow pavements, and dimly lit or unsafe walkways like the Elephant Path.

### *Shepperton*

- **Pinch Points (Orange Stickers):** Narrow, uneven pavements (e.g., Govett Avenue), poor crossing design (e.g., Green Lane), and lack of traffic calming measures on busy roads.
- **Pain Points (Red Stickers):** Dangerous pedestrian areas due to fast-moving traffic (e.g., Russell Road, B375), poor lighting, unsafe crossings near schools, and ASB in poorly lit public spaces.

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## **Task 5: Edge Conditions**

In this task, the Citizen's Panel looked at how new developments will fit with their surroundings, focusing on making spaces safer, more accessible, visually appealing, and better for the environment, with attention to reducing noise, adding greenery, and improving connections for the community.

### Type 1 – Trees and Hedgerows

#### *Challenges*

- Promised tree planting often fails, e.g., Watersplash Farm and Charlton Village Incinerator, with dead saplings and poorly enforced plans.
- Removal of hedgerows (e.g., Shepperton Studios) and lack of replacements reduce biodiversity.
- Biodiversity net gains (BNG) and Local Nature Recovery Networks (LNRNs) are ineffective, with nature still being depleted.

#### *Community Values*

- Hedgerows and trees enhance privacy, seclusion, and aesthetics (e.g., Wraysbury's Garden, Rosefield in Staines).
- Residents appreciate well-maintained greenery as habitats and buffers for noise and pollution.

#### *Recommendations for Design Code*

- **Protect:** Preserve existing hedgerows and enforce Tree Preservation Orders (TPOs).
- **Sustain:** Promote native and diverse planting, avoid non-native species, and use rainwater for maintenance.
- **Enhance:** Use hedgerows as natural barriers instead of walls or fences and integrate greenery into development designs.



- **Maintain:** Ensure regular upkeep, including clearing debris and replacing removed trees.

*Key Words: Preservation, Biodiversity, Buffers, Sustainability.*

## Type 2 – Open Spaces

### *Challenges*

- Poor maintenance and safety issues (e.g., Riverside at Staines, Ashford Park).
- Limited connectivity between developments and open spaces (e.g., Bungle Nursery Proposal, Moormeade Estate).
- Lack of smooth integration with residential areas.

### *Community Values*

- **Accessibility:** Clear pathways and safe links for pedestrians and cyclists (e.g., Linear Park in Sunbury).
- **Privacy:** Green spaces backing onto homes for quiet and pleasant views.
- **Biodiversity:** Diverse planting and wildlife support (e.g., Sunbury Tree Wardens' wildflowers).
- **Community Use:** Spaces enhanced with amenities like gardens or small cafes.

### *Recommendations for Design Code*

- **Ensure Connectivity:** Link open spaces to residential areas with walkways and cycle paths.
- **Focus on Safety:** Improve lighting and reduce ASB.
- **Enhance Biodiversity:** Use native trees and wildlife-friendly features.
- **Integrate with Communities:** Design transitions that blend developments with green spaces.

*Key Words: Connectivity, Privacy, Biodiversity, Safety.*

## Type 3 – Watercourses, Ponds, and Rivers Summary

### *Challenges*

- Poor maintenance (e.g., rubbish, unadvertised spaces like River Colne).
- Limited public access (e.g., missing walkways near Staines Church Street).
- Flood risks and inadequate infrastructure (e.g., Shepperton flood zone 3).

### *Community Values*

- **Access:** Walkways, leisure spaces, and safe edges for activities.
- **Aesthetics:** Low-rise, well-designed riverfront properties.
- **Flood Resilience:** Raised properties, waterproof paths, and sustainable drainage.
- **Biodiversity:** Natural water features supporting wildlife.

### *Recommendations for Design Code*

- Ensure continuous public access to riverfronts with amenities.
- Promote biodiversity through natural landscaping and drainage systems.
- Preserve low-rise, proportional developments.
- Plan for flooding with raised structures and proper drainage.
- Improve maintenance via regular cleaning and dredging.

*Key Words: Access, Flood Resilience, Biodiversity, Maintenance, Aesthetics.*

### Type 4 – Streets and Roads

#### *Challenges*

- **Narrow pavements:** Insufficient for safe pedestrian use (e.g., Halliford Road).
- **Speeding and HGV traffic:** Problematic in residential areas (e.g., Halliford Road, A Roads).
- **Cycling infrastructure:** Often an afterthought, with unsafe or poorly designed cycle lanes.
- **Aesthetic issues:** Developments like Eden Grove feel too close to roads and lack visual appeal.

#### *Community Values*

- **Safety:** Wider pavements (minimum 2m), speed control measures, and proper crossings (e.g. pedestrian or Pegasus).
- **Green Buffers:** Trees and hedges to reduce noise, enhance biodiversity, and improve aesthetics.
- **Accessibility:** Properly designed pavements, bike lanes, and off-road parking for better flow and usability.

### *Recommendations for Design Code*

- Widen pavements to 2m minimum where possible; enforce this standard.
- Integrate green barriers like trees and hedges between roads and developments.
- Add safe cycling lanes, separated from street parking, and improve crossings.
- Design buildings set back from roads with varied facades to avoid blocky appearances.
- Implement traffic calming and enforce HGV bans on non-A roads.

*Key Words: Safety, Accessibility, Green Buffers, Cycling Infrastructure, Aesthetic Design.*

### Type 5 – Dual Carriageway and/or Motorway

#### *Challenges*

- **Noise and Pollution:** Dual carriageways like Halliford Bypass and Staines Bypass create noise pollution and unsafe environments for pedestrians.
- **Unightly Areas:** Roads like Sunbury to Staines dual carriageway and A316/M3 have neglected areas, poor habitats, and safety issues.

- **Safety Concerns:** HGV parking, debris, and dangerous pedestrian crossings (e.g., Crooked Billet roundabout, Sunbury Cross Roundabout).

#### *Community Values*

- **Noise Mitigation:** Residents value natural sound barriers, such as noise bunds and trees, to reduce traffic noise.
- **Connectivity:** Safe pedestrian and cycling routes are essential for accessibility across busy roads.
- **Aesthetic Improvements:** Green buffers and well-designed bridges are preferred over underpasses for better integration with the environment.

#### *Recommendations for Design Code*

- **Noise Barriers:** Use natural materials (e.g., trees, vegetation) for noise bunds to absorb traffic sound.
- **Safety:** Improve pedestrian and cycling access with designated routes and crossings, including wildlife corridors (e.g., hedgehog paths).
- **Aesthetic Integration:** Incorporate green buffers and well-designed bridges to enhance the visual appeal and connectivity between roads and residential areas.
- **Maintenance and Management:** Keep verges and green spaces well-maintained for safety and visibility, particularly on key roads like Halliford Bypass.

*Key Words: Noise Mitigation, Connectivity, Safety, Aesthetic Integration, Maintenance.*

#### Type 6 – Railway

##### *Challenges*

- **Noise & Aesthetics:** Residents near railways (e.g., Staines to Reading) face noise and unattractive views.
- **Safety:** Walkways like Staines Station to High Street are dark, neglected, and unsafe.
- **Proximity to Homes:** Areas like Sunbury Station are impacted by noise from both railways and nearby roads.

##### *Community Values*

- **Noise Barriers:** Trees and embankments are valued for reducing noise and blocking views.
- **Safety:** Well-lit, active routes around stations enhance safety.
- **Separation:** Prefer railways to be setback from residential areas with noise-reducing barriers.

##### *Recommendations for Design Code*

- Use natural barriers (trees, embankments) to reduce railway noise.
- Improve lighting and activity along pathways to reduce ASB.
- Buffer residential areas from railways with barriers or light industry.
- Collaborate with Network Rail to enhance aesthetics and safety.



- Ensure easy, safe access to stations while reducing noise impact.

*Key Words: Noise Barriers, Safety, Separation, Aesthetics, Access.*

## Type 7 – Residential

### *Challenges*

- **Privacy:** Lack of privacy in dense terraced streets despite trees (e.g., Sunbury Avenue).
- **Overcrowding:** New housing is often too cramped (e.g., London Road in Staines, Ashford Town Centre).
- **Access:** Poor connections between new and existing developments.

### *Community Values*

- **Privacy:** Trees and gardens are essential for privacy and community feel.
- **Community:** Easy access to amenities and green spaces fosters engagement.
- **Design:** Developments should blend with existing homes and nature.

### *Recommendations for Design Code*

- **Privacy:** Use trees and buffers for privacy without blocking sunlight.
- **Access:** Ensure pedestrian and cycling routes between developments and amenities.
- **Density:** Avoid overcrowded developments; ensure design harmony with existing homes.
- **Green Spaces:** Retain and create communal green areas.
- **Infrastructure:** Provide adequate parking and consider the impact on local amenities.

*Key Words: Privacy, Access, Density, Green Spaces, Parking.*

## Type 8 – Local Centres and Facilities

### *Challenges*

- **Lack of Amenities:** Staines Bus Station lacks public toilets.
- **Access Issues:** Facilities like Ashford Hospital and Fordbridge Centre are hard to access, with insufficient parking.
- **Underutilised Spaces:** Some centres, like Hythe Centre, don't fully use outdoor spaces for activities.
- **Limited Cultural Venues:** Few cultural spaces and limited access to some facilities.

### *Community Values*

- **Easy Access:** Pedestrian and cycling routes to facilities are key (e.g., Eden Grove, Staines).
- **Community Engagement:** Facilities should foster community use, like gardens and activity hubs.
- **Health & Wellbeing:** Centres should support physical activity and sustainability (e.g., Sunbury Gymnastics, Hythe Centre).

### *Recommendations for Design Code*

- Ensure easy pedestrian and cycling access to all facilities.
- Design spaces for community engagement and multi-use.
- Integrate sustainability, like green spaces and solar panels.
- Ensure facilities blend with their surroundings and support local needs.

*Key Words: Accessibility, Community, Sustainability, Health & Well-being, Multi-use.*

### Type 9 – Industry and Commercial Uses

#### *Challenges*

- **Lack of Greenery:** Many industrial sites are grey and lack landscaping (e.g., Lower Sunbury, A30 near Enterprise Rent-a-Car).
- **Noise and Pollution:** Proximity of industrial sites to residential areas causes disturbances.
- **Parking:** Insufficient parking at commercial sites leads to residential driveway blockages (e.g., New Street, Staines).
- **Underuse:** Some industrial areas are empty or lack amenities.

#### *Community Values*

- **Green Buffers:** Trees and hedges improve the appearance and reduce noise.
- **Vibrancy:** Adding open spaces and facilities can make industrial areas more engaging.
- **Access:** Multi-use paths are needed for pedestrians and cyclists.

### *Recommendations for Design Code*

- Add trees and landscaping to buffer industrial sites.
- Use noise barriers and tree buffers to reduce disturbance.
- Provide adequate parking to prevent overflow into residential areas.
- Create open spaces and facilities for community use.
- Ensure proper separation between industrial and residential zones.

*Key Words: Landscaping, Noise Control, Parking, Vibrancy, Buffer Zones.*

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## Task 6: High Quality Taller Buildings

Participants were invited to evaluate four distinct higher density development types:

1. Back-to-Back Mews
2. Garden Villas
3. Maisonettes
4. Podium & Towers

The panel was tasked with providing feedback on the suitability of these development types for different areas of Spelthorne. They explored aspects such as design considerations, materials, height, sustainability, amenities, and how each type could complement specific locations within the borough.

Through discussions, the panel shared insights into what works well for these developments, identified key design features and considerations, and suggested areas where each type might be most appropriate. Their feedback is summarised below and forms the basis for guiding future design codes in Spelthorne.

### Type 1 – Back-to-Back Mews

The panel suggested that the Back-to-Back Mews concept could be well-suited to specific locations within Spelthorne, such as Shepperton or riverside areas. They provided the following feedback:

- **Height & Parking:** Developments should be limited in height and include underground parking where possible to maximise space. Ground-level parking may be considered if practical.
- **Design Considerations:** The panel recommended incorporating roof gardens and individual gardens where feasible, ensuring spaces feel open and are not overly shaded. Balconies should allow light and outdoor access.
- **Accessibility:** It was highlighted that wheelchair accessibility to upper floors is essential to promote inclusivity for all residents.
- **Aesthetic & Functionality:** To avoid a sense of overcrowding, the panel advised against overly long corridors, suggesting varied, regular patterns such as inset balconies and terraces. Materials and styles should harmonise with surrounding developments.
- **Amenities & Sustainability:** The inclusion of secure bike storage, solar panels, rainwater collection systems, planting, and shared play areas was proposed to enhance community and environmental benefits.
- **Location Suitability:** The panel felt these developments would be appropriate for green spaces or riverside locations but less suitable for town centres due to their scale and parking requirements.



### Type 2 – Garden Villas

The panel identified Garden Villas as being best suited to green-edge locations in Staines, Stanwell, or areas near parks. Key points raised include:

- **Green Space:** The panel emphasised the importance of central green spaces open to the public, featuring a variety of greenery, including trees and bushes, to create usable and welcoming areas.
- **Family Focus:** Larger homes (3-4 bedrooms) designed for families were considered important, with a focus on natural light and outdoor access.
- **Parking & Sustainability:** Underground parking was recommended in areas not prone to flooding, along with sustainability measures such as solar panels, water recycling systems, and bike storage.
- **Community Features:** Family-friendly spaces such as playgrounds, splash parks, outdoor gym equipment, seating, and water features were suggested to create a vibrant environment.
- **Mixed Use:** Ground floors could house public amenities like cafes, libraries, or small retail units to foster community engagement.
- **Height & Materials:** The panel suggested developments could be up to 8 stories tall, with additional height justified by added amenities. Materials should complement local architectural styles.

### Type 3 – Maisonettes

The panel saw Maisonettes as an urban development option that could emphasise community and design innovation. Their feedback included:

- **Green Space:** Open internal green areas to public view, creating inviting shared gardens designed for functionality and safety.
- **Design Features:** The panel suggested including varied roof styles (e.g., pitched or green) and light wells for ventilation. Balconies and dual-aspect windows should maximise natural light and outdoor access.
- **Height & Layout:** They felt heights could increase to include more facilities but should remain sympathetic to local surroundings. Ground floors could include public amenities such as gyms or cafes.
- **Parking & Security:** Underground parking was considered essential, along with secure bike storage. The panel also noted that ground-floor flats should address security and privacy concerns.
- **Amenities:** Suggestions included play areas, allotments, rainwater collection systems, and solar panels. Ground-floor retail units should prioritise independent businesses.

#### Type 4 – Podium & Towers

The panel indicated that Podium and Tower developments are most suitable for urban areas like Staines Town Centre or Sunbury Cross. Key considerations from their feedback include:

- **Location & Height:** Developments should not exceed 15 stories and must be located near transport hubs. Towers should taper in height when transitioning to suburban or rural areas.
- **Design & Materials:** The panel prioritised elegant, non-brutal designs with curved structures, recessed balconies, and high-quality materials like brick and green walls. They recommended avoiding flat frontages and including communal gardens between towers.
- **Sustainability:** Solar panels, rainwater collection systems, and green roofs were seen as important features. Public rooftop spaces could include gardens or recreational facilities.
- **Community Focus:** Ground floors should host shops, cafes, or community spaces, with designs reflecting local character. Public gardens and accessible amenities were considered essential for inclusivity.
- **Transport & Parking:** The panel stressed the need for excellent public transport links and reduced reliance on cars. Underground parking should prioritise residents, with secure bike storage provided.

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#### **Staines-Specific Feedback from the Citizens' Panel**

The panel shared several ideas and preferences for developments in Staines, reflecting a desire for thoughtful design that enhances liveability and community wellbeing. Key points included:

- **Rethinking Transport and Roads:** The panel proposed removing cars from the streets in Staines, relying instead on taxi or ride-sharing services such as Uber to reduce congestion and improve air quality.
- **Design and Aesthetics:** Larger windows were highlighted as essential for allowing natural light to promote health and wellbeing. The panel expressed a strong preference for moving away from building square blocks, suggesting stepped-back floors at higher levels to create a less imposing visual impact.
- **Architectural Style:** Regency-style designs, as exemplified in one of the images presented, were favoured for their elegance and timelessness. The panel noted that shorter or staggered buildings, if designed with sophistication, would mitigate concerns about height.
- **Creating Harmony:** Overall, the panel emphasised that well-considered, elegant architecture, combined with functional design, could transform Staines into a more appealing and harmonious urban environment.

## Conclusion

The Citizens Panel provided valuable feedback that will inform Spelthorne's Design Code, ensuring future developments align with the community's vision. They emphasised preserving green spaces, integrating sustainable practices, and fostering inclusivity across all neighbourhoods.

Specific priorities included protecting heritage sites, improving public transport links, and addressing challenges like poor lighting and underutilised spaces. For new developments, the panel encouraged thoughtful designs that blend functionality with aesthetic appeal, such as incorporating natural light, accessible green spaces, and community facilities.

These guiding principles reflect the feedback and suggestions provided by the panel, aiming to create vibrant, sustainable, and inclusive developments that serve Spelthorne's diverse communities.



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# Appendix 4- Draft Code Public Engagement Report (Stage 3)

May 2025



## Report purpose

The purpose of this report is to provide a comprehensive summary of the key outcomes from the public and stakeholder engagement regarding the Spelthorne Design Code. This includes the main areas of feedback received during the public engagement, which took place from 18 March to 8 April 2025, and subsequent proposed changes to the draft Design Code.

## Reason for consultation

The Spelthorne Design Code (SDC) is being produced to support the delivery of high quality design of new development in the borough, that is reflective of local character and design preferences. It will set out the design requirements for proposed new development to ensure that it is locally supported, sustainable and functions well for all. The Design Code team wanted to hear community views about the draft Design Code, that had been developed following previous community input, to check if we had addressed the key priorities and if it could be improved. This consultation also formed a part of stage 3 (Test phase) of the project, enabling the public and other key stakeholders to test the draft Code.

## Approach

The draft Design Code engagement opportunity was accessible via the SDC Commonplace, an online engagement hub. The consultation was spread over the following three pages for people to read through, understand the draft Code and share their opinions and improvement recommendations on it:

- **What is in the draft Code**: a page for people to find out what is in the Design Code, understand the key elements of the Code and download a copy to read it in full
- **How the Design Code has responded to the community**: a page for people to find out how the Design Code has reflected the views of Spelthorne's communities on key design priorities
- **Survey to get feedback on the draft Code**: a page where local people could tell us what they thought about the draft Design Code and play their part in its creation. While the survey to get feedback on the draft Code has closed, members of the public can still view all of the comments submitted [here](#).

In order to generate public interest and feedback and to enable and encourage local people to have a say at this stage, before a more technical statutory consultation, the draft Code engagement opportunity was extensively communicated.



## Stakeholders

The draft Code engagement opportunity was communicated to the following stakeholders, which you can see in the diagram below.

### Local Community

- Any interested local resident and regular visitor
- Citizens' Panel- a demographically representative panel of Spelthorne Residents
- Local community centres, libraries and other local groups

### Local Businesses

### Staff Members/ Internal Teams

- Development Management Team
- Assets Team
- Economic Development Team
- All staff

### Elected Members

- Design Code Task Group
- All Councillors

### Housing Associations

### Resident Associations

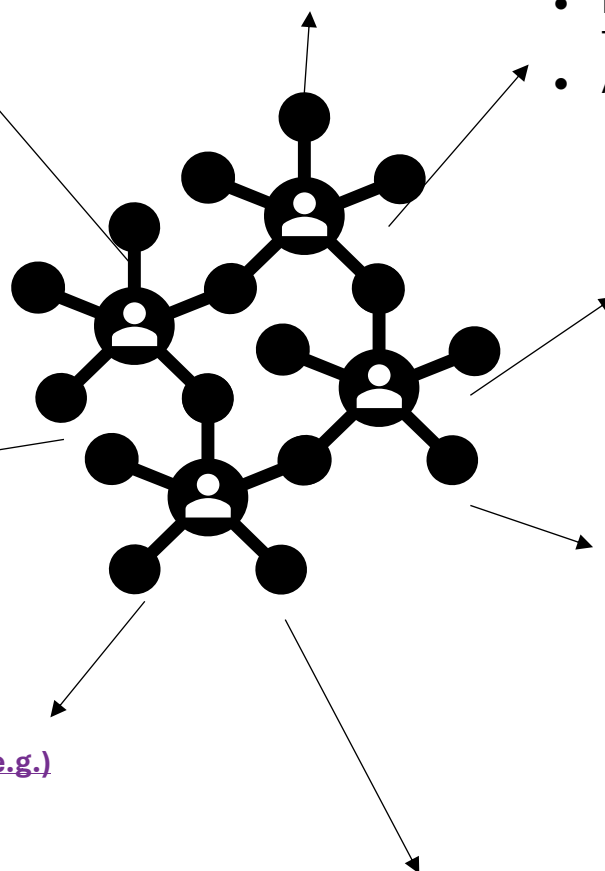
- 60+ Resident Associations and local community groups
- Spelthorne Partnership Assembly

### Key Statutory Consultees (e.g.)

- Surrey Highways
- Environment Agency
- Sport England and local Active Partnership
- Surrey Flooding
- Active Travel England
- Surrey Police
- Staines Business Improvement District
- Surrey Developer Forum

### Young People

- Schools- Bishop Wand School, The Matthew Arnold School, Thamesmead School, Thomas Knyvett School, St Paul's Catholic College Sunbury Manor School
- Youth Groups- Ashford Youth Club, scout groups
- Citizens' Panel- applicants and Panel members from 13 years old



## Communications and promotions

Many communication channels were used, both online and in-person to promote the draft Code engagement opportunity and gather feedback on the draft Code.



### SBC social media

**What: Social Media Posts** (Instagram, Facebook, X, LinkedIn etc.)

- Regular posts throughout consultation
- Countdown posts published to highlight opportunity closing soon

**Social Media Stories** (Instagram, Facebook)

**Social Media Advert**

- Ran from 21/03/25- 8/04/25 targeted at whole borough area

**YouTube video**

**Result:** lots of people aware and directed to the consultation. Posts, videos and advert generated 130,000+ views, reached almost 300,000 people and had 1000+ local people engage and visit the consultation

### Physical display promotion

**What: Posters**

- On noticeboards, in council offices, libraries, community centres and other key locations

**Digital Screens**

- In Elmsleigh and Staines High Street

**Result:** Directed people to the consultation. Over 40 people scanned QR Code and followed through to the Commonplace.



### News articles

**What: Press release**

**Spelnet news**

**Commonplace news stories**

- 8 news stories published on engagement

**Council e-news**

**Spring Bulletin**

**Result:** More people aware of the consultation and receiving updates about the progress of the project. Over 300+ people receiving Commonplace news about the engagement opportunity, which translates to more people following along and engaging in the Design Code.

## Councillor communication



### *What:* **Emails to Councillors**

- Regular prior to, and throughout, consultation

### **Presentation at All Member meeting**

### **Member Briefing Note**

*Result:* Regular Councillor contact and communication, enabling them to understand the progress of the project, engagement opportunities for residents and be able to communicate these opportunities.



## In-person communication and engagement

### *What:* **Citizens' Panel session**

- Input from the Spelthorne Design Code Citizens' Panel, which was a randomly selected group of 40+ local that is demographically representative of the borough, has been instrumental in developing the draft Code through two in-person sessions held in November 2024.
- A third and final in-person session with the Spelthorne Design Code Citizens' Panel took place in March 2025, where participants collaboratively reviewed and tested the draft Code.
- Activities/ parts of the session included:



1. Explanation from the project team about the content of the draft Design Code, its development and role in planning, how public feedback (including Citizens' Panel input) shaped it and who will use it.
2. Applying the draft Design Code to real-world scenarios by using past developments within the Borough as case studies.
3. Sketching out design principles that could be used to positively influence future developments in Suburban, Town Centre Neighbourhood and New Residential Neighbourhood.

*Result:* An engaged local resident group could further understand the Code, raise questions, provide feedback and suggest improvements from a diverse range of perspectives. The Panel could also see how their input so far had helped co-produce the Code and that their views were being directly translated into the Code to make it reflective of the Borough's needs and aspirations. Some Panel members expressed in a survey following the last session that:



- “[It’s] good to see ideas from the Panel sessions included in the Code”
- “I learnt a lot and felt that my thoughts and opinions influenced the final product of the code.”
- “I am hopeful that it will result in real urban design change.”
- “I believe all ideas and comments have been considered and implemented where appropriate.”

### Key findings:

1. General Agreement: The Design Code is effective for both large and small sites.
2. Smaller Sites: Particularly valuable for guiding development on smaller infill sites, which, though individually minor, collectively shape much of the built environment.
3. Clarity Needed: The Code would benefit from a clearer introductory section outlining how it relates to the wider planning system.
4. Heritage Considerations: The treatment of heritage buildings remains unclear, especially given existing protections like Conservation Areas and Listed Building status.
5. Larger Sites: Ensuring strong foundational principles for larger sites is viewed as essential, especially as a final task.

**Feedback:** Overall, the Spelthorne Design Code Citizens’ Panel received positive feedback from Panel members, with **95%** of those who answered the survey selecting the **‘3-5’ range** when asked on a scale of 1-5 how they felt the Panel was representative of the wider community. Panel members also appreciated the opportunity for different voices in the community to be heard, with one Panel member saying: *“It has been an engaging experience, very interesting to hear various perspectives from other members”*. Many Panel members also indicated they would endorse this experience, with **79%** saying they would recommend other residents to become a member of a Spelthorne Citizens’ Panel. This demonstrates the high levels of engagement and invaluable diverse local participation, as





### *What:* Emails to Residents' Associations

- ### Emails to other community groups (housing associations etc.)

- ## Presentation at Spelthorne Partnership Assembly

**Result:** Other people and community members are more aware of the draft consultation and so it was shared with a wider group and range of local people.

## Draft consultation survey response overview

- **45 responses**
- Most responses were from the **wider community user group**
- Most common impression of the Code was **neutral** (on a scale of very negative to very positive)
- **80%** respondents felt a neutral to very positive impression of the draft Code
- The most common area for improvement indicated was **scale, massing and density**
- For the design vision and principles (borough-wide and for specific places in Spelthorne), the most common responses were **support**
- The most common rating for how people felt the Code had responded to community priorities was **3** (on a scale of 1-5)
- The community priority people found the Code had responded to the **best** was **Community Priority 1: Importance of green open spaces**
- The community priority people found the Code had responded to the **least** was **Community Priority 5: Climate change and flood resilience**
- Some concerns were: **flood risk, infrastructure provision, green space provision and preservation, height and density, design quality, housing type and affordability, Code weighting, Staines focus, and enforcement**

The next section of this document will further address the key themes and feedback received from the survey on the draft Code, Citizens' Panel and key stakeholders.

In addition, a further section has been included to consider other issues and concerns raised during the engagement that do not directly relate to the Design Code. This section will guide you on the relevant planning policies, guidance or documents associated with those matter, ensuring transparency and clarity.

## Key themes and feedback from the public and stakeholders

### Heights and marker buildings

#### You told us...



Confusion was noted around the role and status of 'Marker Buildings' and their potential for additional height.

Clarity needed around height of different types of storeys

Community feedback indicated concerns about height increases, particularly in the Bridge Street Car Park, Debenhams site and Staines Conservation Area

Some feedback suggested a need for clearer definition and distinction between marker buildings and potential height extensions.

#### The proposed changes include...

- Marker building definition made clearer.
- Marker Buildings will be differentiated from height extensions and will only be designated in areas where a distinctive structure would enhance townscape legibility. Additional height will be visually set back from the street.
- Extend the heights plan to cover the Staines Village Conservation Area, to provide further reassurance of protection.
- Avoid ambiguity in storey heights by including heights diagram and approximate heights throughout text to supplement storey heights.



## Information accuracy, presentation and document navigation

### You told us...



Feedback from the DM Team and community indicated that some sections of the Design Code were difficult to navigate and understand.

Diagrams could be more clearly presented and labelled in places.

Information needing to be updated in the section about the history and background of Spelthorne's places



### The proposed changes include...

- The document will be reformatted to include:
  - graphical highlights
  - cross-referenced definitions
  - specific checklists at the end for clarity.
- Clearer diagrams throughout and labelling improved
- Factual updates to 'Places Past, Present and Future' section for accuracy

## Residential extensions and backland development

### You told us...



The DM Team emphasised the need to include more comprehensive guidance for suburban infill, apartment, and backland development.

### The proposed changes include...

- A specific section on residential extensions will be reinstated, consolidating guidance previously in a separate SPD and ensuring continuity of guidance.
- Removal of 'backland plots' in this area type due to the geometry of plots.

## Landscape character and river frontage

### You told us...



From Task Group and community feedback, more reference to planting, trees and materiality of the landscape required

More emphasis and design consideration needs to be placed on the river frontage area

### The proposed changes include...

- Additional content will be added to emphasise planting, landscape materiality, and river frontage design.
- A new town centre strategy has been created for the river frontage, setting out the character that is intended within different parts of the frontage and potential strategic interventions.

## Conservation areas

**You told us...**



Increased references to Conservation Areas were requested to ensure that the Design Code effectively protects heritage areas.

**The proposed changes include...**

- Additional clarity will be provided regarding design considerations in Conservation Areas, particularly for Staines village.

## Groundwater and flooding

**You told us...**



Concerns were raised regarding references to groundwater flooding.

**The proposed changes include...**

- Highlight references to flood risk and emphasise the need to demonstrate that the scheme does not negatively impact groundwater flows through evidence of an engineering solution as part of the planning application.

## Other concerns raised outside the scope of the Design Code

General key concern	Where it is dealt with
<p>Infrastructure provision</p> <ul style="list-style-type: none"> <li>- Police</li> <li>- Doctors</li> <li>- Schools</li> <li>- Public transport</li> </ul>	<p>The delivery of supporting infrastructure is covered by the <a href="#">emerging Local Plan</a>. <a href="#">Healthy Streets for Surrey</a> also covers design principles in relation to road design, integrating public transport and pedestrian and pavement design.</p>
<p>Access and congestion in roads</p>	<p>Surrey County Council's <a href="#">Local Transport Plan</a> and the <a href="#">emerging Local Plan</a> cover transport policies, schemes and other transport matters. <a href="#">Healthy Streets for Surrey</a> also covers accessibility and traffic design principles.</p>
<p>Current buildings and development</p>	<p>The Code will come into play in terms of the design of all <a href="#">future</a> development coming forward.</p>
<p>Scope of the Code</p>	<p>The Spelthorne Design Code will set out the design requirements for proposed new development in the borough. It will ensure that new development is locally supported, sustainable and functions well for all its users. The Design Code will be used to determine whether planning applications are acceptable in design terms, and will support the emerging Spelthorne Local Plan. It will contain simple, concise, illustrated design requirements for streets, open spaces and buildings. It will also set out expectations for the process to be followed when proposals are designed. It will be based on wide-ranging input including that from the local community, other stakeholders and wider understanding of the places within the borough, to ensure it is locally-supported, robust and can be used in practice.</p> <p>The Spelthorne Design Code includes:</p>



		<ul style="list-style-type: none"> <li>• A vision for future development in the Borough and its key places</li> <li>• Design principles across a range of topics</li> <li>• Expectations for a comprehensive and considered approach to the design process</li> <li>• Tailored design requirements for different area types within the Borough, covering buildings, open spaces, streets and public Realm, Landscape and other physical aspects of the design of proposals.</li> </ul> <p>As mentioned in this section, there are many other documents, policies and guidance that deal with other issues.</p>
What built form envelopes are	→	The built form envelope is the outward-facing built-up area. The Design Code sets out design requirements for future development in response to current and future built form.
Amount of new homes and allocations	→	The number of new homes are allocated in the <a href="#">emerging Local Plan</a> (Site Allocations).
Flooding	→	Flooding is covered in the current <a href="#">Development Plan 2009</a> (Policy LO1), <a href="#">emerging Local Plan</a> 2024-2039 (Policy E3), <a href="#">Climate Change SPD</a> , <a href="#">Flooding SPD</a> , <a href="#">Healthy Streets for Surrey</a> (SuDS).
How residents' views are being heard and how they are heard in planning applications	→	The Design Code is based on wide-ranging inputs including that from the Spelthorne community, other stakeholders and a wider understanding of the places within the Borough, to ensure it is locally-supported, robust and can be used in practice. The process has prioritised and been based around local engagement at every stage, including the use of an innovative Citizens Panel (a demographically representative group of Spelthorne residents), to ensure that the Code reflects and responds to community views and visions for the Borough.



Additional statutory public consultation will take place before the final version of the Design Code is officially adopted. In terms of engagement during the planning application process, the Council has produced a Statement of Community Involvement (SCI) that set out how and when the community can be involved in the preparation of the planning policy documents and the determination of planning applications. Residents and stakeholders can get involved in the planning application process by viewing and commenting on proposals. The Council encourages applicants to seek pre-application advice and guidance on a development proposal before submitting a planning application.

## Next Steps

The revised draft Design Code incorporating the proposed changes based on the above feedback from diverse stakeholders will be presented to the public for a statutory consultation.

## Implementation

Design requirements are arranged as follows:

- **Must:** to comply with the Design Code, all proposals must adhere to these requirements
- **Should:** all proposals should comply with these requirements unless non-compliance can be justified, and demonstrating compliance will add supporting weight to the design element of the planning application decision

Some parts of the Code sets out guidance, best practice or design inspiration from elsewhere that could provide the basis for the development of design proposals. These are design ideas that development **could** implement, and are highlighted as such.

The Design Code is to be used following a principle of ‘Comply or Justify’. Deviation from requirements set out will only be permitted with robust and evidence-based justification that any proposed design solutions still achieve the underlying Aim of the requirement. Deviation from ‘must’ requirements will require a very high level of justification.

Proposals that do not comply with these principles and fail to provide compelling justification are likely to be refused.

## Conclusion

The engagement process has provided valuable insights into community and stakeholder priorities, leading to substantive changes to the Spelthorne Design Code. These changes aim to enhance clarity, address specific concerns around height, conservation, and residential extensions, and ensure the document is accessible and informative for all users.

## Appendix 5 – Executive Summary

### Spelthorne Design Code

#### Executive summary

##### What is the issue

Local residents have expressed dissatisfaction with poor quality design of some developments in the Borough.

There is sensitivity about the type and form of proposed development in built up areas such as Staines town centre given the scale and impact this may have.

##### How the issue can be addressed

The national planning policy framework includes a requirement to deliver well designed places and buildings.

It includes guidance to enable local authorities to set local standards of design that developers are required to meet and that address the concerns of local communities. The National Model Design Code and National Design Guide set out the process and approach for local authorities to produce a design code.

A design code is ‘A set of illustrated design requirements that provide specific, detailed parameters for the physical development of a site or area.’ It sets the standards for development that developers will be required to meet for new development.

##### What we have done

Appointed consultants that have the expertise to write a design code and who have previous experience of advising Spelthorne.

Set up a Task Group and Project Board, including Councillors, external advisors, consultants, and officials.

Appointed an [communications digital platform](#), Commonplace, that enables and supports community engagement and issuing regular updates.

Set up a Borough wide engagement strategy including workshops, 121 meetings, exhibitions and digital engagement so that all residents had the opportunity to communicate their views about positive and negative aspects of their environment.

This included workshops with young people and schools.

A Citizens’ Panel was set up with more than 40 local residents from each settlement and representative of age, ethnic background and disability who volunteered to work with consultants to help shape the Design Code content.



The consultants, directed by the officials and Task Group and informed by the consultation process, produced a comprehensive suite of information that covers all aspects of design quality and sets out what a well-designed places should address.

The Citizens' Panel has reviewed the Draft Design Code and confirmed that it reflects their views.

Other stakeholders, including local civic societies, statutory consultees, developers and experts have been given the opportunity to express their views during the preparation of the Design Code.

### About the Spelthorne Design Code

The format and process for developing the Spelthorne Design Code has been informed by the National Model Design Code guidance document and latest guidance from the MHCLG, the Government Department that sets out planning policies.

It includes an appreciation of Spelthorne and its key places, an outline vision for its future, identifies the primary areas of change that aligns with the Local Plan and sets out design requirements for new development.

These include proposals for the form of buildings, their height, scale and massing, landscape and natural environment, movement and transport principles and quality of spaces between buildings.

It reflects the key issues raised in the community engagement process.

A feature of the document is the focus on specific areas where change is necessary and anticipated including Staines Town centre and Sunbury Cross. It provides guidelines on future residential sites that might arise, and for infill and domestic extensions.

The Spelthorne Design Code can be found in Appendix 1.

### What supporting information is provided

The report in the E&S pack provide further detail to support this summary report. These include appendices to cover:

Approval to Conduct Statutory Consultation on Spelthorne Design Code – Committee Report;

A Draft Copy of the Spelthorne Design Code (Appendix 1) – this is the document that is proposed for approval;

Approach to Statutory Consultation Strategy (Appendix 2) – this sets out the process for statutory consultation of the document;

The Engagement Process (Stage 1 to Stage 2) Draft (Appendix 3) – this sets out the approach that has been taken to engage with stakeholders;

Draft Code Public Engagement Report (Stage 3) (Appendix 4) – this sets out the approach that has been taken to engage with stakeholders

### How will the Design Code be implemented

Subject to the decision by E&S Committee, there will be further consultation for a 6-week period and subject to the outcome the Draft will be further revised and represented to E&S prior to submission for full Council approval.

This provides further time for the views to be expressed including those of other agencies such as developers and landowners if they have not done so to date.

Following the public consultation and any final changes that are required, the Spelthorne Design Code will be adopted as a Supplementary Planning Document that supports the Local Plan.

### Why this is important

The adoption of a Spelthorne Design Code would be a first most important step towards ensuring new development is high quality and reflects the communities priorities. Other steps will need to follow.

#### Options for the E&S Committee

Option 1 - Recommended option: The Committee approve the commencement of the public consultation.

Option 2 – Not Recommended: The Committee resolve to seek further amendments to the SDC before commencement of public consultation.

This option is not recommended as any delay in the start of the statutory consultation, outside the window set out at para 2.25 [of the committee report] would mean that it would not be advisable to begin the consultation until September, as consulting during summer typically results in lower levels of engagement. The knock-on effect of this would mean the adoption of the Spelthorne Design Code would be delayed, likely to late 2025/early 2026.

Option 3 – Not Recommended: The Committee resolve to reject the request for publication of the SDC.

This option is not recommended as in order to adopt the Spelthorne Design Code, so that it can be considered as a material consideration in decision making a statutory consultation is required by legislation as set out above. If a public consultation is not held the project cannot move forward and the Spelthorne Design Code cannot be adopted.

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